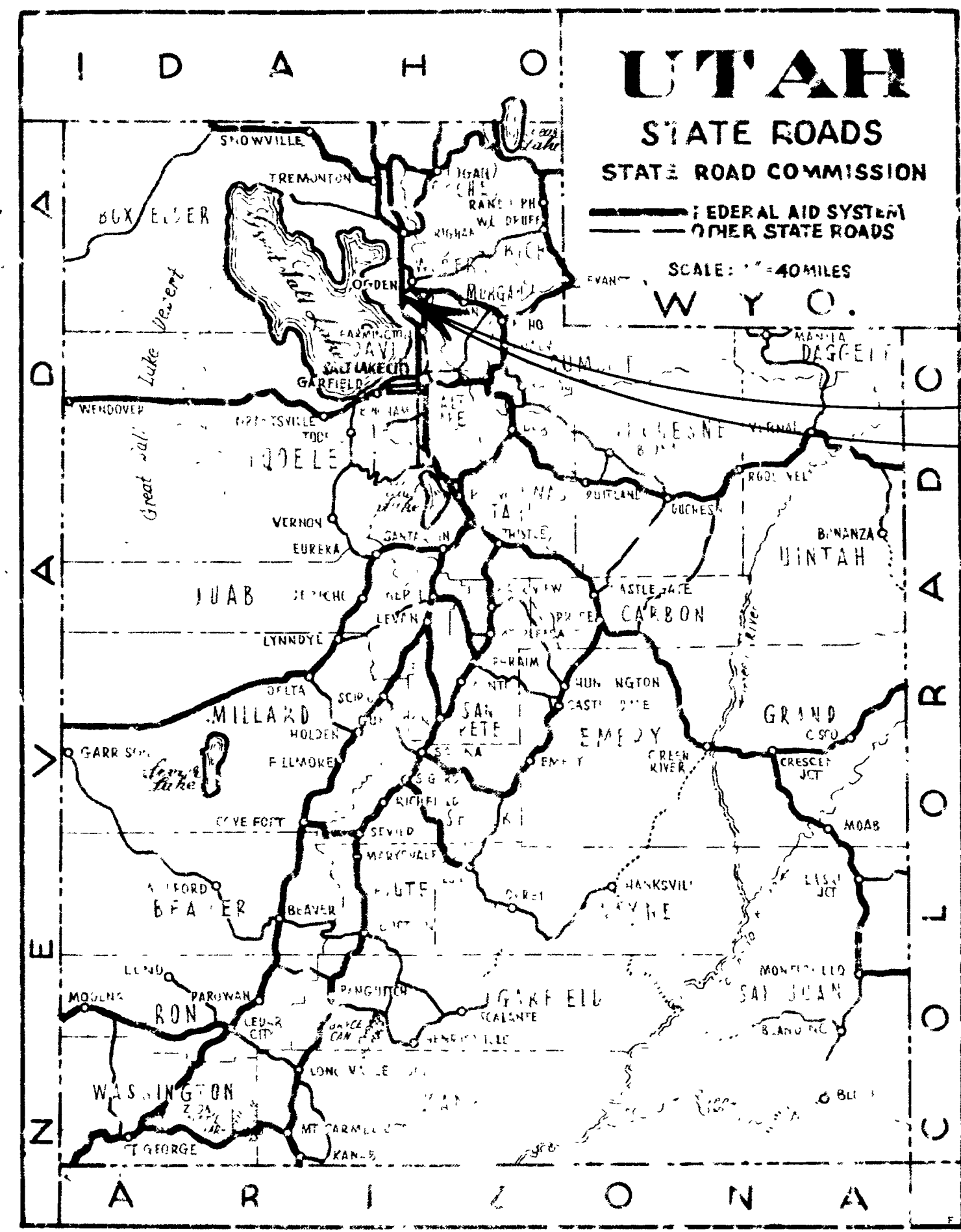


STATE OF UTAH STATE ROAD COMMISSION



1-15-7(58) 332
1-15-8(20) 334
1G-15-8(18) 334

PLANS OF PROPOSED STATE ROAD

FEDERAL AID PROJECT
1-15-7 (58) 332
LENGTH = 2.585 MILES
DAVIS COUNTY

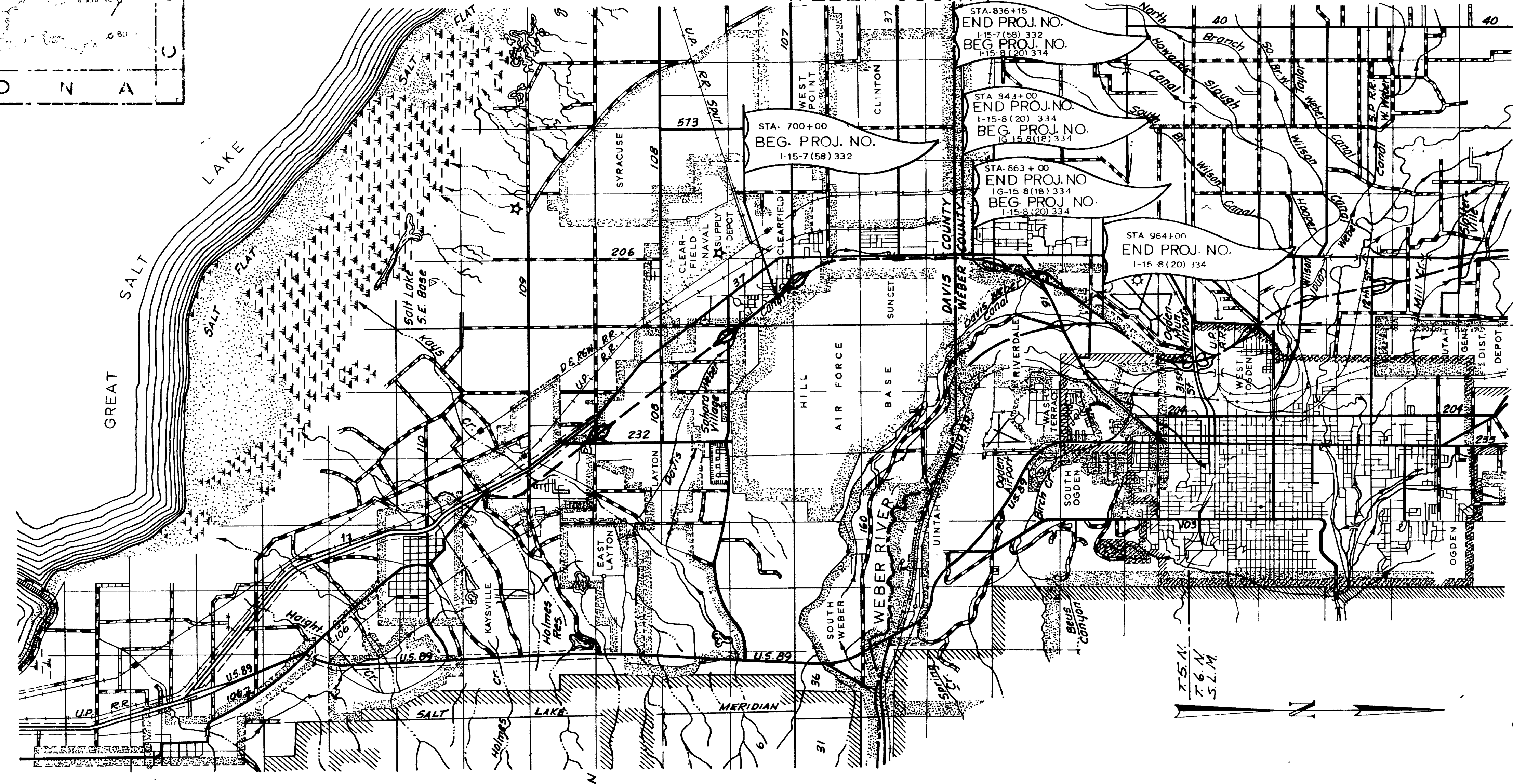
1-15-8 (20) 334
LENGTH = 2.043 MILES
WEBER COUNTY

1G-15-8(18) 334
LENGTH = 0.380 MILES
WEBER COUNTY

For Plans 1-15-7
Davis County #17

INDEX TO SHEETS

NO.	DESCRIPTION	DRAWING NO.	DATE
1	TITLE SHEET		
2A-C	TYPICAL SECTIONS & MAT'L S. SHEETS		
3A-E	SUMMARY SHEETS		
4-31	PLAN AND PROFILE		
32-44	HILL FIELD OVERPASS	D-719	7/29/62
45-69	URRR. OVERPASS	F-80	8/5/67
70-95	5600 SOUTH OVERPASS	F-66	8/21/63
96-101	5600 SOUTH CANAL X-ING	D-744	12/10/62
102-131	RIVERDALE ROAD UNDERPASS	C-453	2/11/63
132-145	4400 SOUTH UNDERPASS	D-735	9/25/63
146	STD. RAILING DETAILS	MR-1	
147	CONCRETE PILES	SP-1	
148	STD. STEEL DETAILS	SS-3	
149	STD. CATCH BASINS	V-752	
150	STD. CATCH BASINS	V-753	
151	STD. CLEANOUT BOXES	V-777	
152-156	STD. ROAD SIGNS	500-1-5	
157	STD. FLARED END SEC. RCP.	B-160-1	
158	RIGHT OF WAY MARKERS	B-164	
159	SUPERELEVATION & WIDENING	202	
160	LEFT TURN MEDIANS	211	
161	CONCRETE CURB & GUTTER	220	
162	DEEP BEAM GUARD RAIL	230	
163	RCP. CULVERTS	520	
164	DROP INLETS	346	
165	CONSTRUCTION IDENT. SIGNS	504	
166	CHAIN LINK FENCE	523	



UTAH STATE DEPARTMENT OF HIGHWAYS
RECOMMENDED FOR APPROVAL: AUG., 1963
Blaine J. Kay
ENGINEER OF ROADWAY DESIGN
RECOMMENDED FOR APPROVAL: AUG., 1963
Blaine J. Kay
ENGINEER OF PLANS AND CONTRACTS
APPROVED: AUG., 1963
James E. ...
ACT. DIRECTOR OF HIGHWAYS

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
APPROVED: _____ DATE _____
DIVISION ENGINEER

5600 SOUTH STREET

IV E S T B D C U V D S T R U C T U R E

Abut # I Drilled

No.	Date	Time	Length to Place	No. Blows last Ft.
1	4-29-64	8:50	22	38.4
2	"	8:45	18	38.3
3	"	8:30	18	38.4
4	"	9:17	16	38.1
5	"	9:02	18	37.7
6	"	9:26	16	38.5
7	"	9:45	17	38.5
8	"	9:35	19	38.2
9	"	9:56	16	37.9
10	"	11:20	16	38.0
11	"	11:32	16	38.0
12	"	11:56	16	38.2
13	"	11:46	17	39.0
14	"	12:45	16	38.0
15	"	12:57	17	38.8
16	"	1:03	18	38.0

Total length of Pile Abut # I 612.0 Ft.

Bent # II

No.	Date	Length to Place	No. Blows last Ft.
1	2-7-64	20.0	63
2	"	19.1	58
3	"	19.8	65
4	"	19.0	72
5	"	21.0	78
6	"	19.9	64
7	"	19.8	73
8	"	16.0	77
9	"	14.0	63
10	"	16.0	58
11	"	22.0	75
12	"	20.2	67
13	"	21.9	82
14	"	24.7	73
15	"	19.7	68
16	"	24.6	77
17	"	23.8	63
18	"	20.1	82
19	"	18.9	72
20	"	18.9	75
21	"	19.1	63
22	2-10-64	19.0	67
23	"	14.8	78
24	"	16.8	75
25	"	21.8	60
26	"	18.8	72
27	"	18.0	75
28	"	18.4	64
29	"	18.0	74

Total length of Pile Bent # II 565.1 Ft.

Bent # III

No.	Date	Length to Place	No. Blows last Ft.
1	2-11-64	21.0	65
2	"	19.0	66
3	"	22.0	62
4	"	15.2	68
5	"	18.3	59
6	"	16.3	72
7	"	16.7	65

Total length of pile 2313.2 Ft.

No.	Date	Length to Place	No. Blows last Ft.
8	2-11-64	24.0	72
9	"	16.7	68
10	"	26.5	72
11	"	26.2	68
12	"	19.8	59
13	2-10-64	26.6	72
14	"	18.7	58
15	"	20.0	63
16	"	20.3	67
17	"	21.4	62
18	"	19.4	64
19	"	18.7	60
20	"	20.8	69
21	"	19.2	61
22	"	20.4	67
23	"	19.4	70
24	"	17.4	63
25	"	17.4	65
26	"	21.2	72
27	"	19.8	68
28	"	19.4	66
29	"	22.3	69

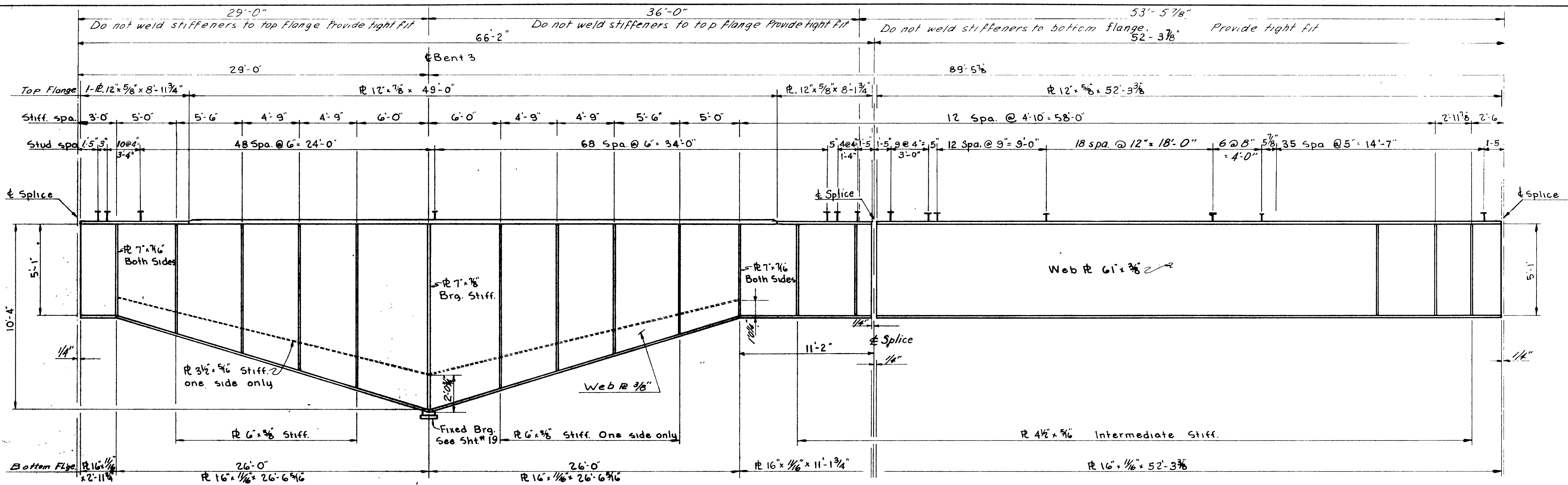
Total length of Pile Bent # III 583.1 Ft.

Abut # IX drilled

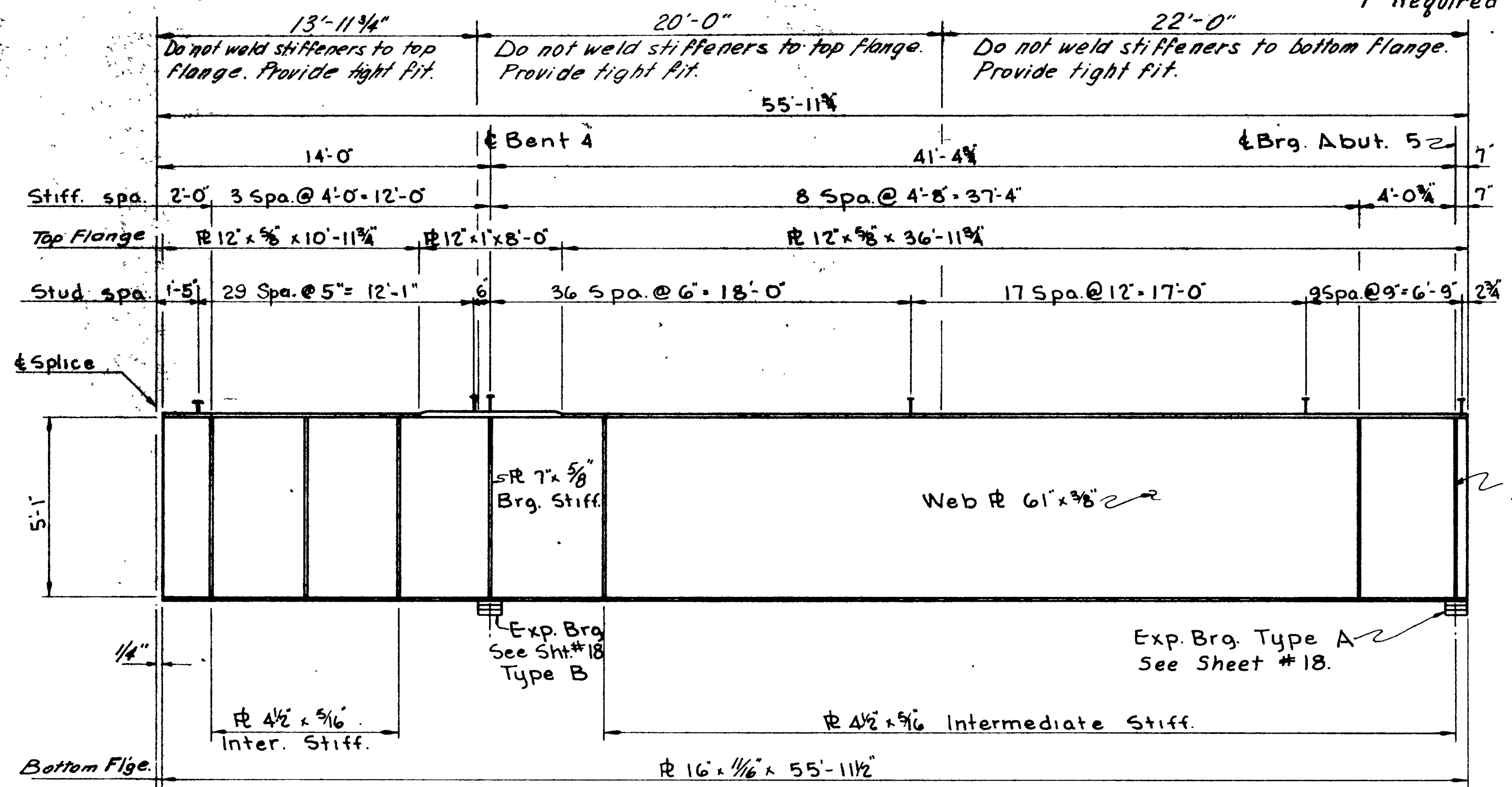
No.	Date	Time	Length to Place	No. Blows last Ft.
1	4-28-64	10:15	16	27.0
2	"	11:13	14	34.4
3	"	10:32	15	36.2
4	"	11:35	9	24.7
5	"	9:55	21	31.0
6	"	12:00	9	22.1
7	"	10:40	24	37.8
8	"	1:10	18	32.1
9	"	10:50	20	38.0
10	"	3:04	20	37.5
11	"	2:52	19	37.6
12	"	3:11	21	38.0
13	"	2:30	20	38.3
14	"	3:40	19	38.2
15	"	2:15	18	37.8
16	"	3:30	18	37.3

Total length of Pile Abut # IX 553.0 Ft.

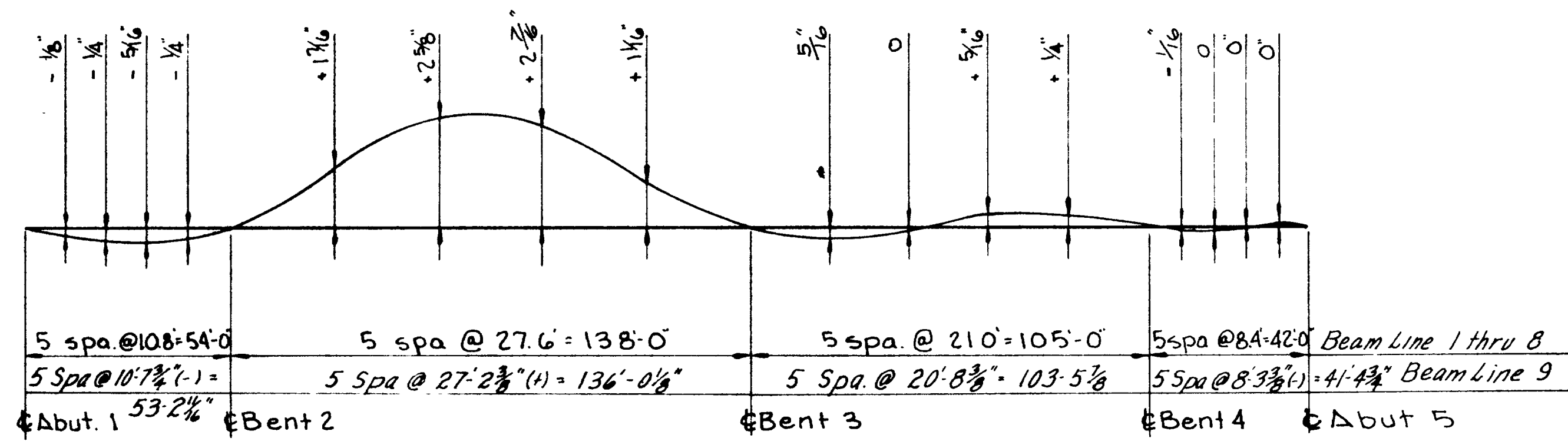




GIRDER B-3A
1 Required



GIRDER B-4A
1 Required



CAMBER DIAGRAM

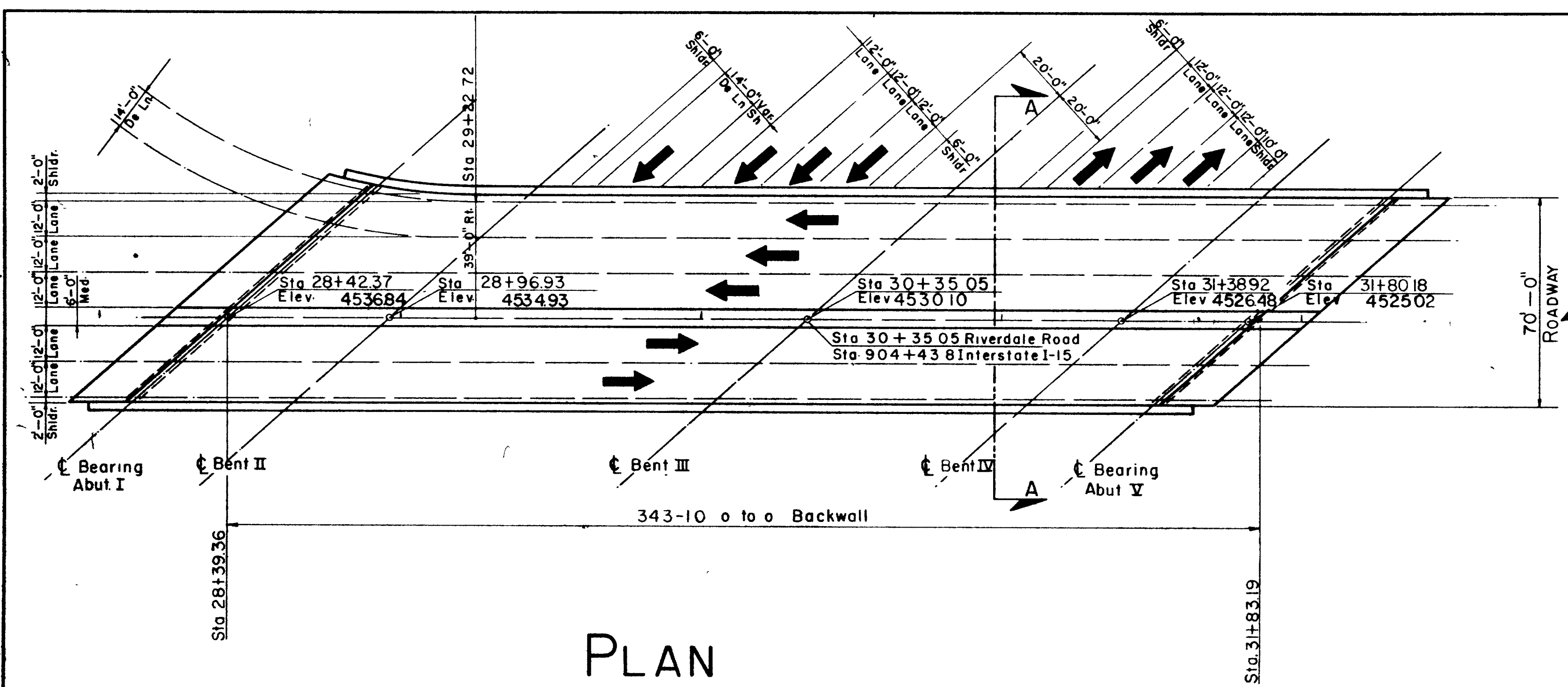
NOTES:

1. For diaphragm connector spacing See Framing Plan.
2. All Intermediate stiffeners (R 4 1/2 x 5/16) are on one side only, except as noted.
3. Camber shown is equal to 1.25 x total D.L Deflection
4. All bearing stiffeners on both sides.

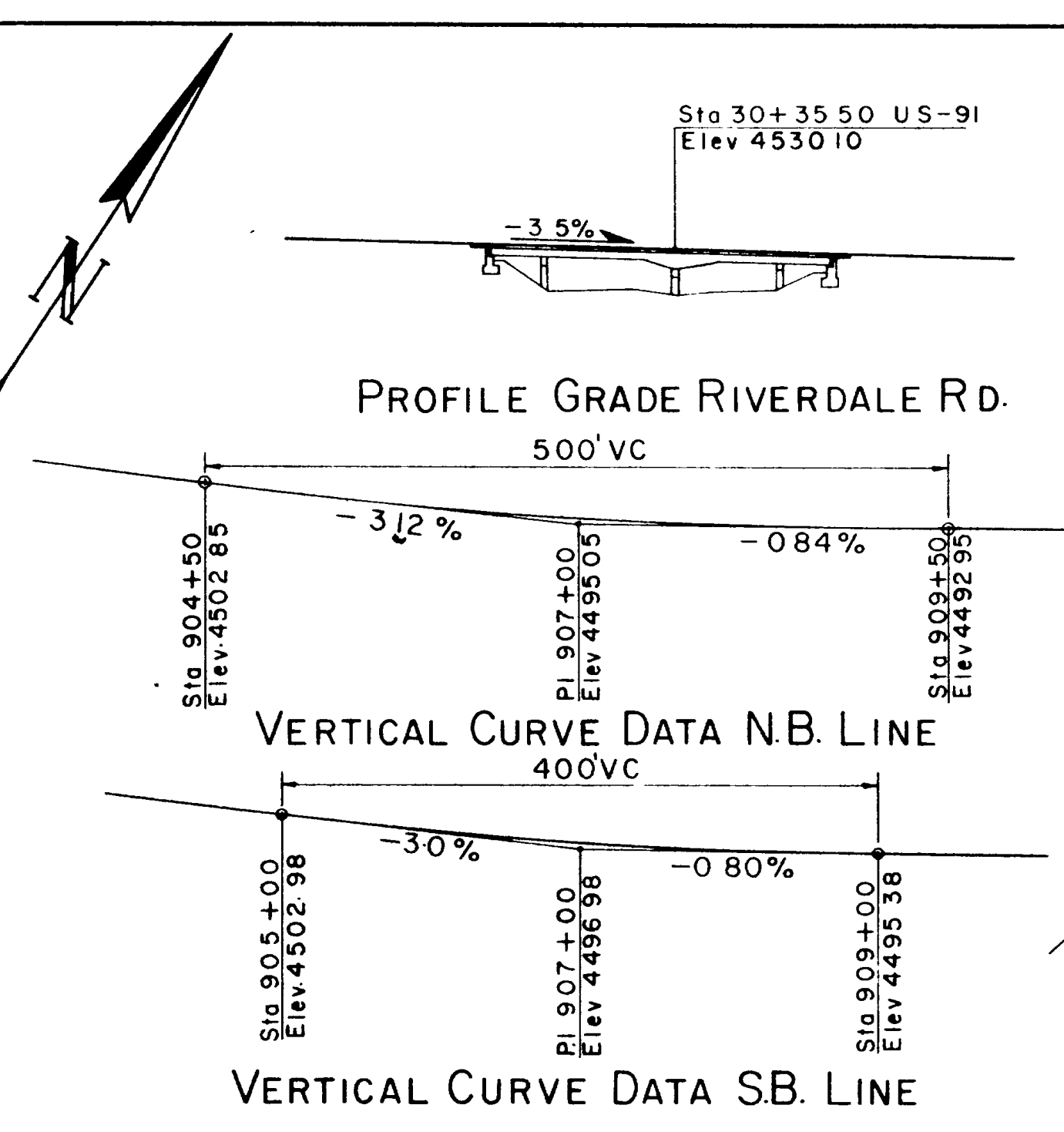
UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS-WEBER CO. LINE TO 4400 SO. RIVERDALE ROAD U'PASS			
GIRDER DETAILS			
DESIGNED BY R.B.U.	CHECKED BY C.E.J.	115-8(20)334	
DRAWN BY D.V.B.	CHECKED BY J.E.H.	PROJECT NUMBER	
QUANTITIES BY R.G.P.	CHECKED BY C.E.J.	904+43.8	
APPROVAL RECORD 8/6/63 Joseph B. West	DATE	STATION	
APPROVED Sept 10, 1963	D. L. SANCHEZ	WEBER	
BR NO	DRG NO C-453	17 OF 29	

NO	BY	DATE	REMARKS
REVISIONS			

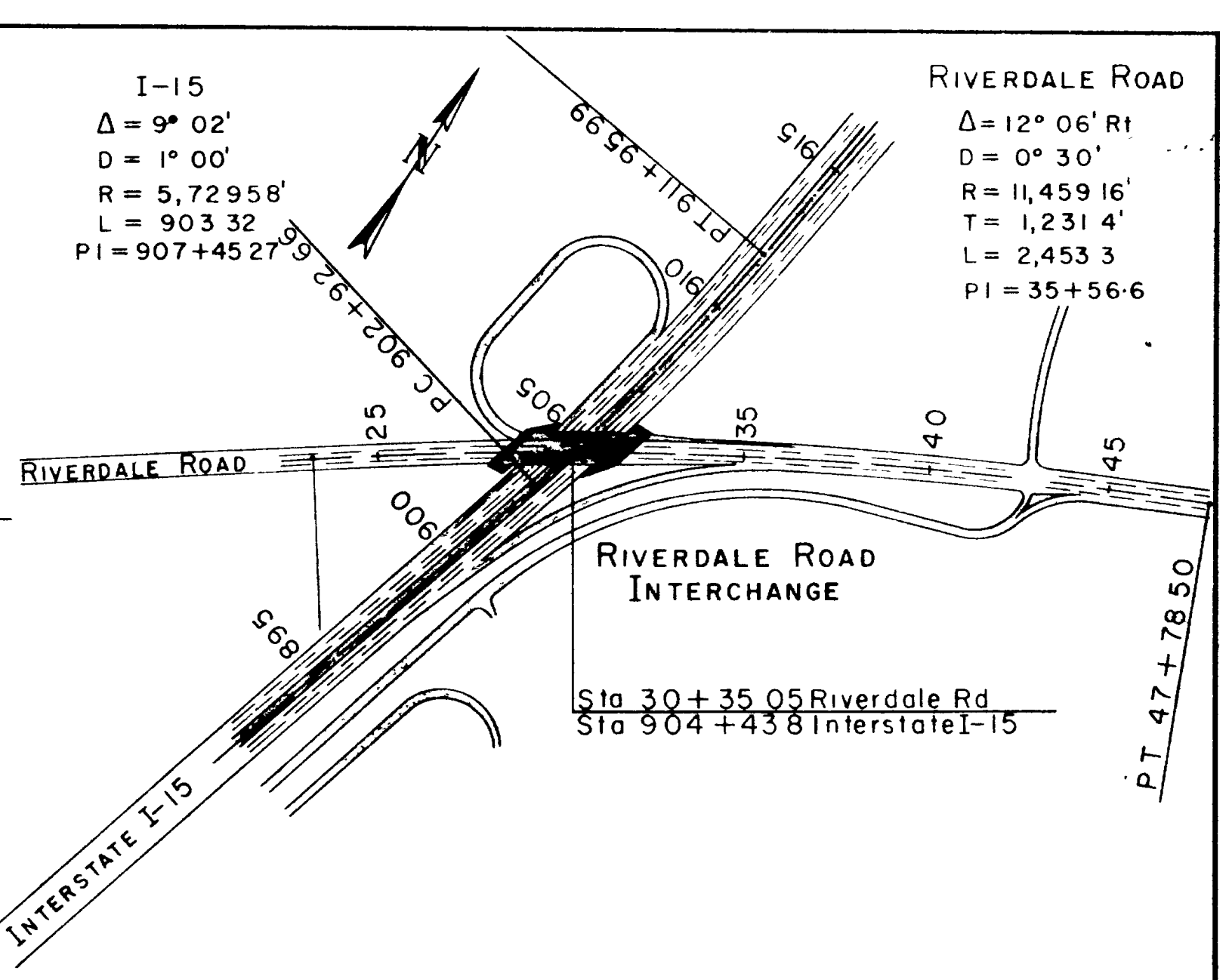
REVISIONS: DATE, REVISED BY, DATE, REVISED BY, DATE, REVISED BY, DATE, REVISED BY



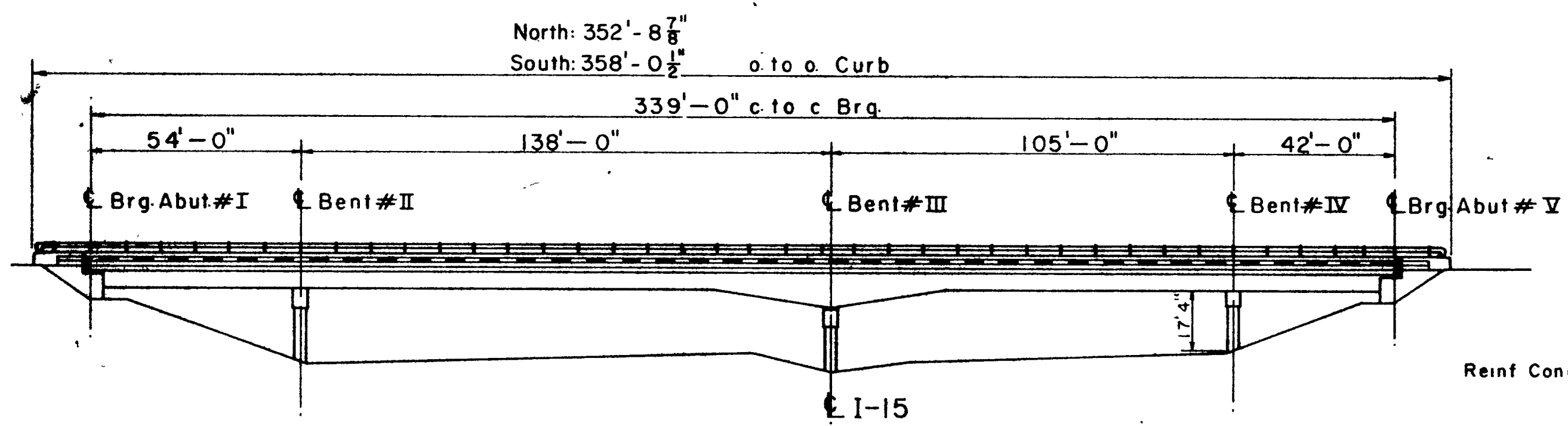
PLAN



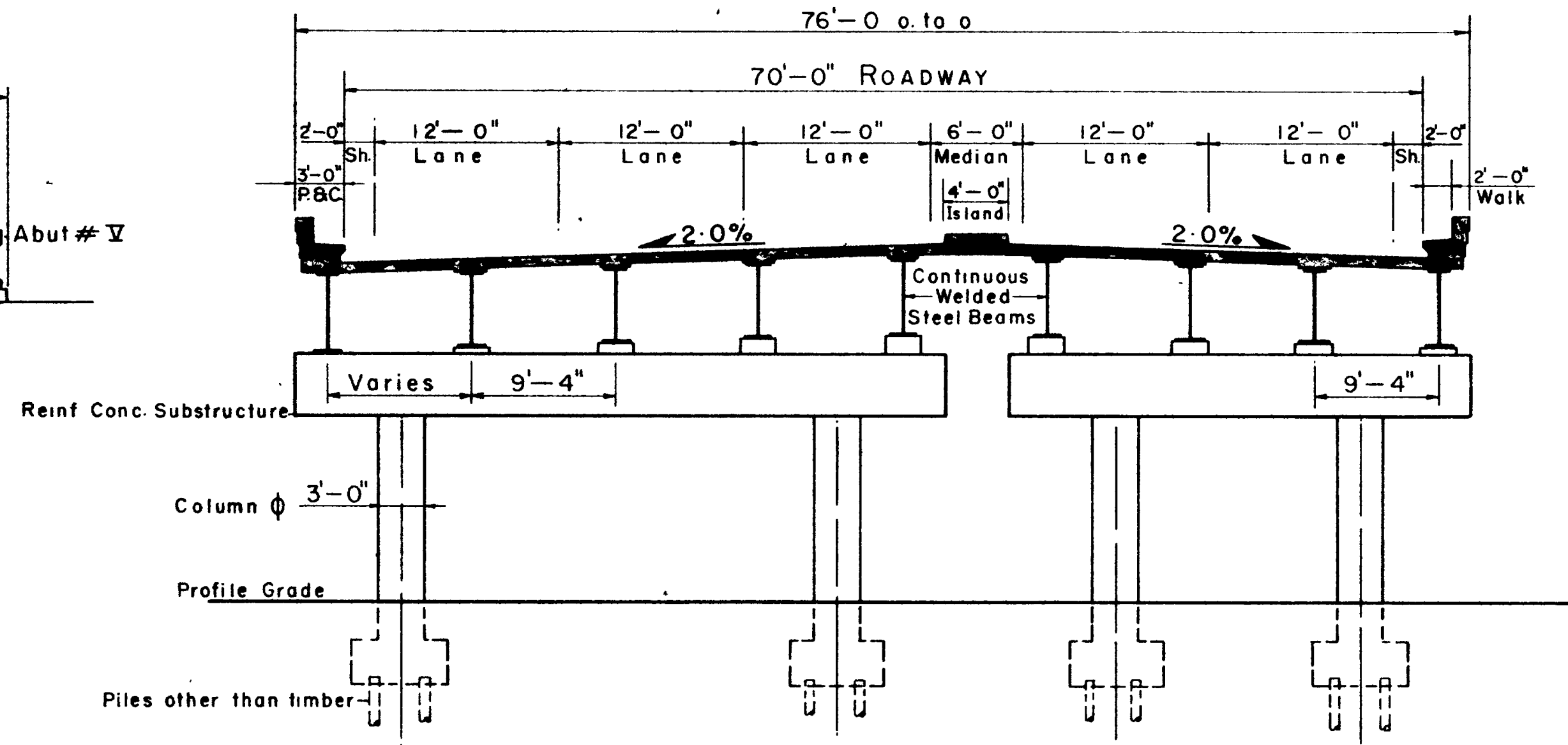
PROFILE GRADE RIVERDALE RD.
VERTICAL CURVE DATA N.B. LINE
VERTICAL CURVE DATA S.B. LINE



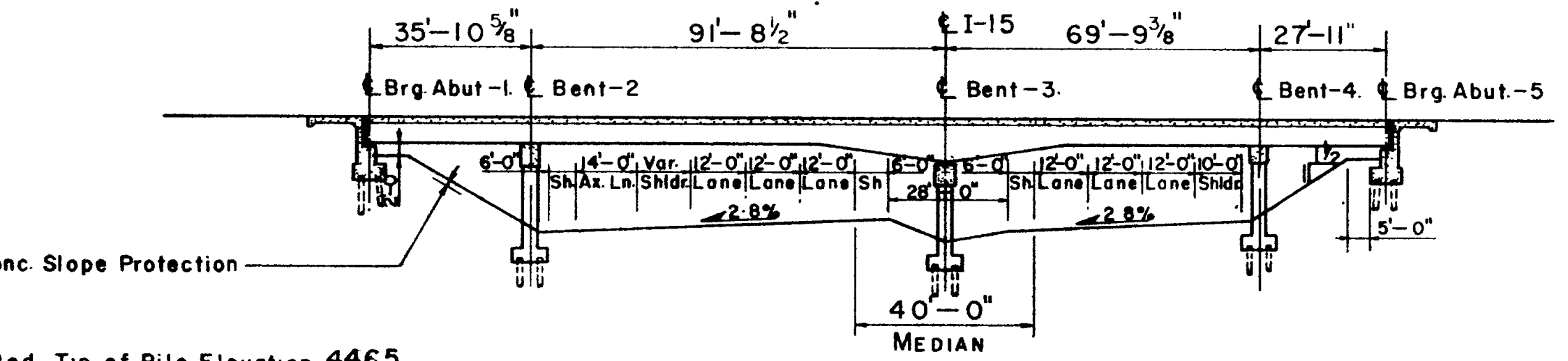
LOCATION PLAN



ELEVATION



SECTION-A-A



SECTION NORMAL TO INTERSTATE-I-15

GENERAL NOTES

- Materials, construction, and workmanship shall be in accordance with State Standard Specifications for Road and Bridge Construction, 1960 Edition and Supplements thereto which are in effect at the date of request for bids.
- All reinforcing bars shall be intermediate grade billet steel, conforming to A A S H O designation M-31. Deformations shall conform to A A S H O designation M-137.
- All structural steel shall be structural carbon steel conforming to A A S H O designation M-183 (A.S.T.M. A-36) unless noted otherwise.
- Exposed corners at joint filler and open joints shall be beveled $\frac{3}{4}$ " or rounded.

DESIGN DATA

H20-S16-44 Loading in accordance with AASHTO Specifications of 1961 and Interim Specifications
 Cast-in-place Concrete $f'_c = 3,000 \text{ p.s.i.}$, $f_s(\text{Reinf}) = 20,000 \text{ p.s.i.}$
 $f'_c = 1,200 \text{ p.s.i.}$, $n = 10$
 Structural Steel $f_s(A-36) = 20,000 \text{ p.s.i.}$, $f_s(A-441) = 27,000 \text{ p.s.i.}$
 Wearing Surface $\frac{1}{2}$ " Concrete, Future Wearing Surface 15 Lbs/Sq Ft

QUANTITIES

ITEM	ESTIMATED	UNIT	AS CONSTR
Excavation for Structure (unclass.)	705	Cu Yd.	582.8
Class A Concrete (A.E)	1344	Cu Yd.	1385.6
Reinforcing Steel	302,879	Lb.	3,432,300
Structural Steel	624,709	Lb.	6,498,300
Pile (other than timber)	4300	Lin. Ft.	3815.4
Furnishing Pile Driving Equipment	1	Lump	100%
Metal Railing (Double Rail)	710	Lin. Ft.	718.0
Mechanical Tamping	48	Hr	36.0
Reinforced Conc. Slope Protection	1,164	Sq. Yd.	1041.8
Electrical Work Bridges	1	Lump	100%
Welding anchor studs	SA # 29	EACH	97.0

INDEX OF SHEETS

- | | |
|------------------------|---|
| 1-Layout & Situation | 16-Girder Details |
| 2-Soil Data | 17-Girder Details |
| 3-Soil Data | 18-Steel Details |
| 4-Foundation Plan | 19-Steel Details |
| 5-Foundation Plan | 20-Framing Plan |
| 6-Abutment # I Details | 21-Deck Layout Plan |
| 7-Abutment # V Details | 22-Deck Details |
| 8-Bent II A Details | 23-Deck Details |
| 9-Bent II B Details | 24-Screed Elevations |
| 10-Bent III A Details | 25-North Parapet Details |
| 11-Bent III B Details | 26-South Parapet Details |
| 12-Bent IV A Details | 27-Reinforced Concrete Slope Protection |
| 13-Bent IV B Details | 28-Reinforcing Steel Schedule |
| 14-Girder Details | 29-Reinforcing Steel Schedule |
| 15-Girder Details | 29A-Lighting Plan |

STANDARD
MR-1
SP-1

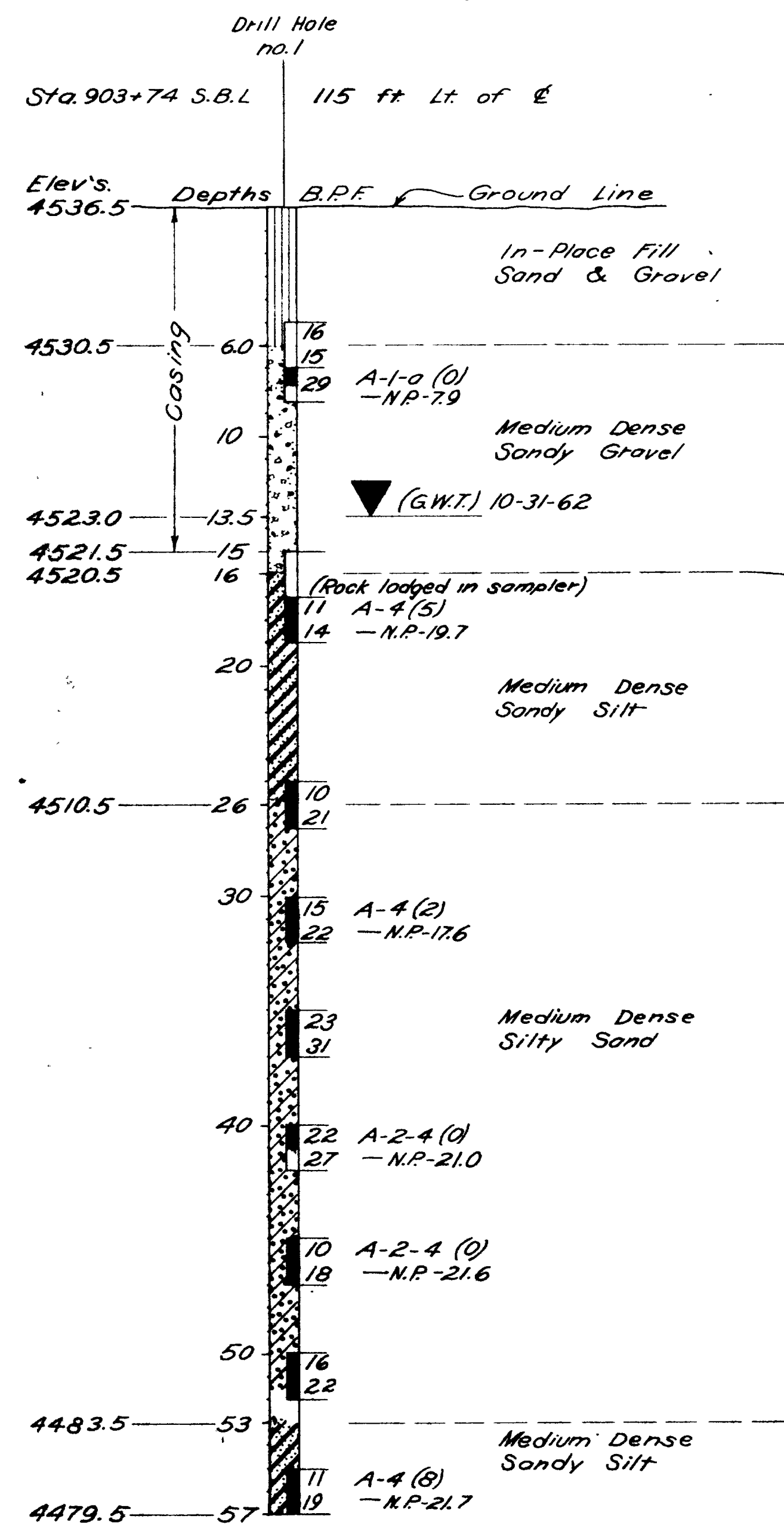
UTAH STATE DEPARTMENT OF HIGHWAYS
SALT LAKE CITY, UTAH
STRUCTURES DIVISION

DAVIS WEBER Co. LINE TO 4400 SOUTH
RIVERDALE ROAD U'PASS
LAYOUT AND SITUATION

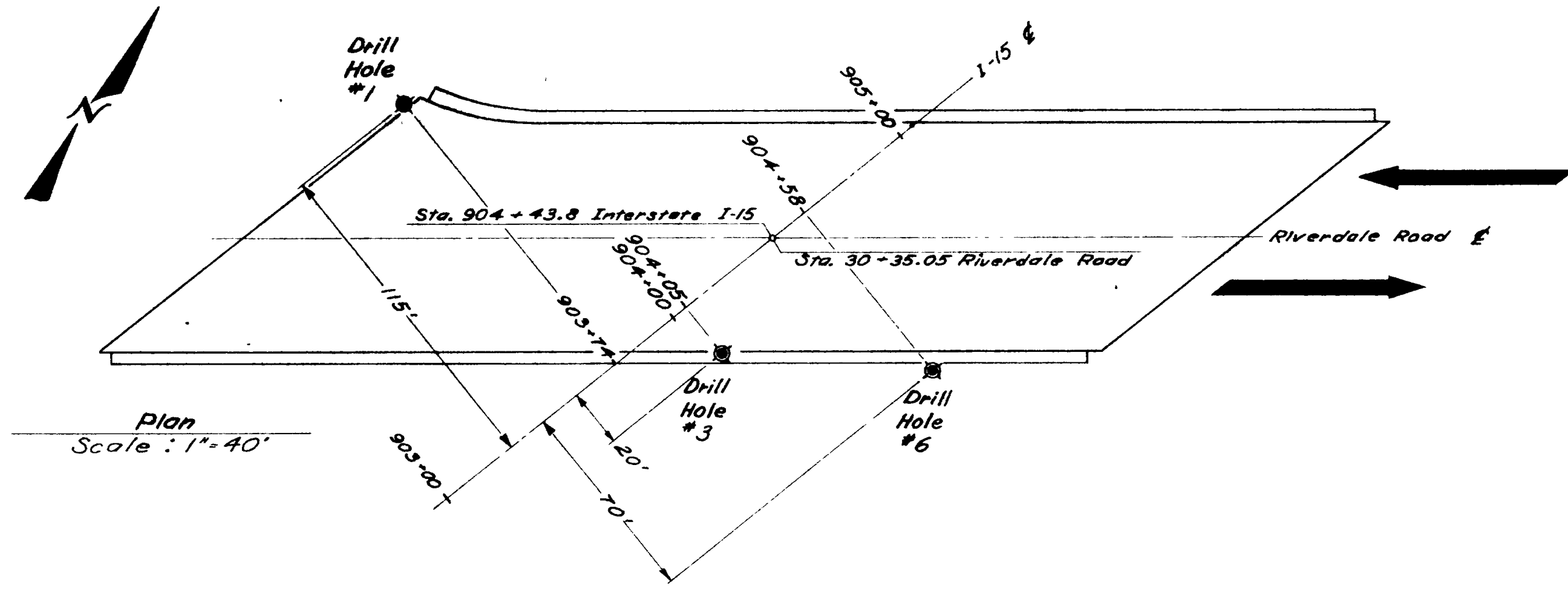
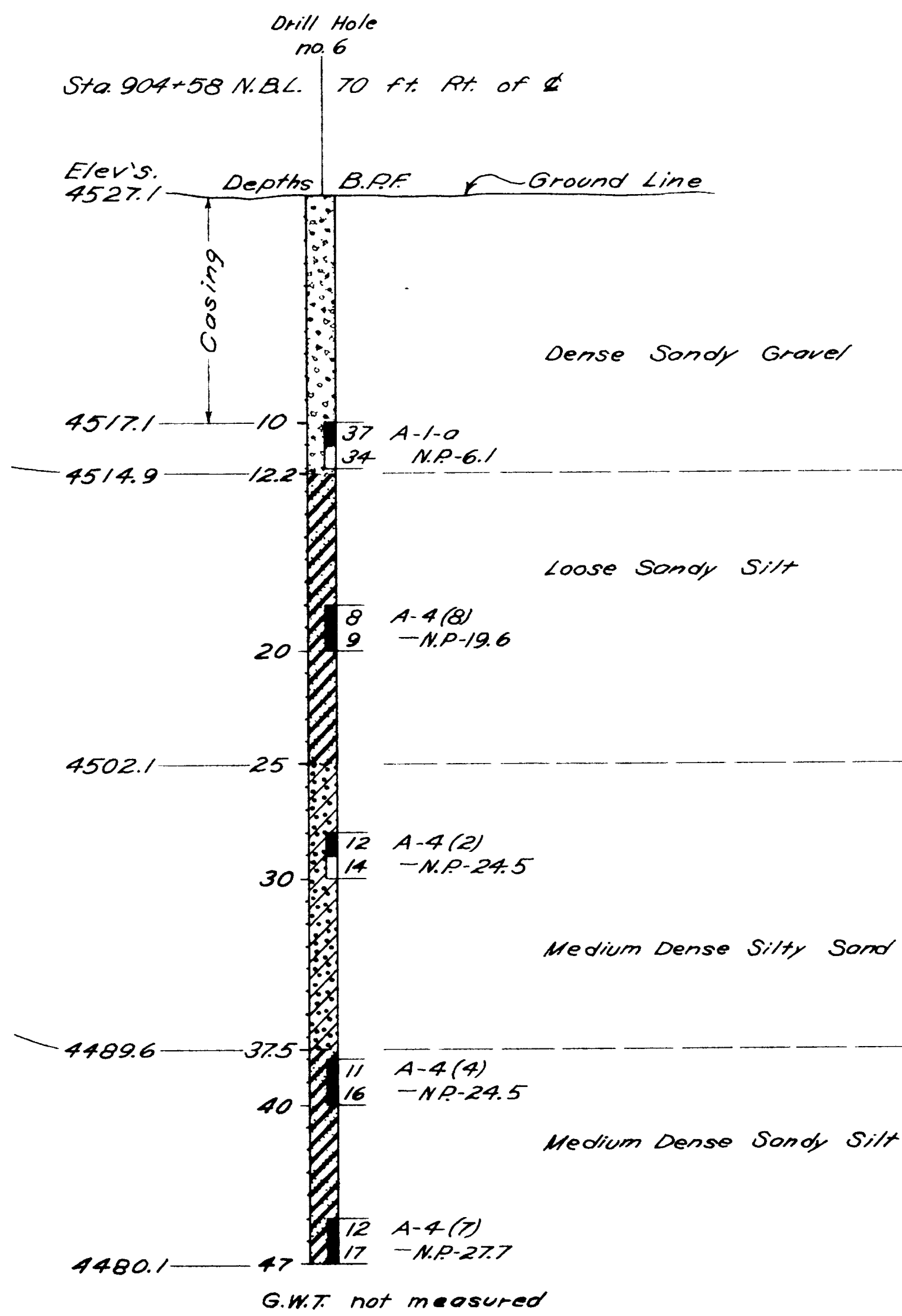
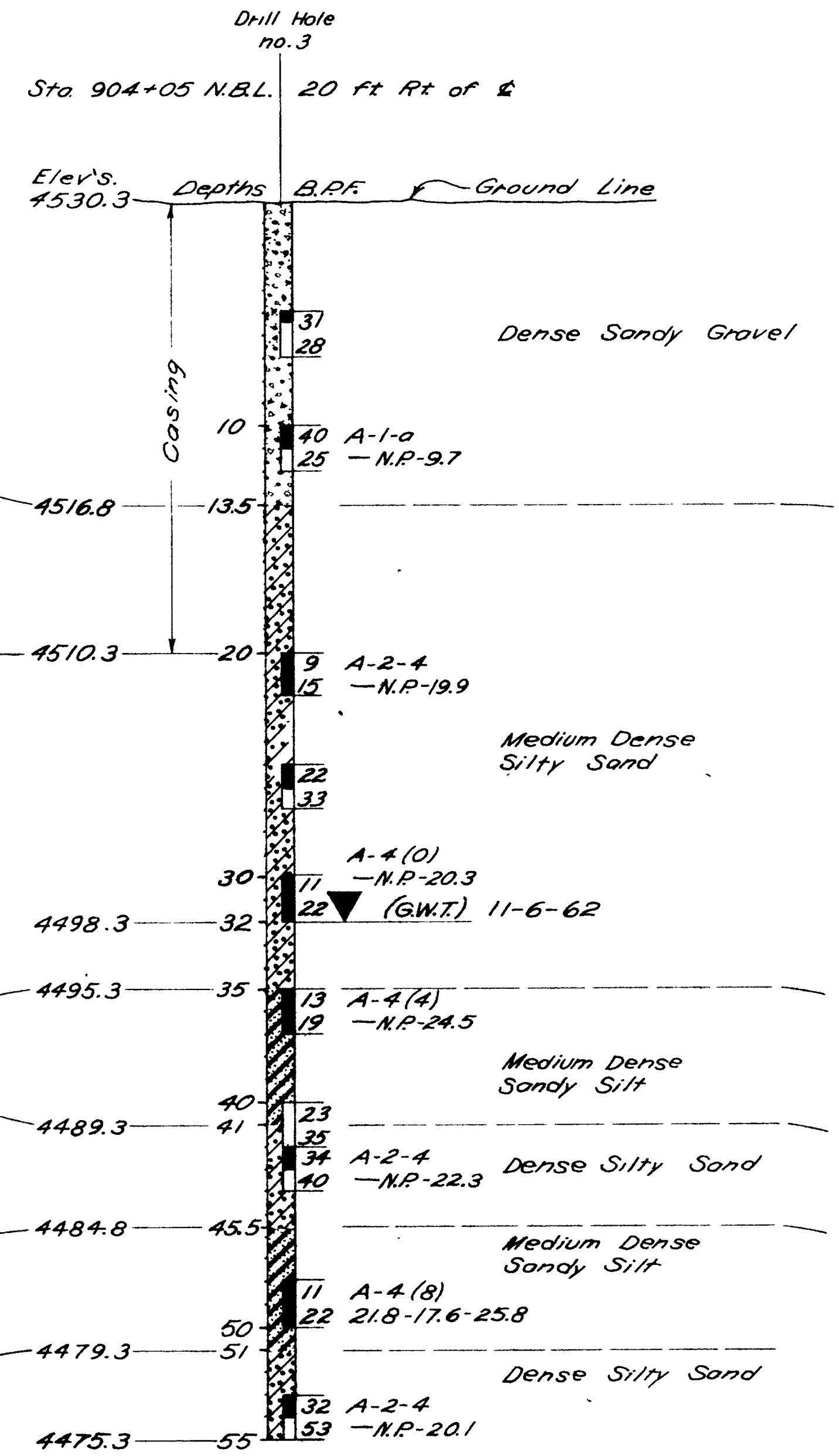
DESIGNED BY: RBJ CHECKED BY: CEY
DRAWN BY: LBL CHECKED BY: JEW
QUANTITIES BY: ACP CHECKED BY: CEY
APPROVAL RECOMMENDED BY: Joseph E. West GROUP LEADER
APPROVED: SUPERVISOR DATE: 9-20-63

PROJECT NUMBER: 1-15-8(20)334
STATION: 904+43.8
COUNTY: Weber

No	By	Date	REVISIONS
1	H G E	9-20-63	Increased Conc. & Reinf Steel Quantities



Profile
Scale: 1" = 5'



Plan
Scale: 1" = 40'

KEY TO DRILLING LOGS
RELATIVE DENSITY (SAND & SILT)
VERY LOOSE - LESS THAN 4 BLOWS PER FOOT
LOOSE - 4 TO 10 BLOWS PER FOOT
MEDIUM - 10 TO 30 BLOWS PER FOOT
DENSE - 30 TO 50 BLOWS PER FOOT
VERY DENSE - MORE THAN 50 BLOWS PER FOOT

CONSISTENCY (CLAY)
VERY SOFT - LESS THAN 2 BLOWS PER FOOT
SOFT - 2 TO 4 BLOWS PER FOOT
MEDIUM - 4 TO 8 BLOWS PER FOOT
STIFF - 8 TO 15 BLOWS PER FOOT
VERY STIFF - 15 TO 30 BLOWS PER FOOT
HARD - MORE THAN 30 BLOWS PER FOOT

TOP SOIL OR FILL	IGNEOUS	SANDY CLAY
GRAVEL	LIMESTONE	CLAYEY SAND
SAND	CONGLOMERATE	SILTY CLAY
SILT	DOLOMITE	CLAYEY SILT
CLAY		SILTY SAND
SHALE		SAND SILT

DRILL HOLE NO. 0+00 E OR LT OR RT IN FT OFFSET.

ELEVATIONS		GR EL. 4562 FT.
GROUND ELEVATION	DEPTHS	EXAMPLE TYPICAL STIFF MEDIUM PLASTIC BRN CLAY, SOME SILT
4555	2	17.2 7.2 11.1
	5	L.L. P.I. W.
GROUND WATER TABLE 4552	7	DATE
	10	5 - THIN WALL SHELBY TUBE, UNDISTURBED SAMPLER USED.
STRATA CHANGE 4546	11	
	14	R - SPLIT BARREL UNDISTURBED SAMPLER WITH LINER RINGS OR CALIFORNIA TYPE SAMPLER
LOCATION OF SAMPLE	18	
	23	
SAMPLE NOT RECOVERED	30	REASON NOT RECOVERED
BOTTOM OF HOLE 4531	30	
	30	CLASSIFICATION OF EACH SAMPLE AND RESULTS OF CLASSIFICATION TESTS.

NO. OF BLOWS OF A 140 LB HAMMER FALLING 30 INCHES REQUIRED TO DRIVE A STD. 1 3/8" I.D. 2" O.D. SAMPLE TUBE 1 FT.

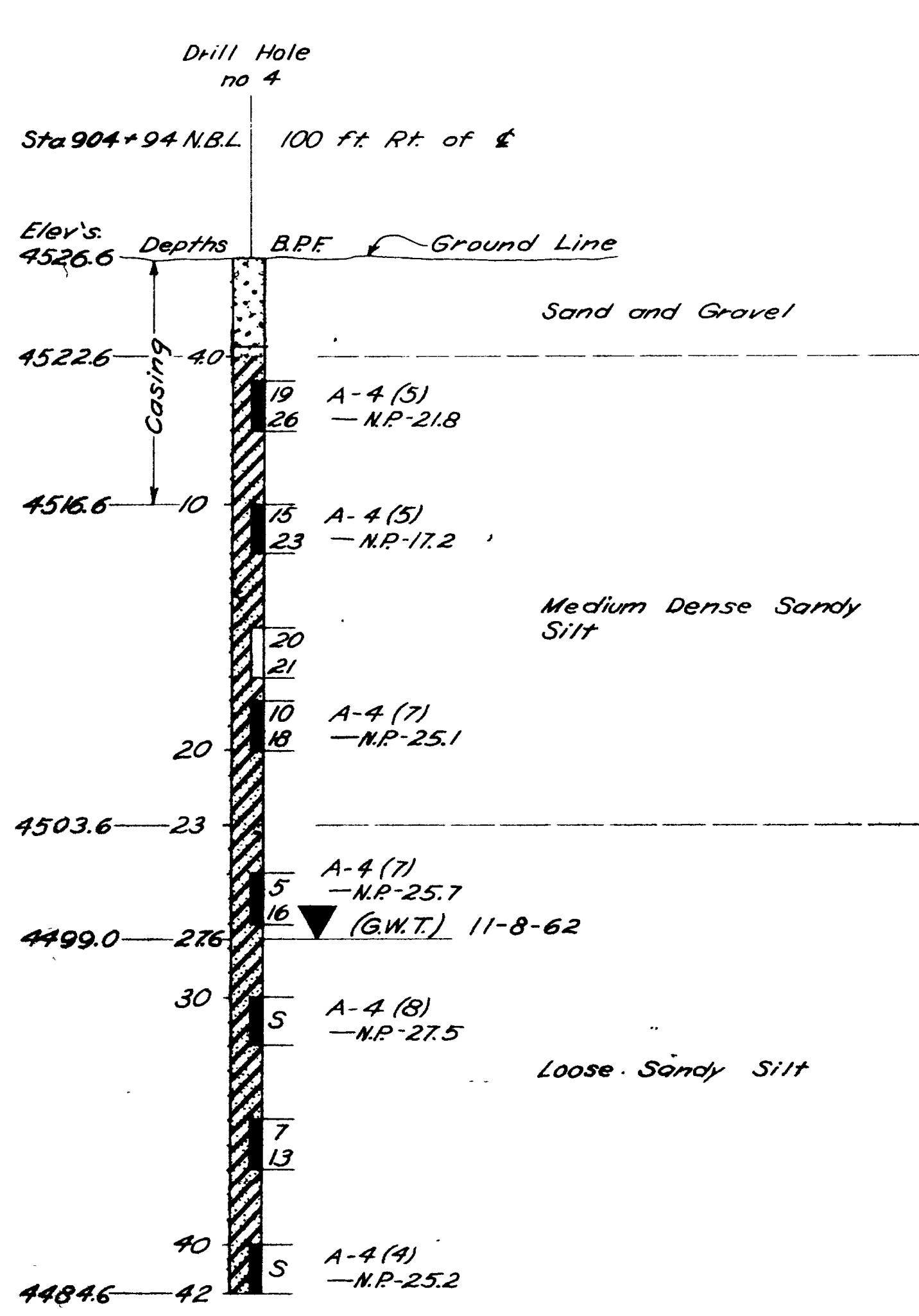
ABBREVIATIONS
L.L. - LIQUID LIMIT %
P.I. - PLASTIC INDEX %
W. - NATURAL MOISTURE CONTENT IN %
W.G. - WELL GRADED
PEN - PENETRATION
G.W.T. - GROUND WATER TABLE
B.P.F. - BLOWS PER FOOT.
N.P. - NON-PLASTIC

UTAH STATE DEPARTMENT OF HIGHWAYS
SALT LAKE CITY, UTAH
STRUCTURES DIVISION
DAVIS-WEBER CO. LINE TO 4400 SO.
RIVERDALE ROAD UPASS
SOIL DATA

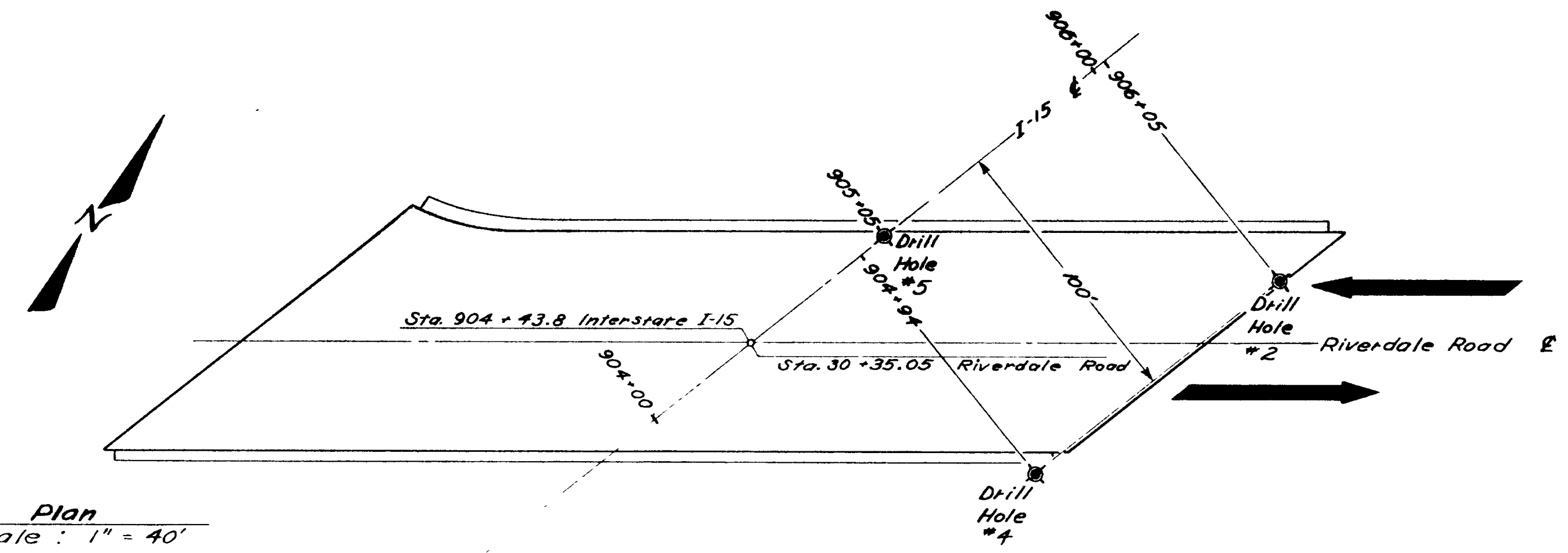
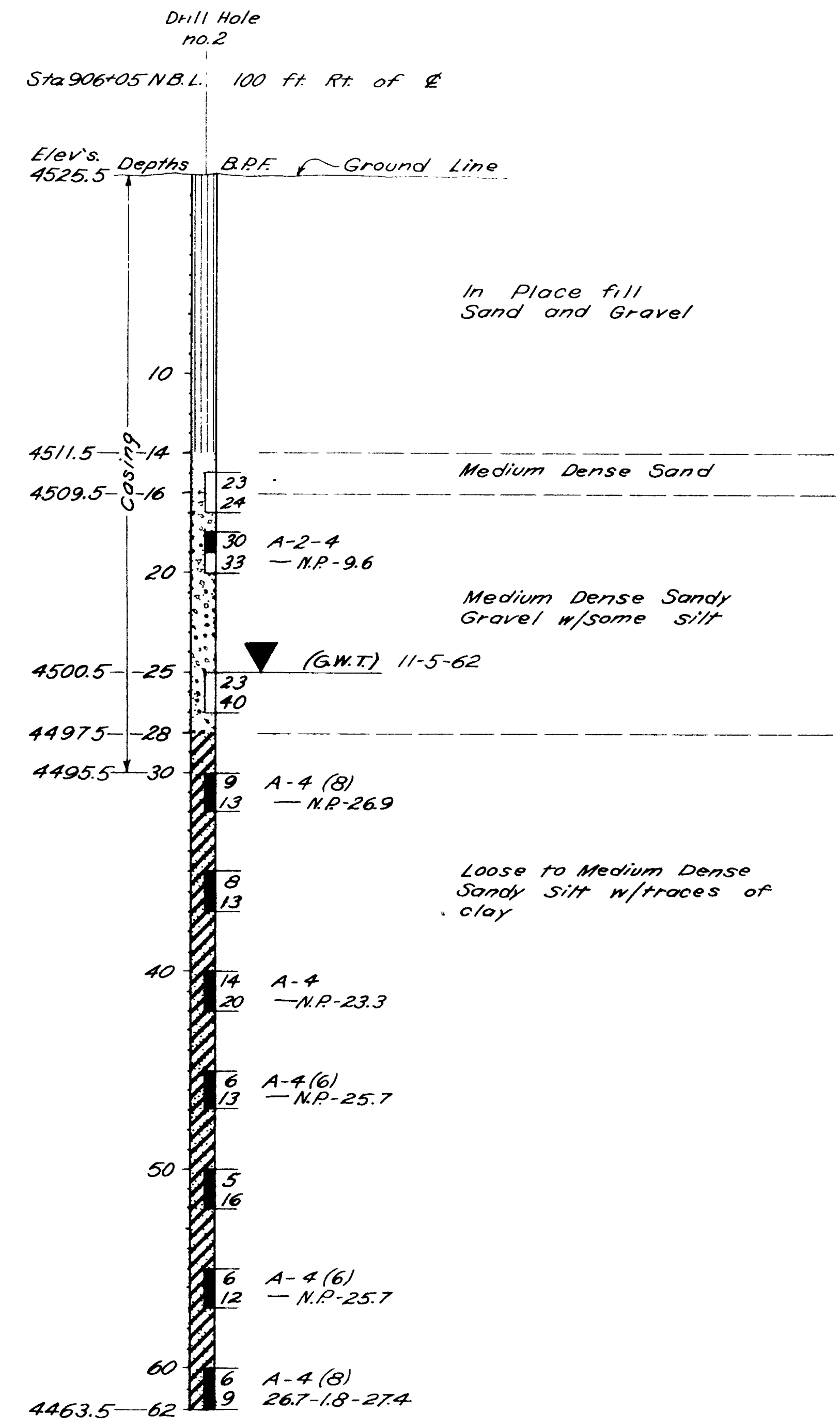
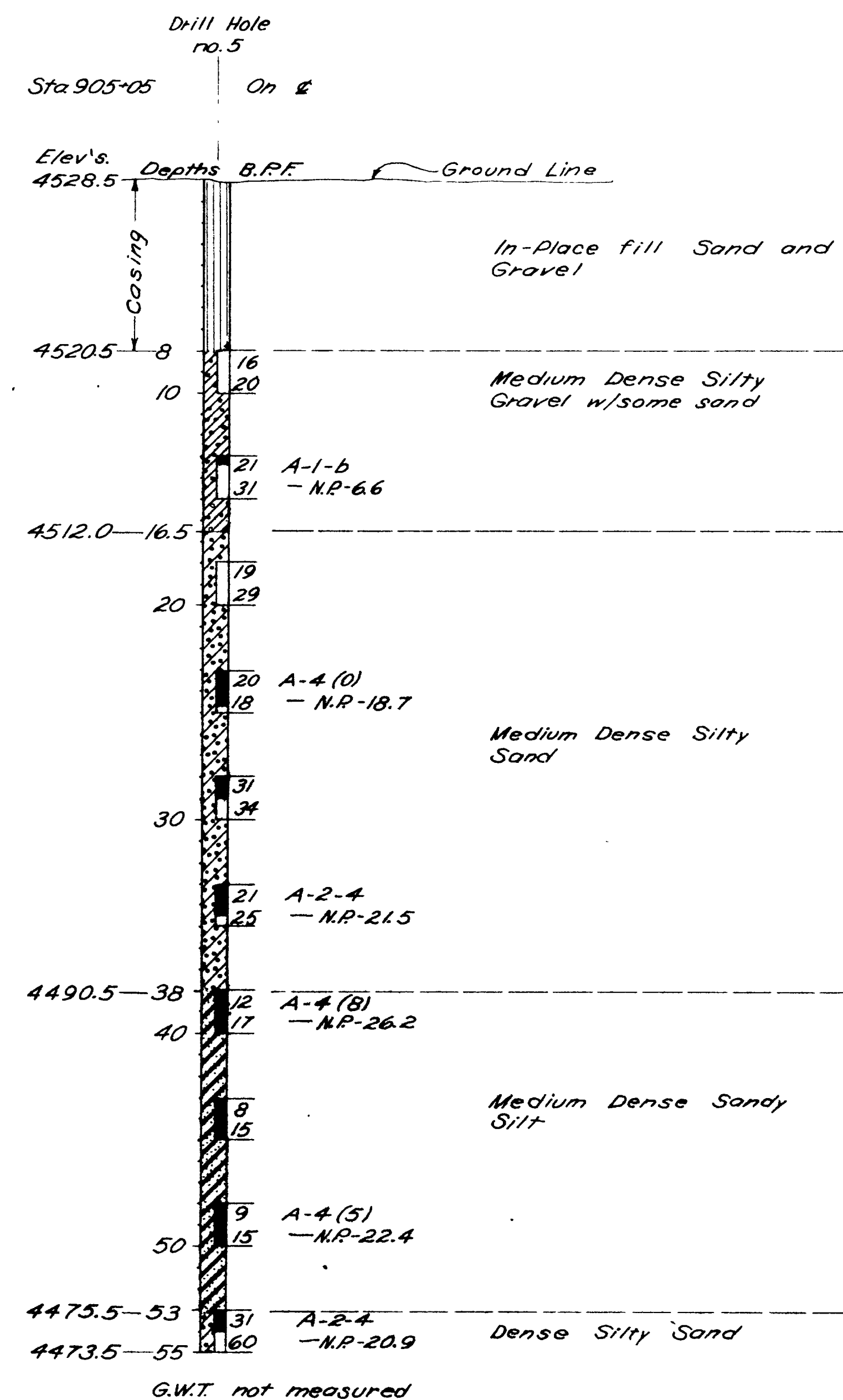
DESIGNED BY: R.B.U.	CHECKED BY: G.E.J.	PROJECT NUMBER: I-15-8(20)334
DRAWN BY: PAULSON	CHECKED BY:	STATION: 904+43.8
QUANTITIES BY:	CHECKED BY:	GROUP LEADER: WEBER
APPROVAL: Joseph E. Wolf	DATE: Sept. 10, 1965	COUNTY: WEBER

NO. BY DATE REMARKS REVISIONS

BR NO. C-453 2 OF 29



Profile
Scale: 1"=5'



KEY TO DRILLING LOGS
RELATIVE DENSITY(SAND & SILT)
VERY LOOSE - LESS THAN 4 BLOWS PER FOOT
LOOSE - 4 TO 10 BLOWS PER FOOT
MEDIUM - 10 TO 30 BLOWS PER FOOT
DENSE - 30 TO 50 BLOWS PER FOOT
VERY DENSE - MORE THAN 50 BLOWS PER FOOT

CONSISTENCY (CLAY)
VERY SOFT - LESS THAN 2 BLOWS PER FOOT
SOFT - 2 TO 4 BLOWS PER FOOT
MEDIUM - 4 TO 8 BLOWS PER FOOT
STIFF - 8 TO 15 BLOWS PER FOOT
VERY STIFF - 15 TO 30 BLOWS PER FOOT
HARD - MORE THAN 30 BLOWS PER FOOT

TOP SOIL OR FILL	IGNEOUS	SANDY CLAY
GRAVEL	LIMESTONE	CLAYEY SAND
SAND	CONGLOMERATE	SILTY CLAY
SILT	DOLOMITE	CLAYEY SILT
CLAY		SILTY SAND
SHALE		SAND SILT

DRILL HOLE NO. STATION 0+00 E OR LT OR RT IN FT. OFFSET

ELEVATIONS	DEPTHS	GR EL. 4562 FT.
GROUND ELEVATION		
4555	5	A-6(4) 17.2 7.2 11.1 L.L. P.I. W.
GROUND WATER TABLE	5	DATE
4552	7	
STRATA CHANGE	15	5-4 THIN WALL SHELBY TUBE UNDISTURBED SAMPLER USED.
4546	11	R-4 SPLIT BARREL UNDISTURBED SAMPLER WITH LINER RINGS OR CALIFORNIA TYPE SAMPLER
LOCATION OF SAMPLE	14	18
25	25	30
SAMPLE NOT RECOVERED	25	30
BOTTOM OF HOLE	4531	

NO OF BLOWS OF A 140 LB HAMMER FALLING 30 INCHES, REQUIRED TO DRIVE A STD. 1 1/2" I.D., 2" O.D. SAMPLE TUBE 1 FT.

CLASSIFICATION OF EACH SAMPLE AND RESULTS OF CLASSIFICATION TESTS.

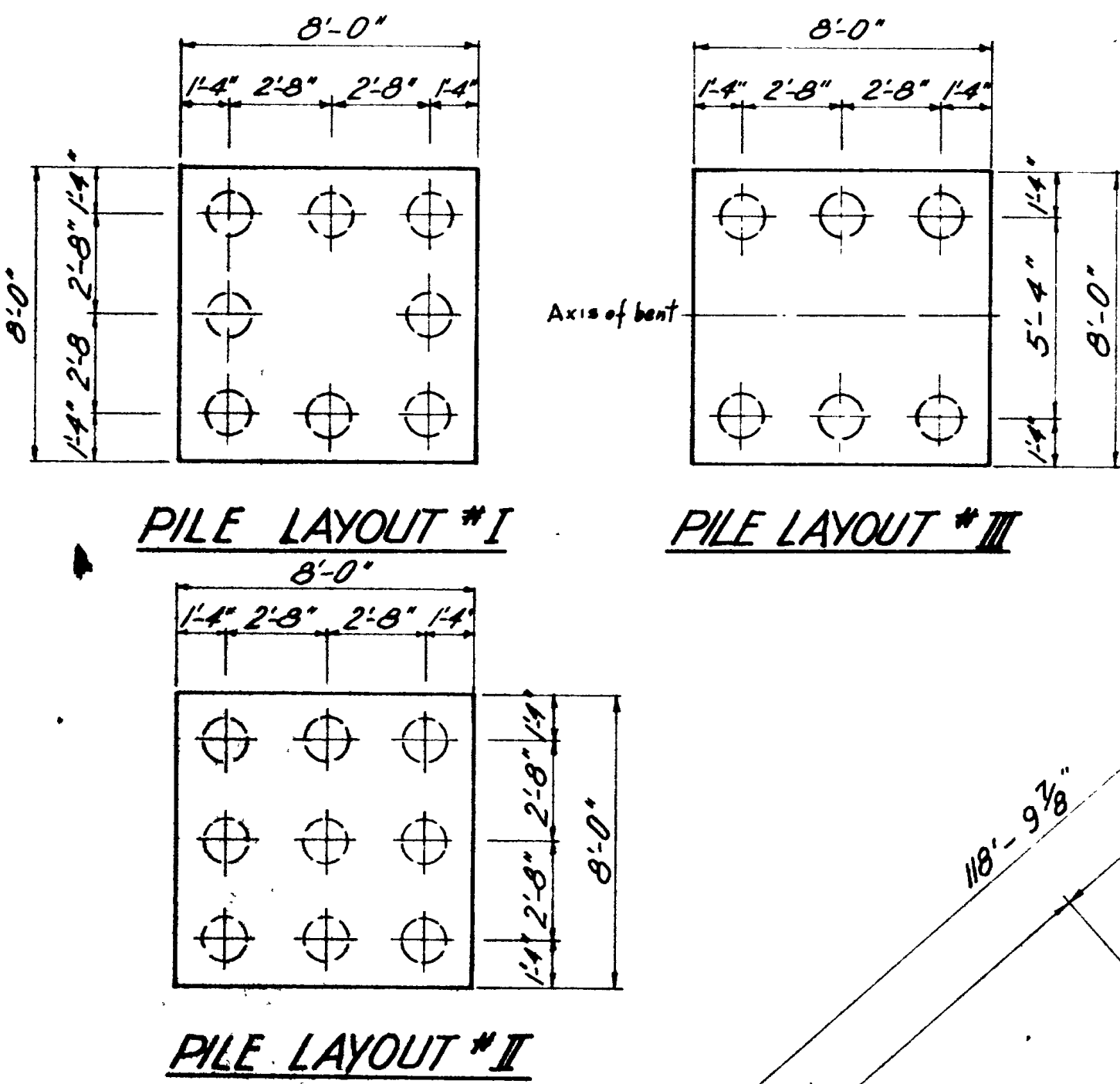
ABBREVIATIONS
L.L. - LIQUID LIMIT IN %
P.I. - PLASTIC INDEX %
W. - NATURAL MOISTURE CONTENT IN %
W.G. - WELL GRADED
PEN. - PENETRATION
G.W.T. - GROUND WATER TABLE
B.P.F. - BLOWS PER FOOT
N.P. - NON-PLASTIC

UTAH STATE DEPARTMENT OF HIGHWAYS
SALT LAKE CITY, UTAH
STRUCTURES DIVISION
DAVIS-WEBER CO. LINE TO 4400 SO.
RIVERDALE ROAD UPASS
SOIL DATA

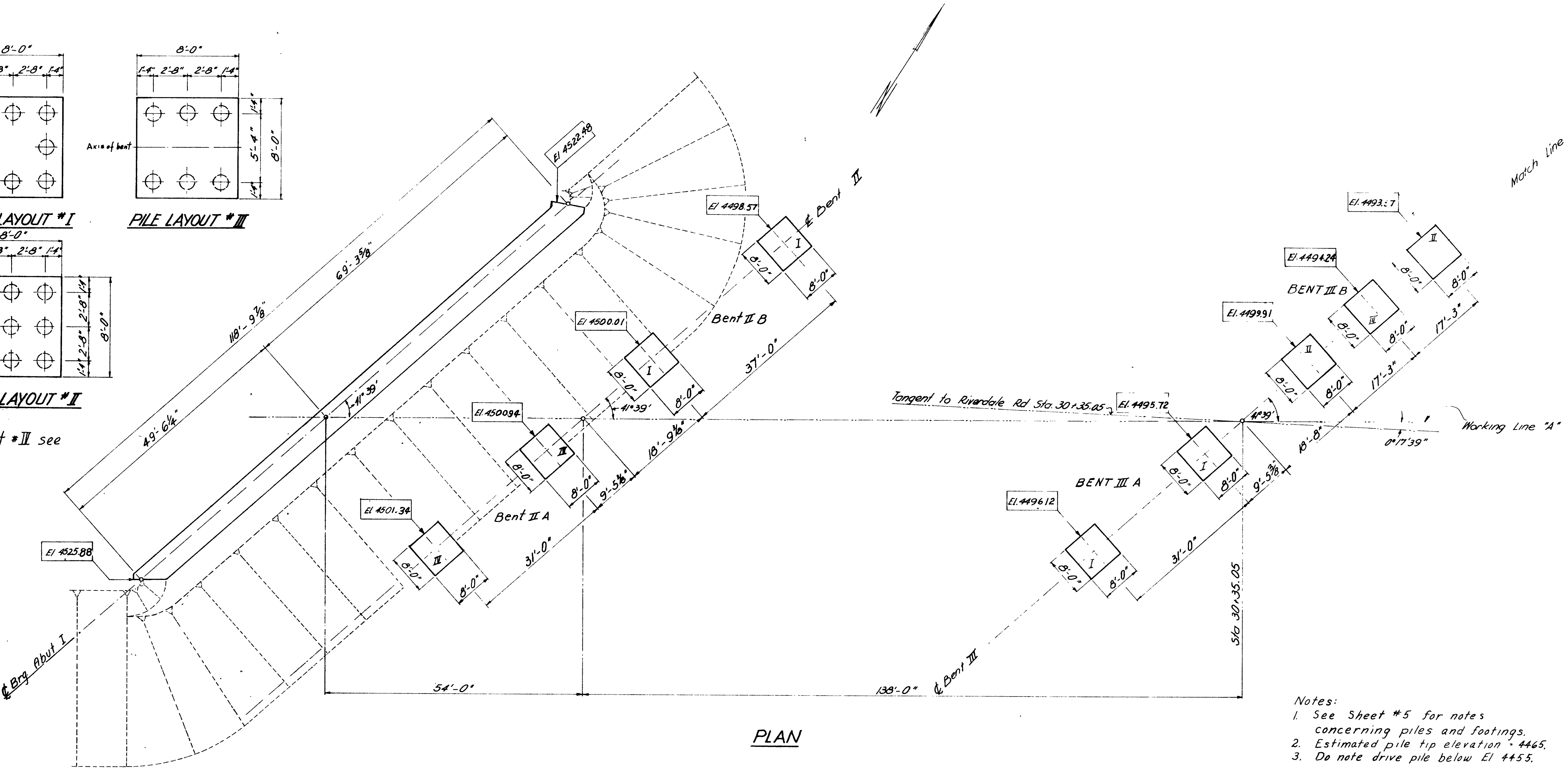
DESIGNED BY: RBV CHECKED BY: CEY I-15-8(20)334
DRAWN BY: PAULSON CHECKED BY: PROJECT NUMBER
QUANTITIES BY: CHECKED BY: STATION 904+43.8
APPROVAL RECOMMENDED BY: Joseph E. West GR. P. LEADER
APPROVED: DATE: 10/1/62 DATE: 10/1/62 COUNTY: WEBER

NO. BY DATE REMARKS REVISIONS

BR NO. DR. NO. C-453 3 OF 29

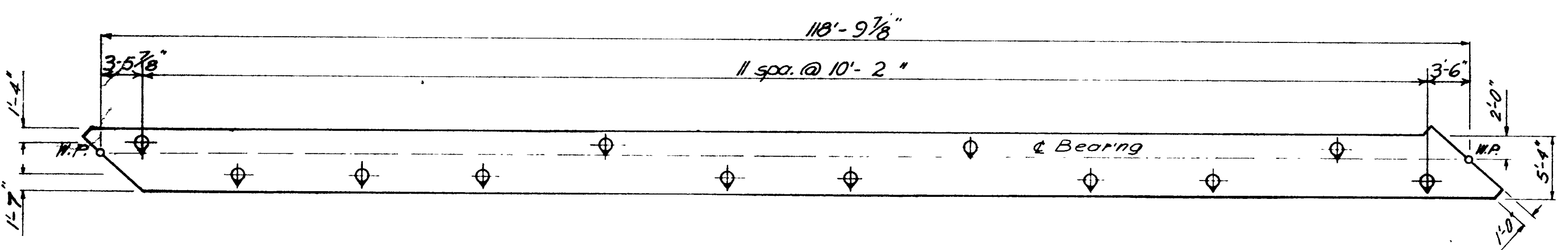


For pile layout #II see sheet 5



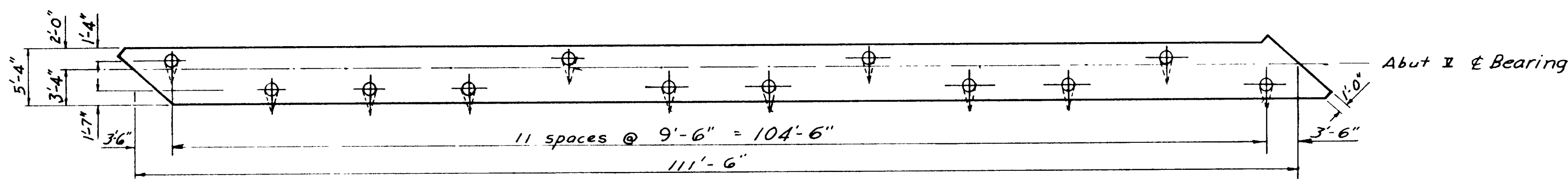
PLAN

- Notes:
1. See Sheet #5 for notes concerning piles and footings.
 2. Estimated pile tip elevation = 4465.
 3. Do note drive pile below El 4455.

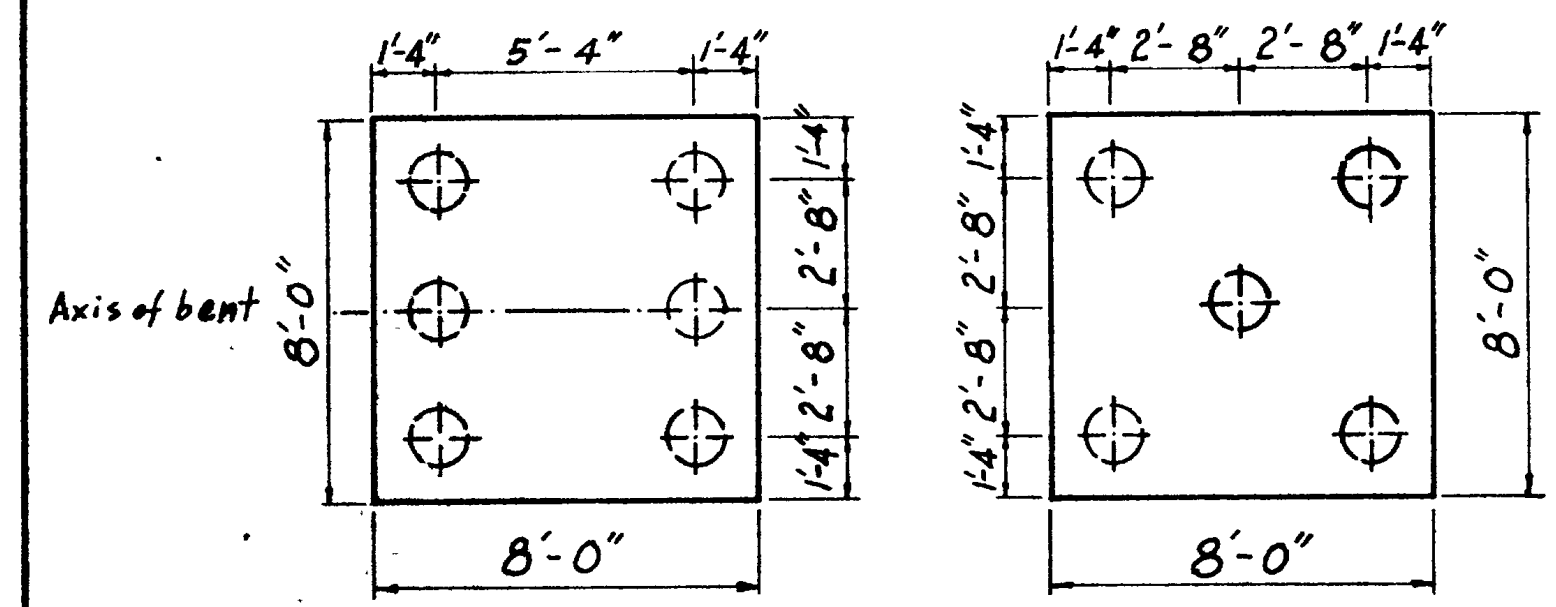


PILE LAYOUT - ABUT. I

UTAH STATE DEPARTMENT OF HIGHWAYS			
SALT LAKE CITY, UTAH			
STRUCTURES DIVISION			
DAVIS-WEBER CO. LINE TO 400 SOUTH RIVERDALE ROAD U-PASS			
FOUNDATION PLAN			
DESIGNED BY RBJ	CHECKED BY CEY	1-15-8(20)334	
DRAWN BY ACP	CHECKED BY JEW	SHEET NUMBER	
QUANTITIES BY ACP	CHECKED BY JEW	90A+43.3	
APPROVED 7/19/63	IN CHARGE Joseph B. West	WEBER	
APPROVED Sept. 16 1963	DATE	DATE	
NO.	BY	DATE	REMARKS
REVISIONS			
BR NO.	DRG. NO.	C-453	4 OF 29



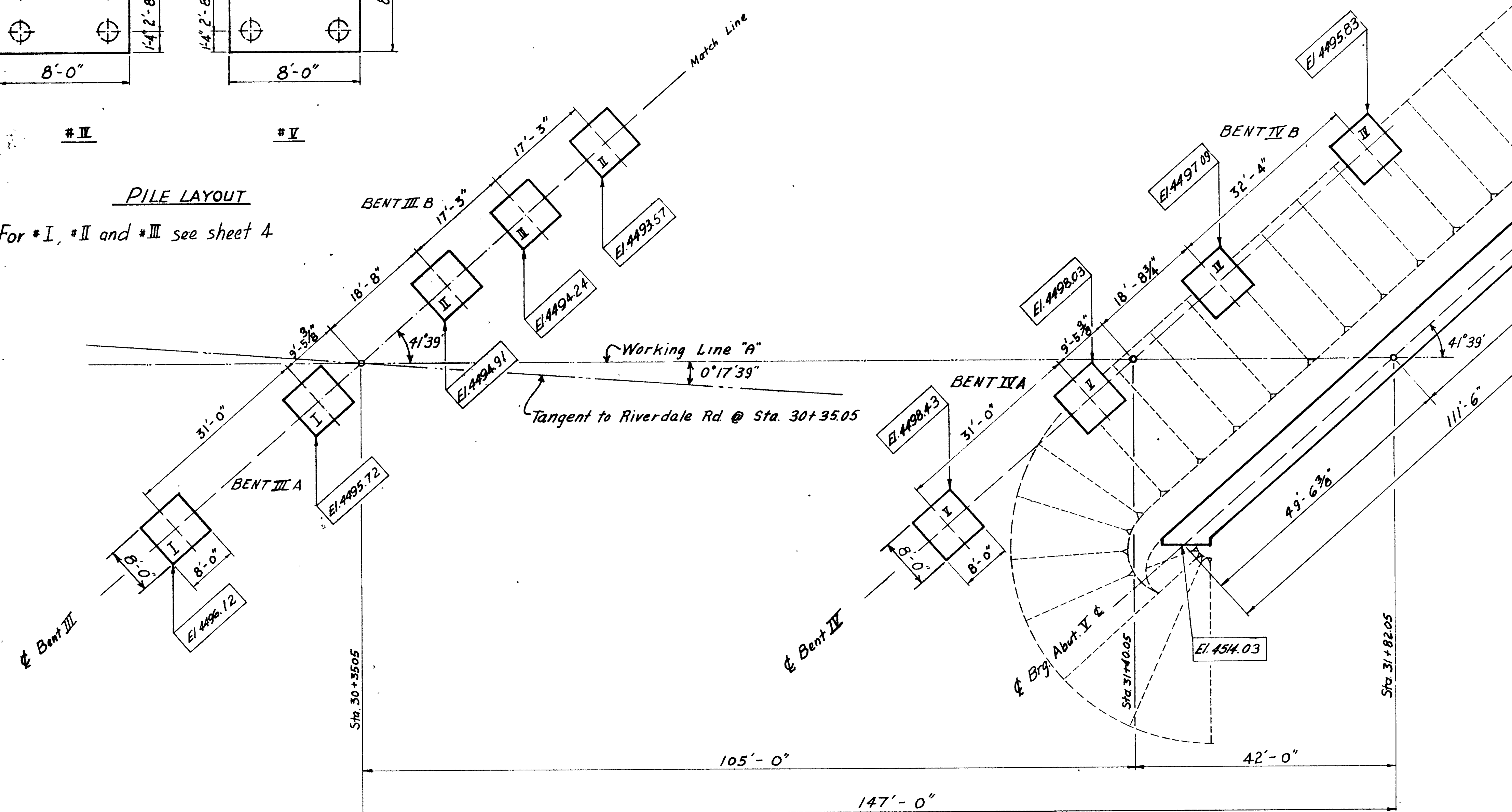
ABUT. I PILE LAYOUT



#II #V

PILE LAYOUT

For *I, *II and *III see sheet 4



NOTES

- 1-Pile design load is 50 tons - D.L. + L.L.
- 2-See drawing S-P-1 for piles (other than timber) details
- 3-Piles shown thus are to be battered 1:4 in the direction indicated unless otherwise shown.
- 4-Pile elevations given are approximate only and will be verified at time of construction in accordance with the specifications.
- 5-Elevations at bottom of footing are shown enclosed in rectangles. e.g. El. 4514.03
- 6-A representative of the Engineer of Structural Design shall be present during pile driving operations
- 7-Estimated pile tip elevation = 4465.
- 8-Do not drive pile below El. 4455

COMPUTED LOAD (Kip)

Loading	ABUTMENTS								Loading	BENTS					
	I				V					II		III		IV	
	Rear	Front	Rear	Front	Plumb	Plumb	Plumb	Plumb		Plumb	Plumb	Plumb	Plumb		
	Batter	Batter	Batter	Batter	V	H	V	H		V	H	V	H		
D.L.E.	25	34	74	34	25	34	74	34	D.L. only	51	0	59	1	43	1
Group I	96	34	88	34	96	34	88	34	Group I	77	2	81	2	74	4
Group III	77	27	70	27	77	27	70	27	Group III	82	3	92	4	78	5

PLAN

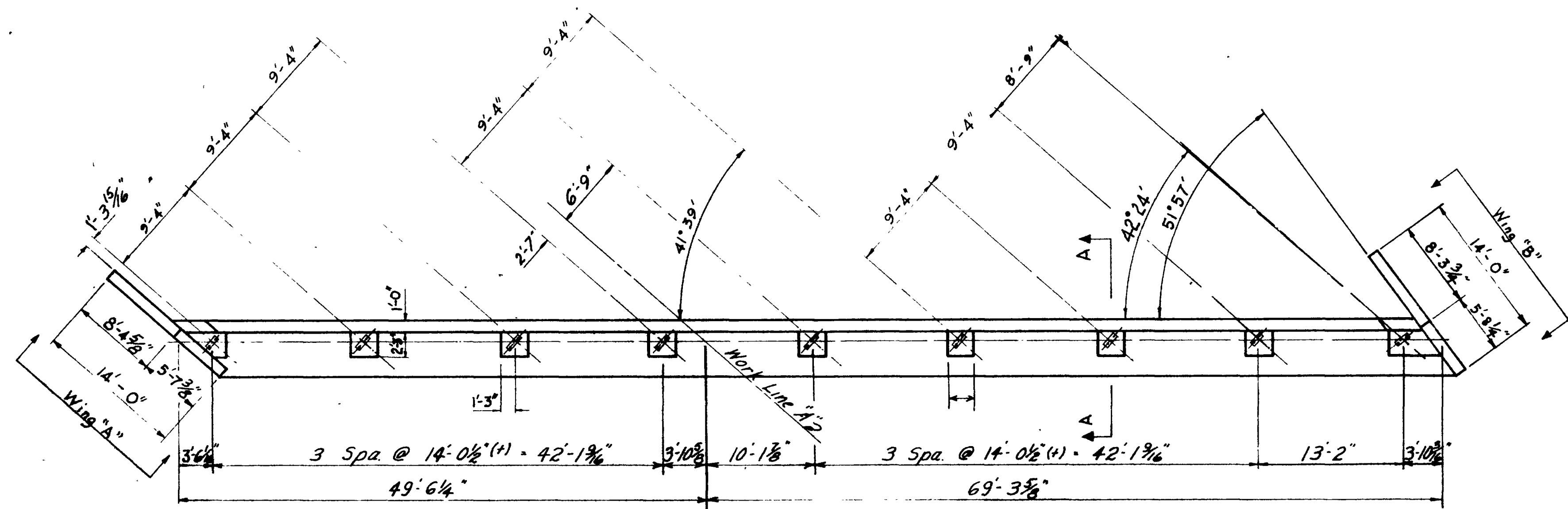
UTAH STATE DEPARTMENT OF HIGHWAYS
SALT LAKE CITY, UTAH
STRUCTURES DIVISION
DAVIS-WEBER CO. LINE TO 4400 SO.
RIVERDALE ROAD U'PASS
FOUNDATION PLAN

DESIGNED BY: A.B.J.	CHECKED BY: CEY	PROJECT NUMBER: 1-15-8-(20)334
DRAWN BY: C.E.Y.	CHECKED BY: PFM	STATION: 904+43.8
QUANTITIES BY: CEY	CHECKED BY: RCP	COUNTY: WEBER
APPROVAL: 7/19/63 Joseph E. West	DATE: 7/19/63	COUNTY: WEBER

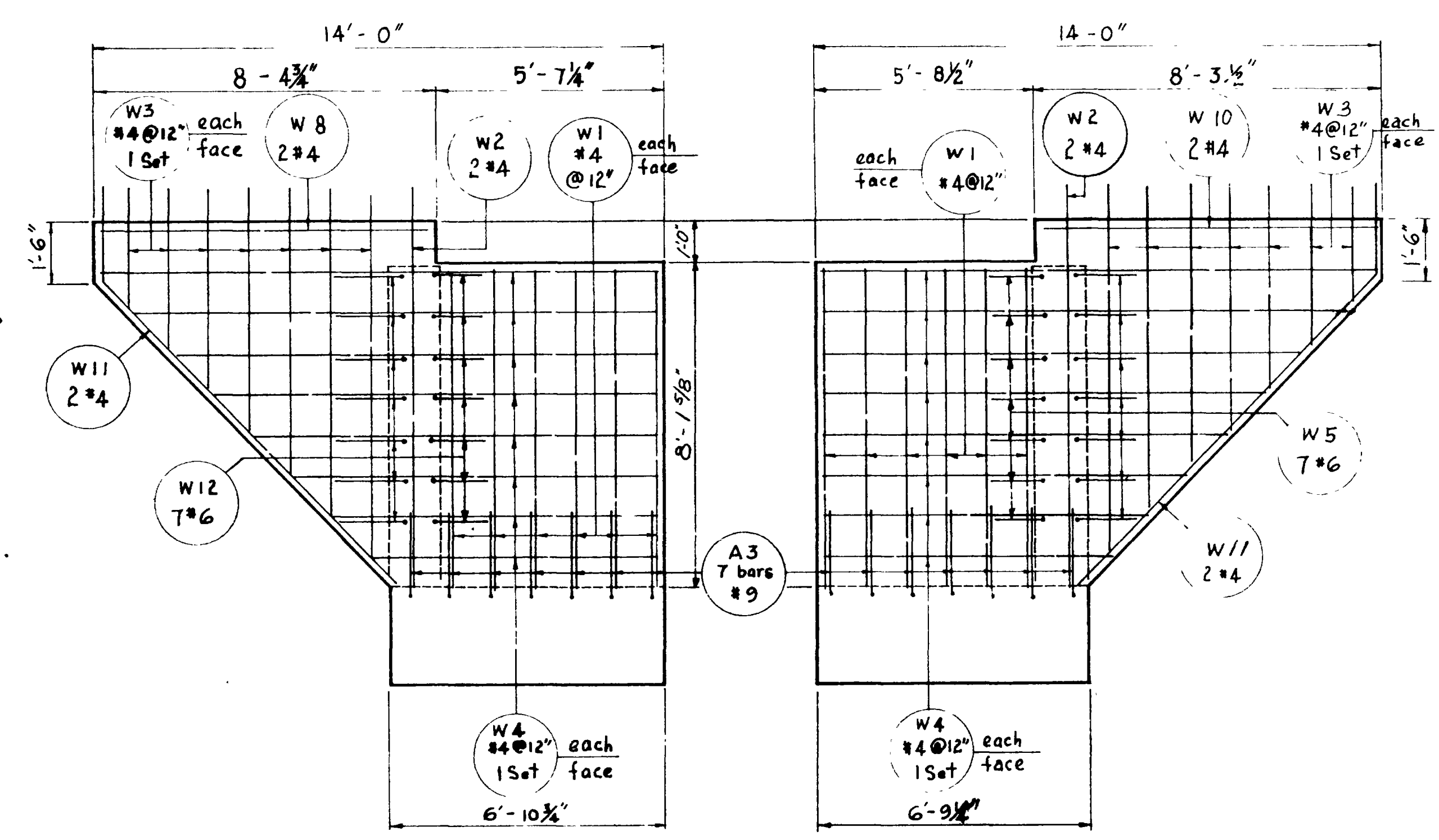
NO. BY DATE REMARKS

DRG. NO. C-453 5 OF 29

REVISOR BY DATE
 REVISOR BY DATE
 REVISOR BY DATE

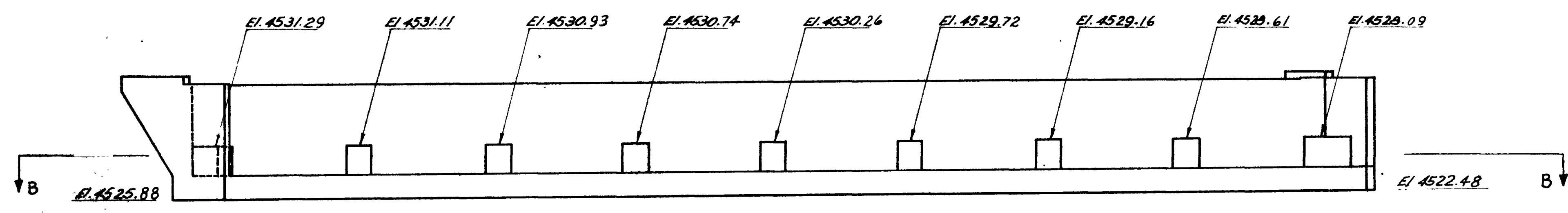


ABUTMENT PLAN

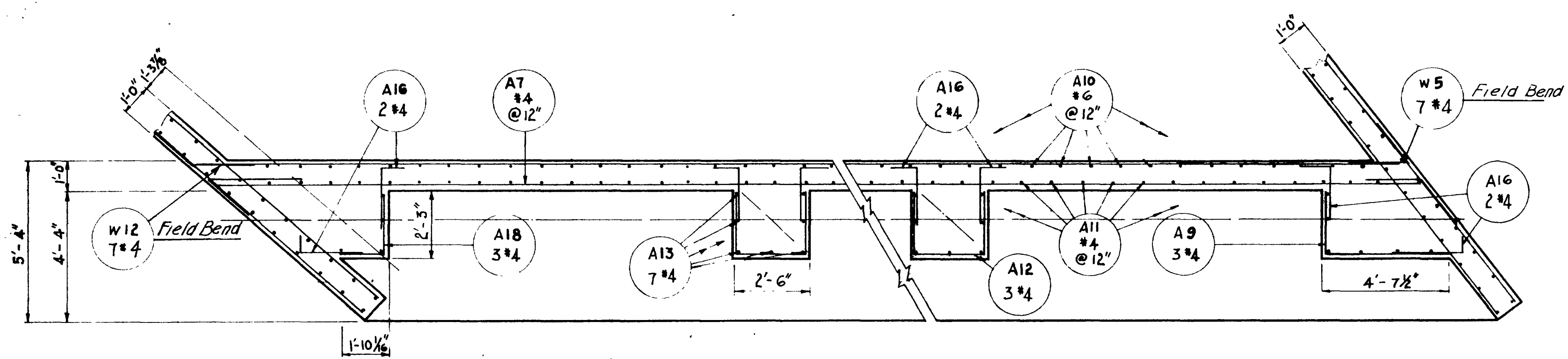


WING "A"

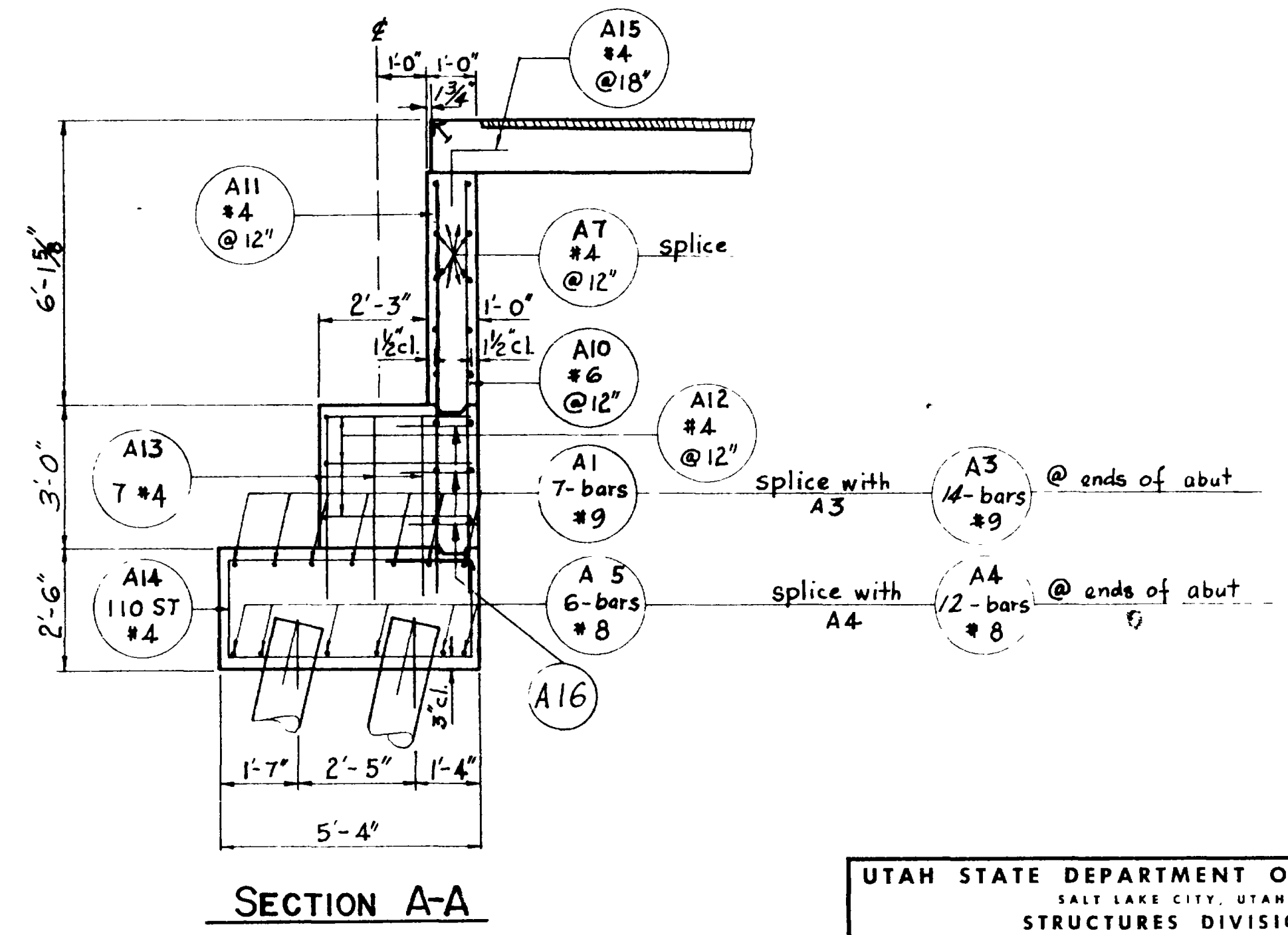
WING "B"



ELEVATION ABUTMENT No. 1



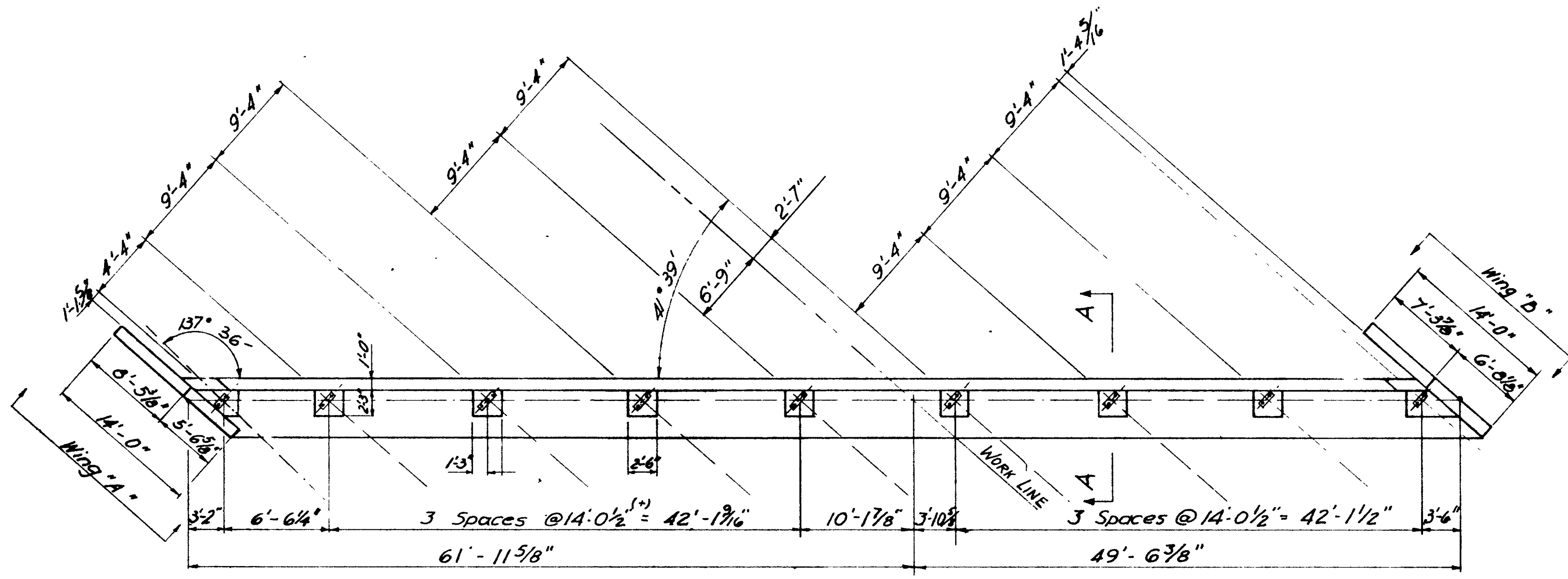
PARTIAL SECTION B-B



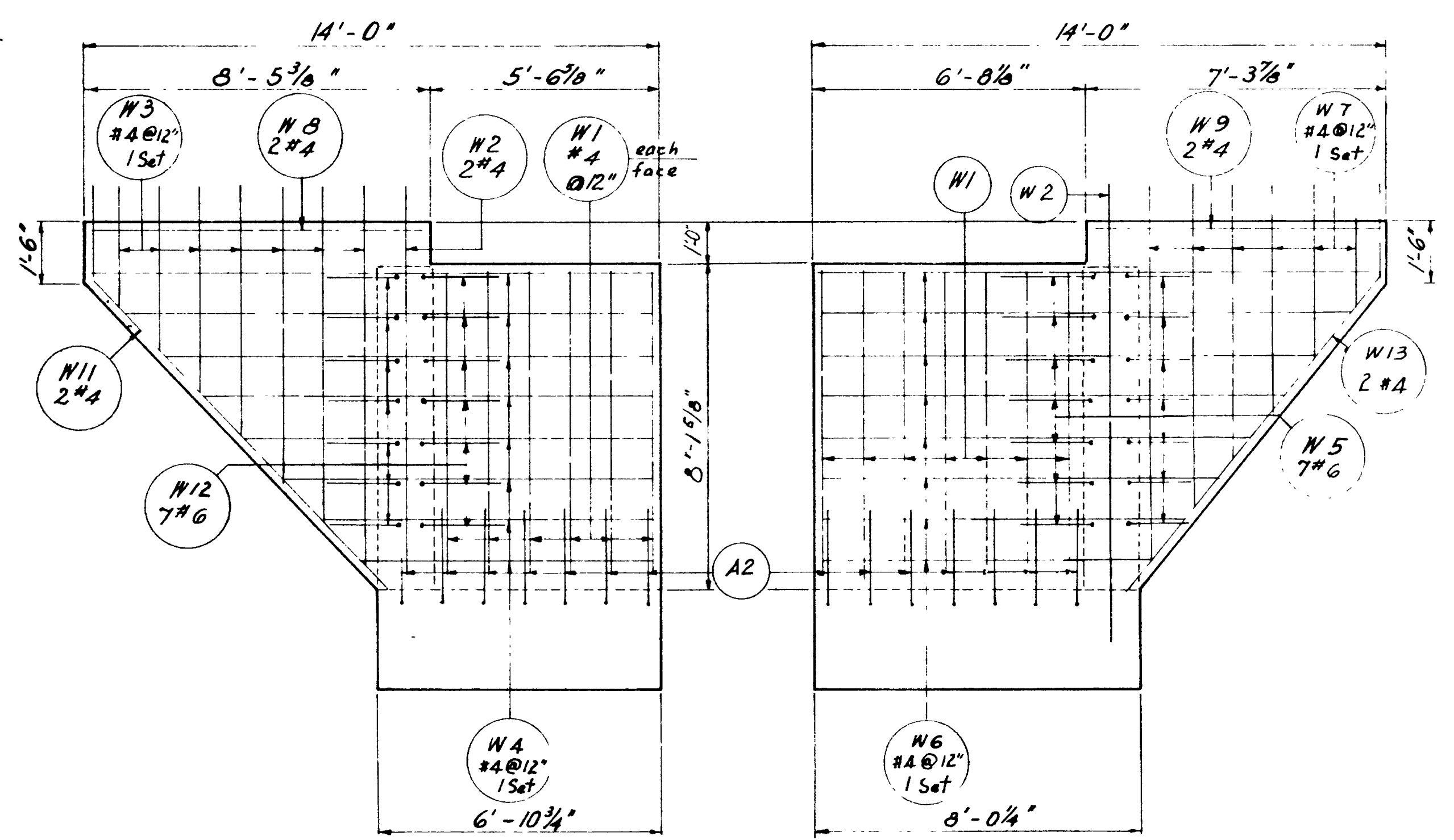
SECTION A-A

UTAH STATE DEPARTMENT OF HIGHWAYS			
SALT LAKE CITY, UTAH			
STRUCTURES DIVISION			
DAVIS, WEBER CO. LINE TO 4400 SOUTH			
RIVERDALE ROAD U'PASS			
ABUTMENT No. 1 DETAILS			
DESIGNED BY R. B. J.	CHECKED BY C. E. Y.	PROJECT NUMBER	15-807334
DRAWN BY C. E. Y.	CHECKED BY A. C. P.	STATION	904+43.8
QUANTITIES BY B. C. P.	CHECKED BY C. E. Y.	GROUP LEADER	WEBER
APPROVED BY Joseph E. West	DATE	DATE	COUNTY
BR NO.	DRG NO. C-453	6 of 29	

REVISOR: DATE
 REVISOR: DATE
 REVISOR: DATE

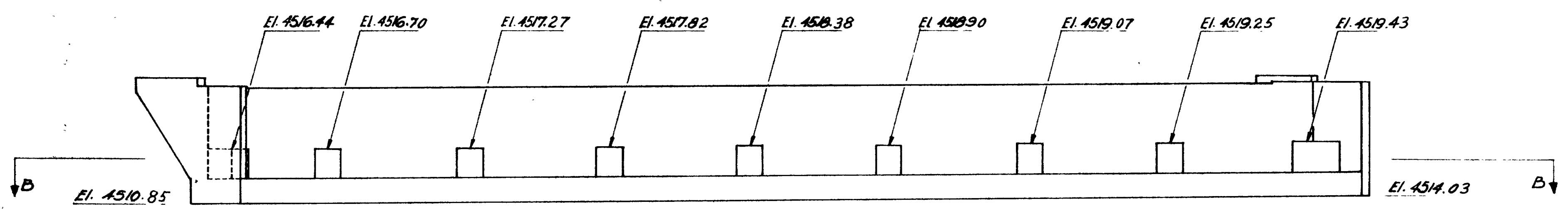


ABUTMENT PLAN

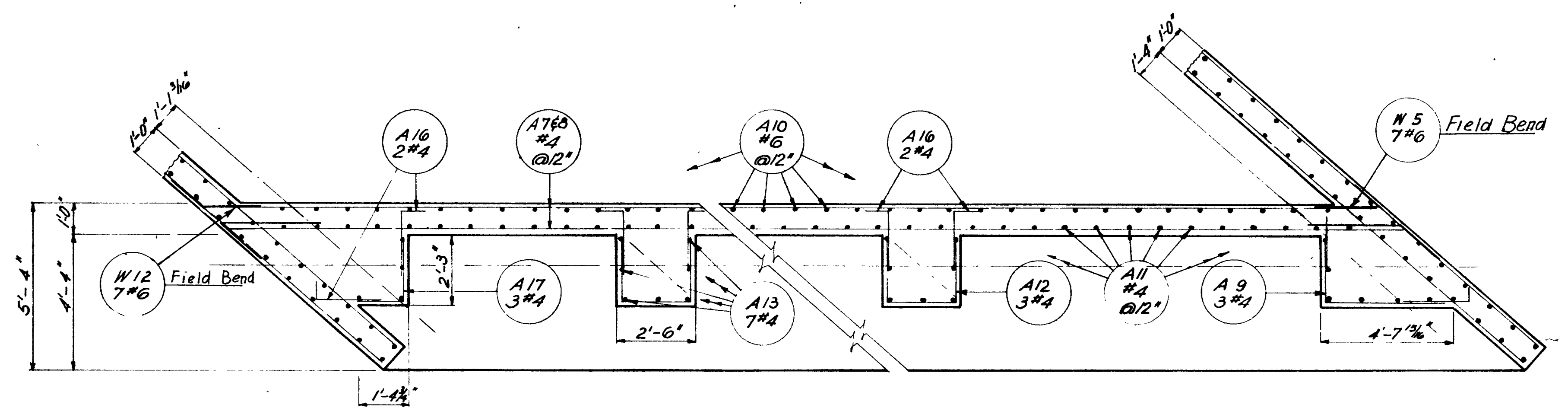


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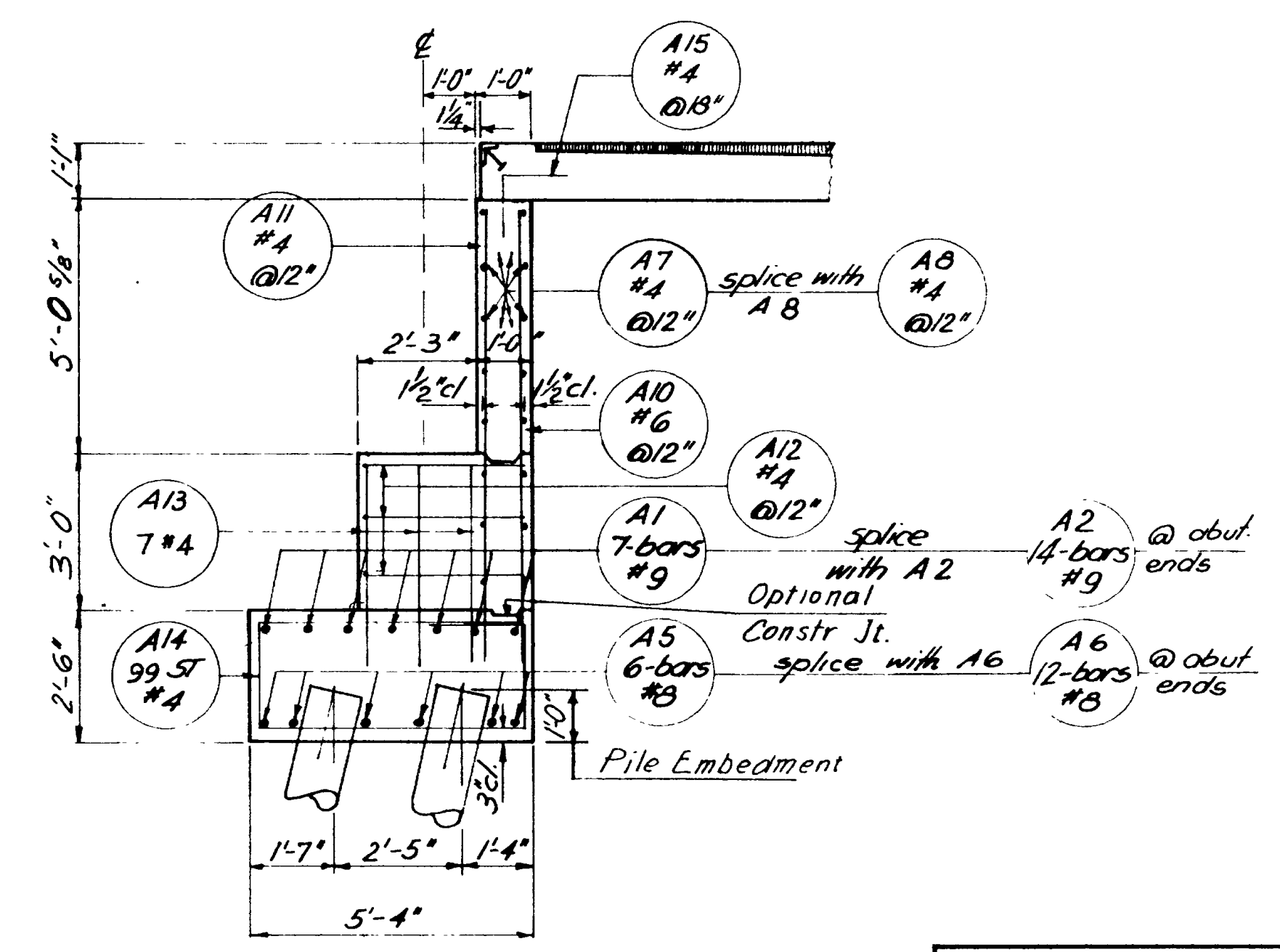
WING 'B'



ELEVATION ABUTMENT NO. I

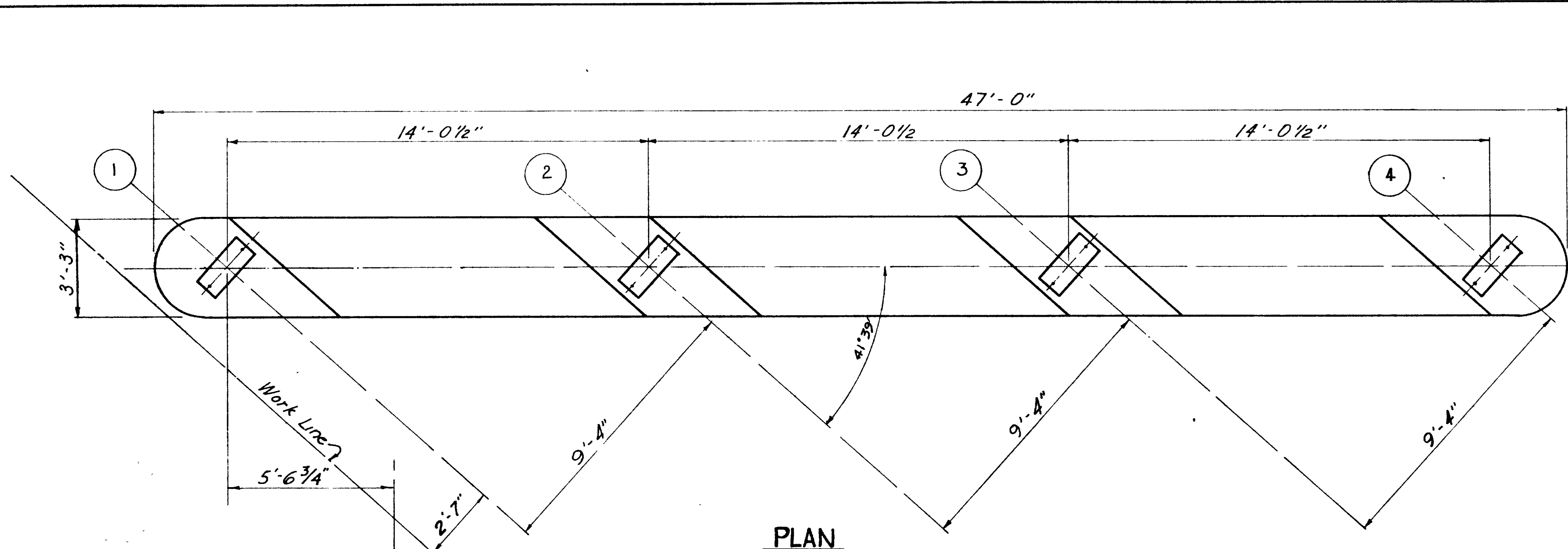


PARTIAL SECTION B-B

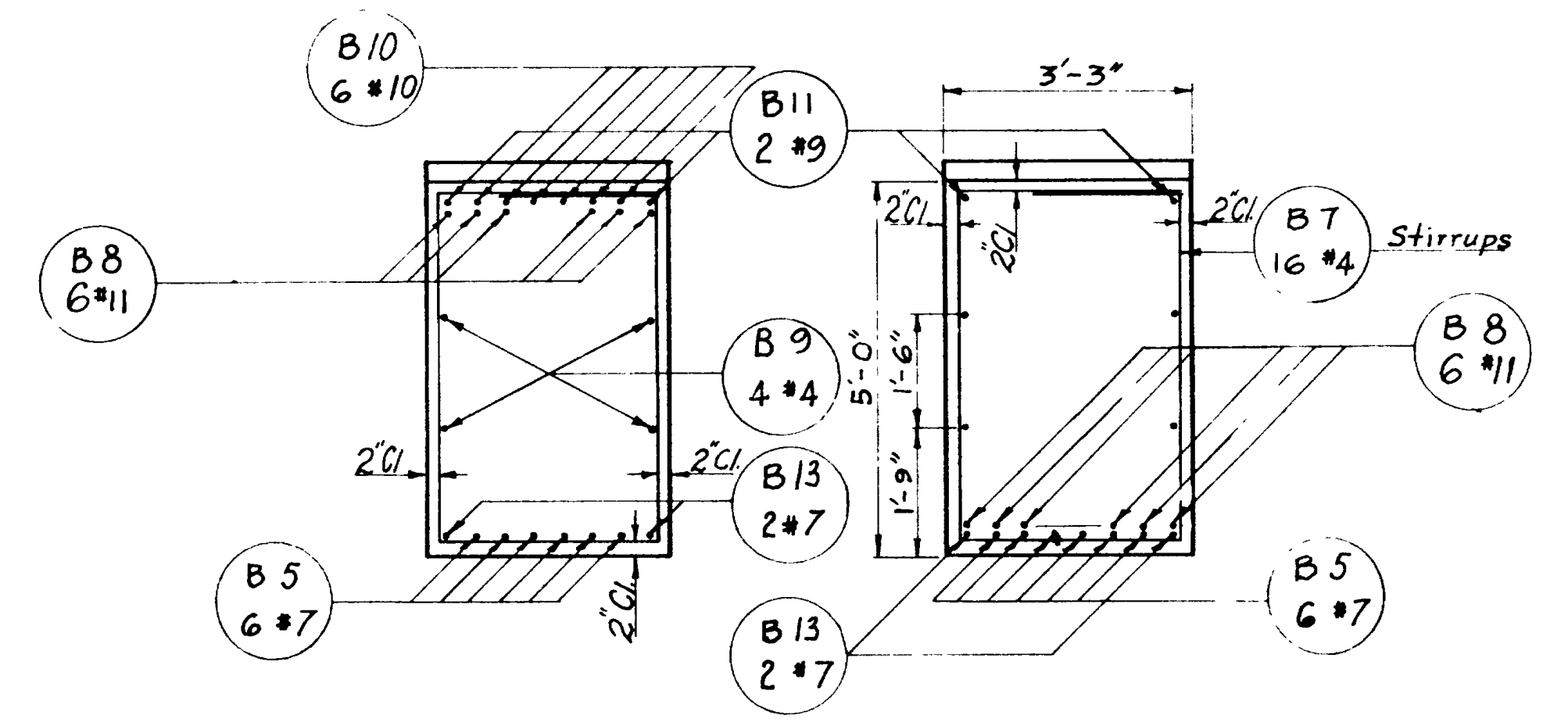


SECTION A-A

UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS-WEBER CO. LINE TO 4400 SOUTH RIVERDALE ROAD LYPASS			
ABUTMENT NO. I DETAILS			
DESIGNED BY: R.B.J.	CHECKED BY: C.E.Y.	PROJECT NUMBER: 1-15-8(20)334	
DRAWN BY: A.C.P.	CHECKED BY: C.E.Y.	PROJECT NUMBER: 904+43.8	
QUANTITIES BY: P.C.P.	CHECKED BY: C.E.Y.	STATION: WEBER	
APPROVAL: Joseph E. [Signature]	GROUP LEADER: [Signature]	COUNTY: WEBER	
APPROVED: 2-2-63	DATE: 2-2-63	CHIEF STRUCTURAL ENGINEER: [Signature]	
BR NO:	DRG NO: C-453	7 OF 29	

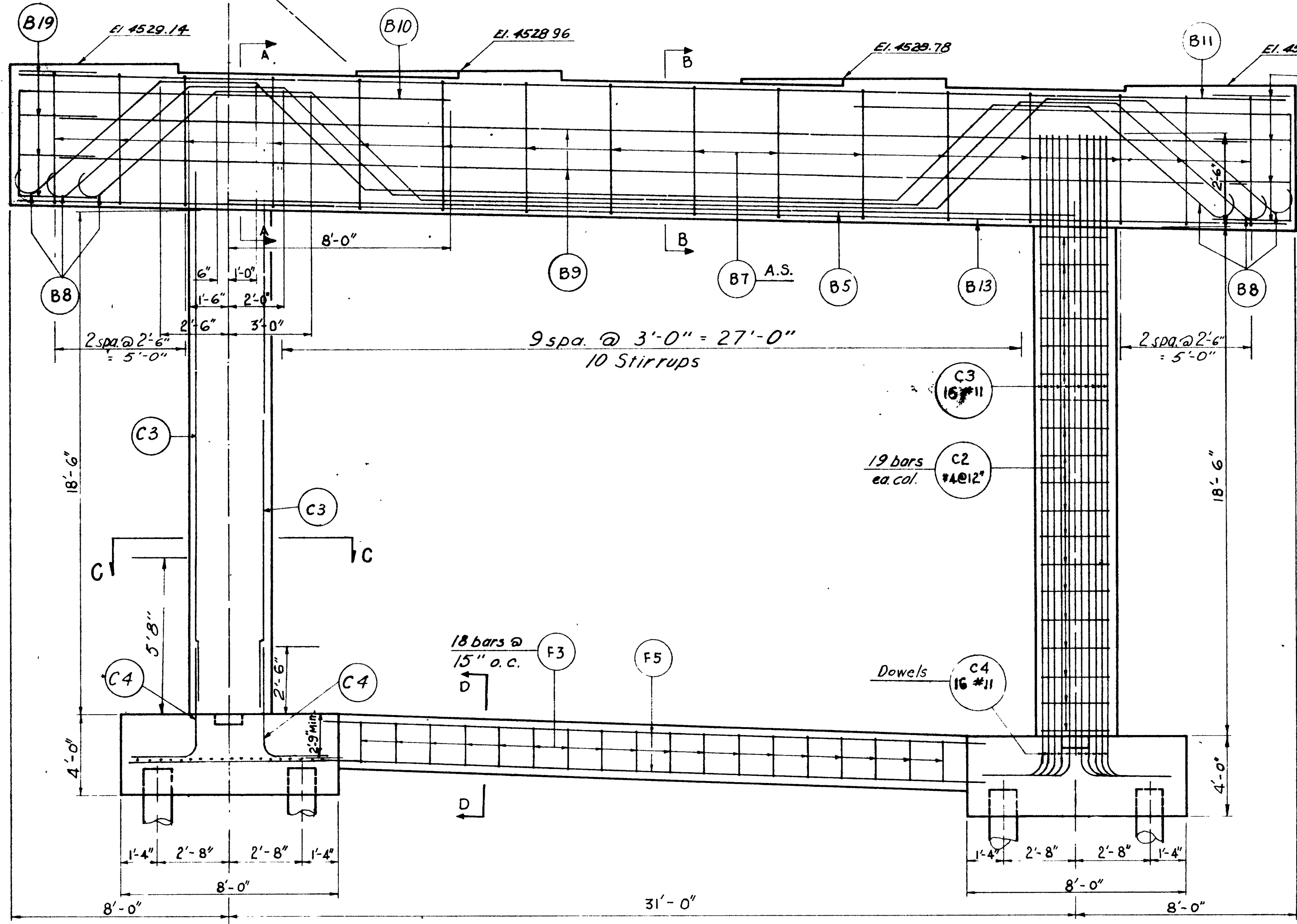


PLAN

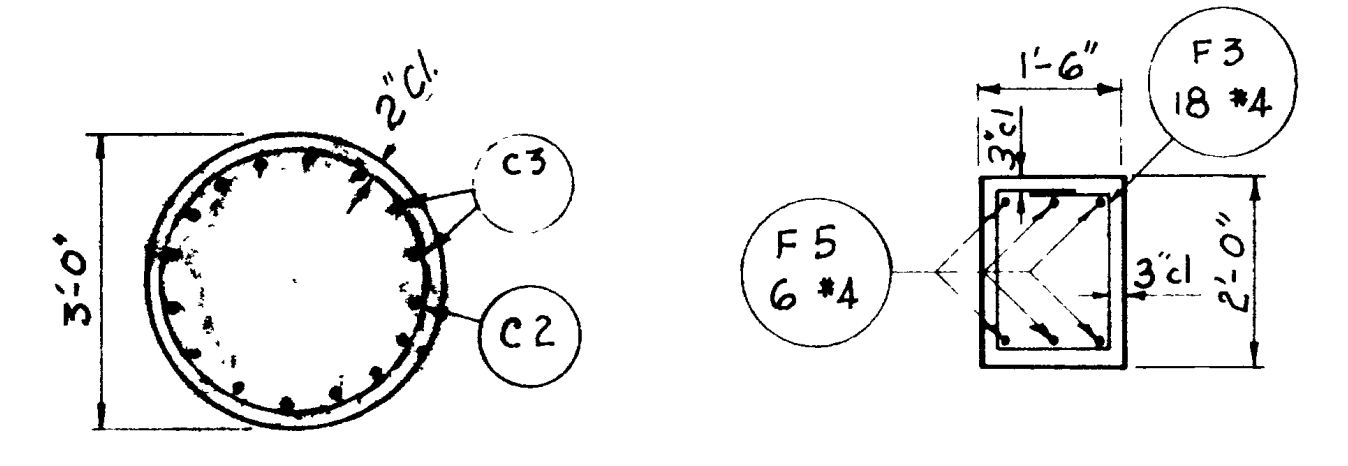


SECTION A-A

SECTION B-B

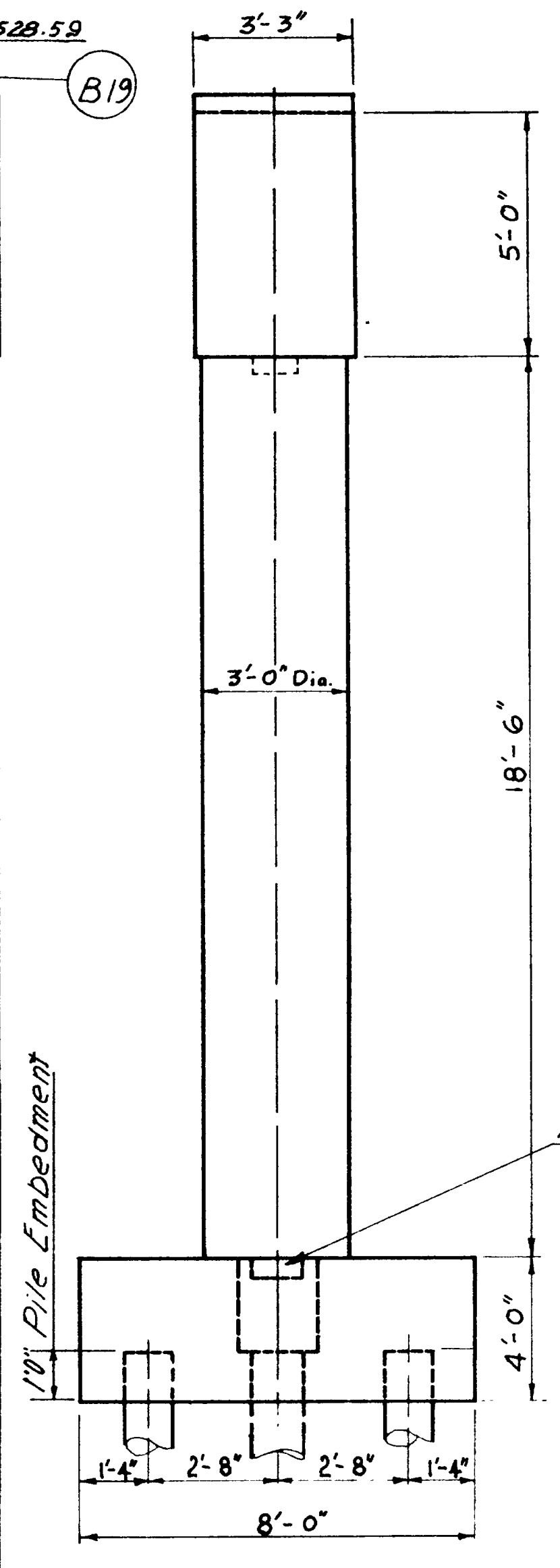


ELEVATION

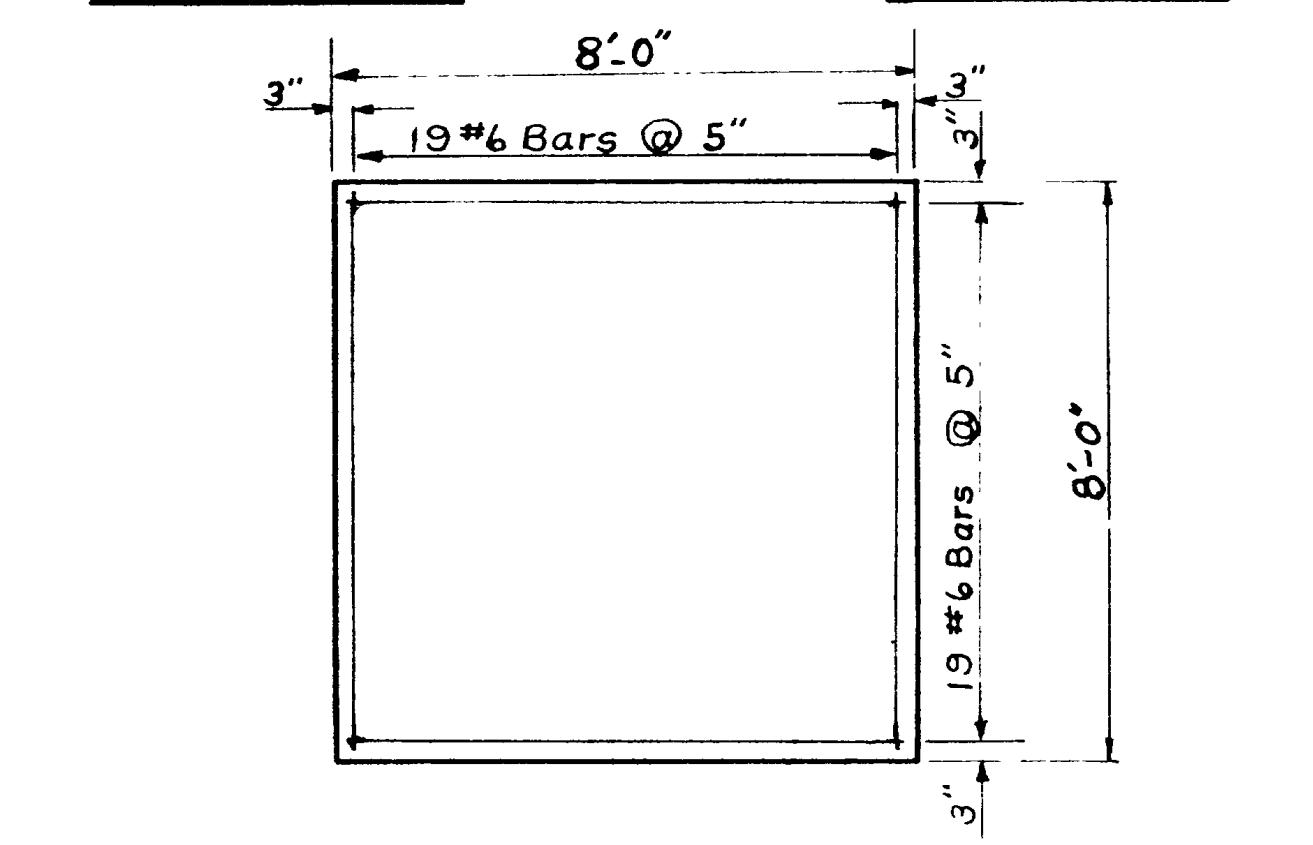


SECTION C-C

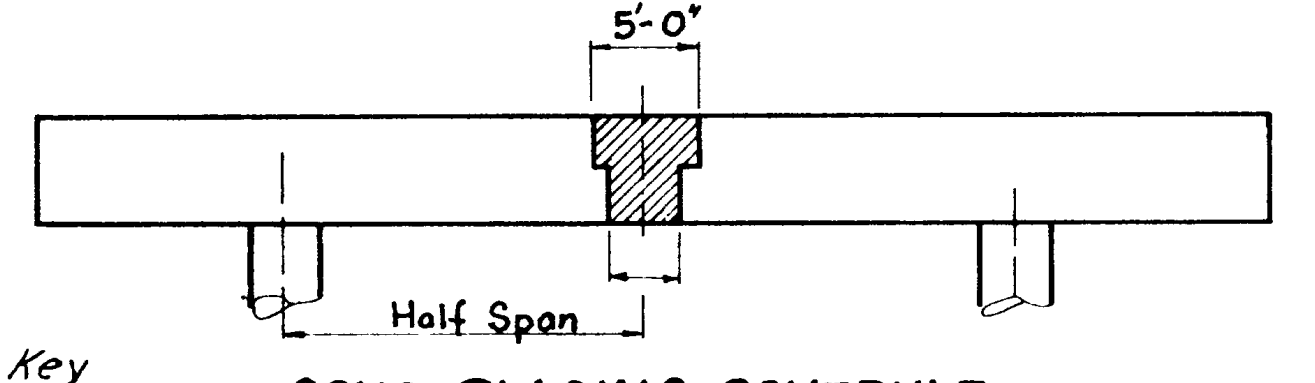
SECTION D-D



END ELEVATION



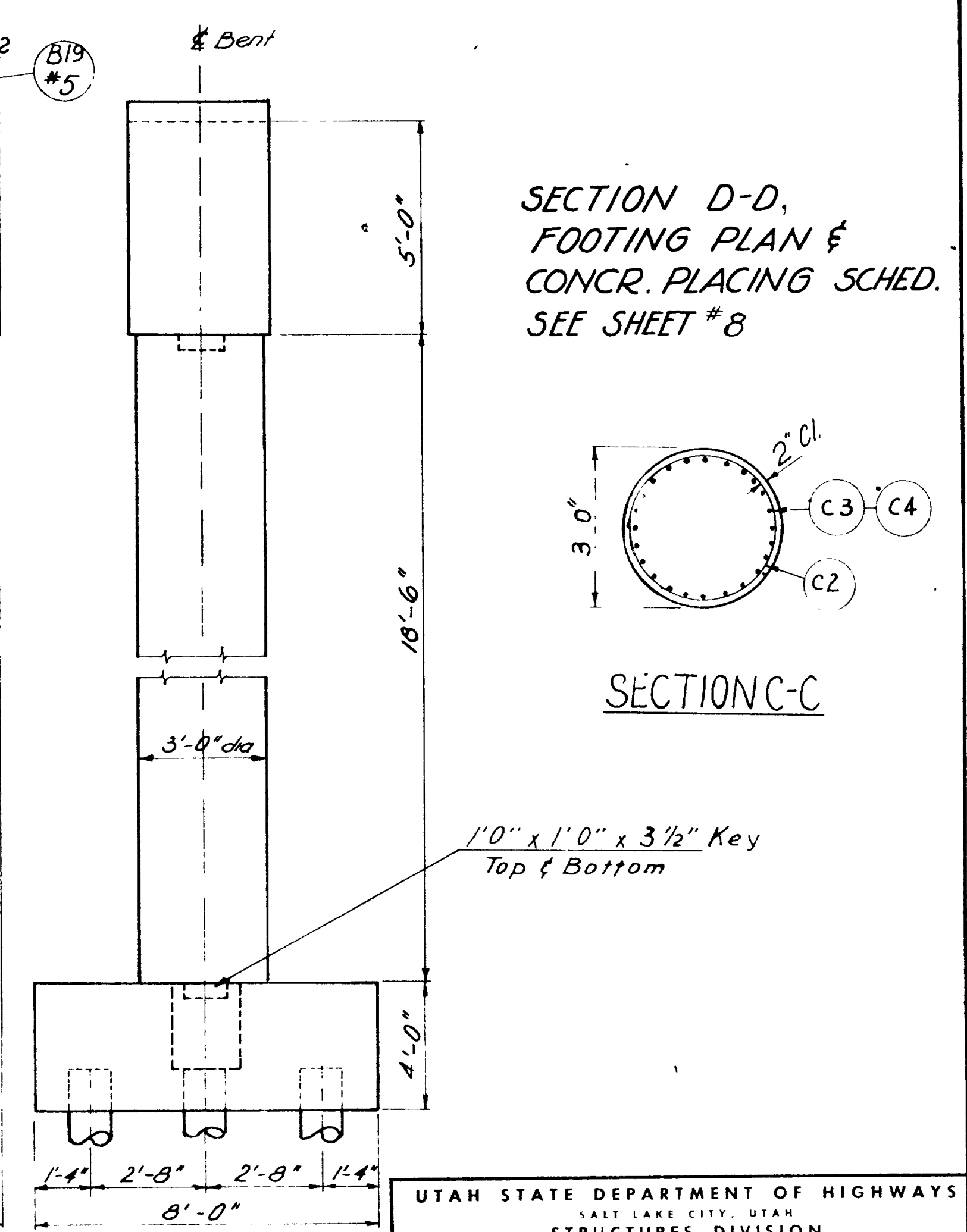
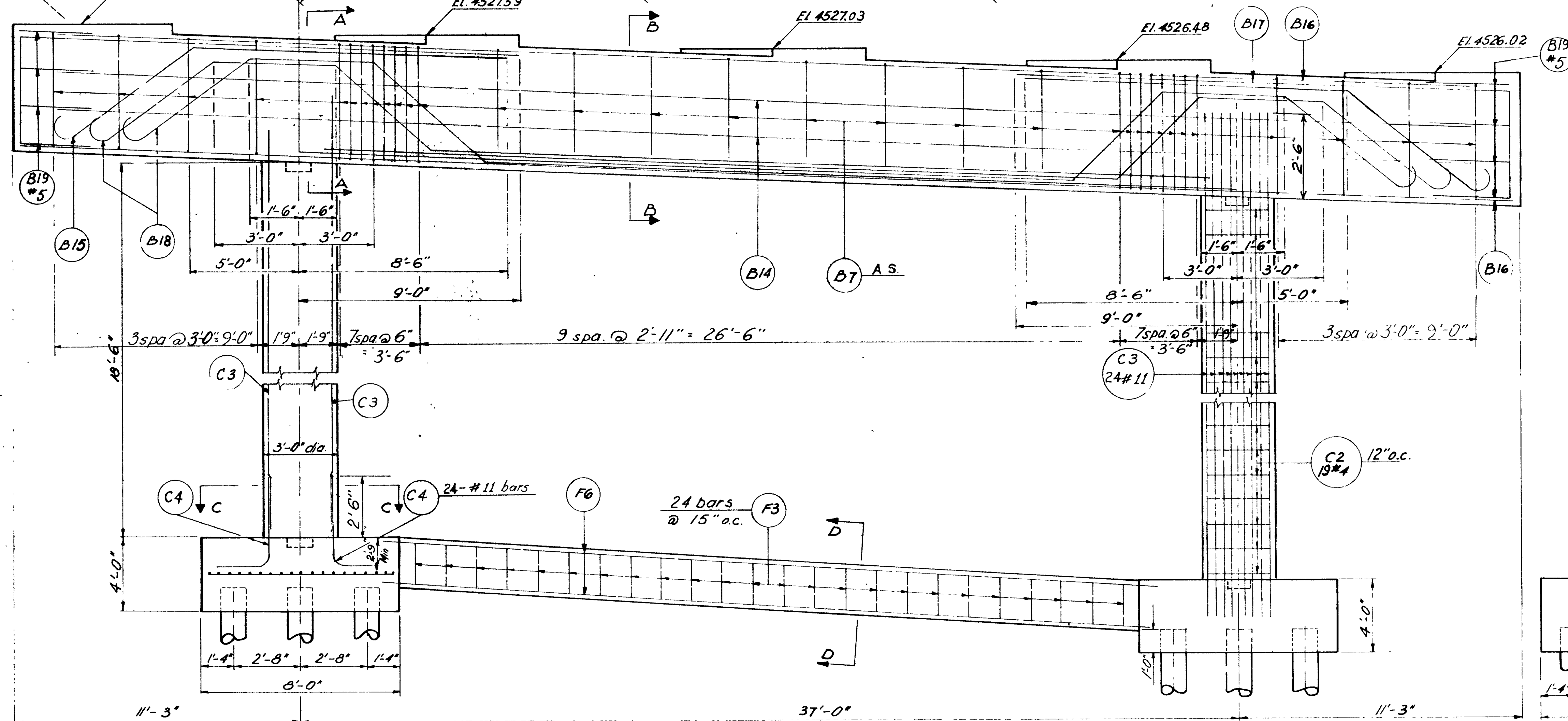
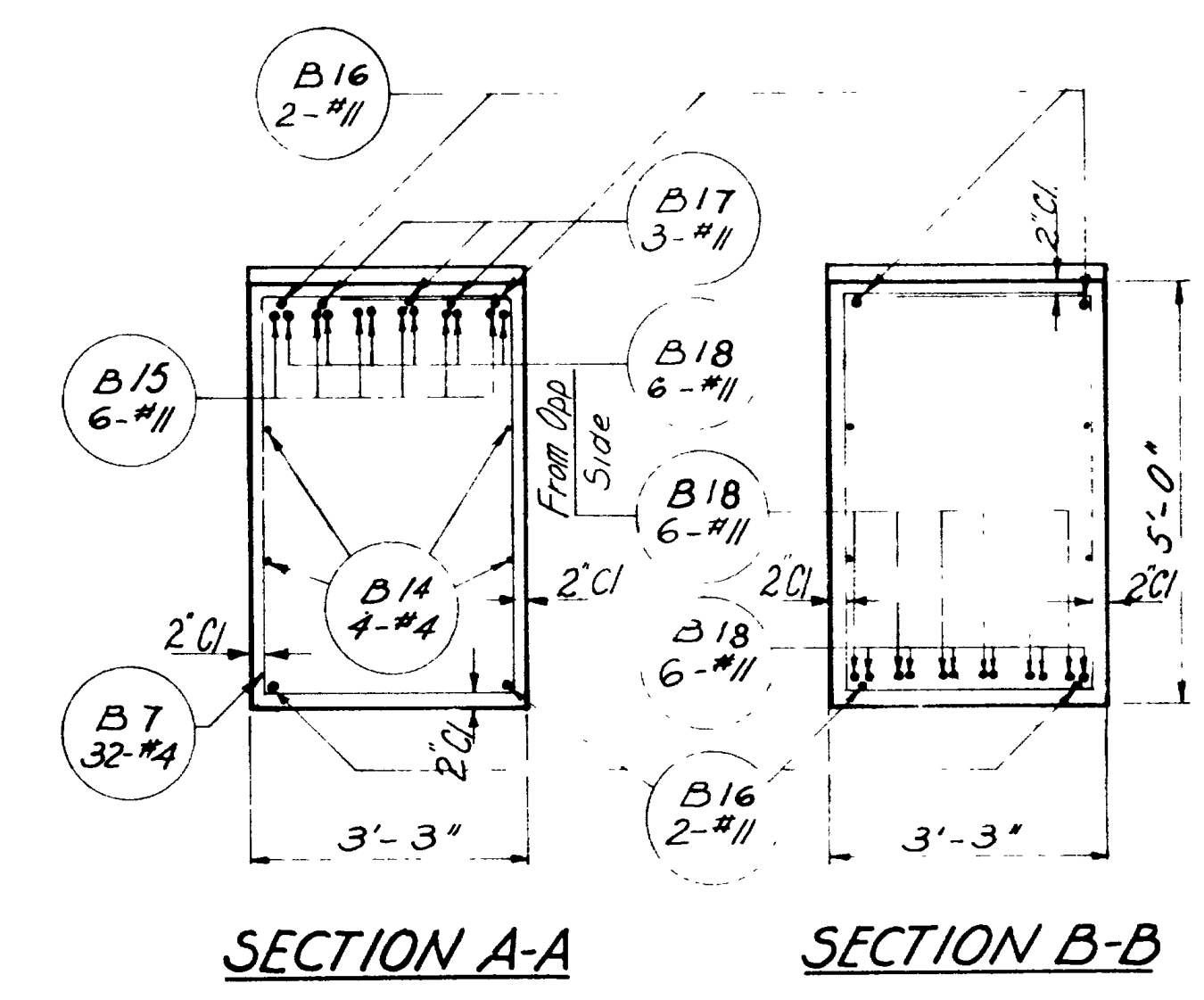
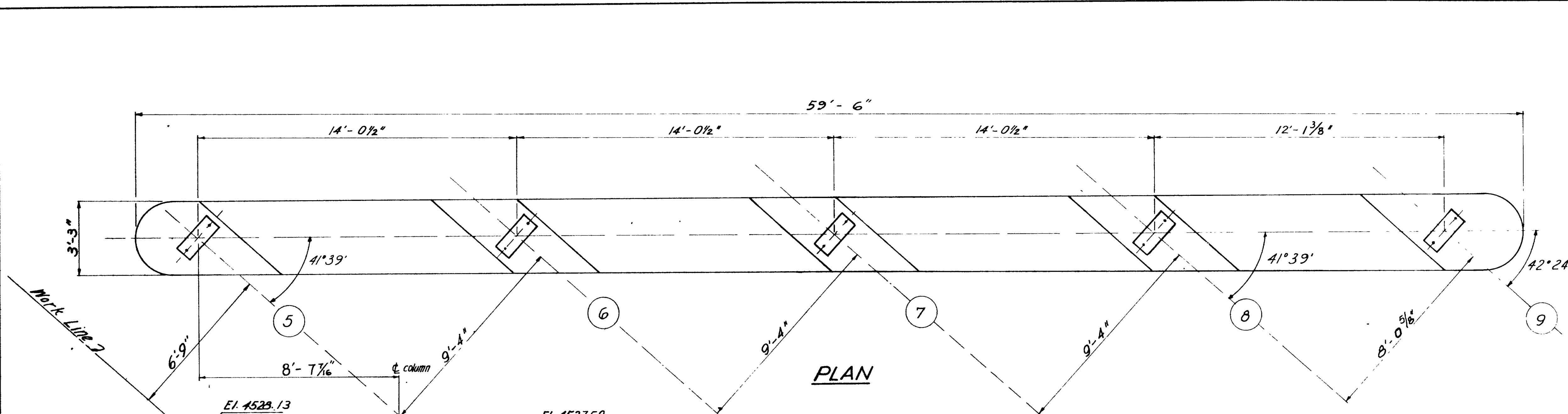
FOOTING PLAN



CONC. PLACING SCHEDULE

Shaded portion to be placed at least 7 days after unshaded portions are placed.

UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS - WEBER COLINE TO 4400 SOUTH RIVERDALE ROAD U-PASS			
BENT II-A DETAILS			
DESIGNED BY R.B.U.	CHECKED BY C.E.Y.	PROJECT NUMBER I-15-8(20)334	
DRAWN BY C.E.Y.	CHECKED BY P.F.M.	STATION 904+43.8	
QUANTITIES BY A.C.P.	CHECKED BY C.E.Y.	COUNTY WEBER	
APPROVAL RECORDAL 7/19/63	DATE	DATE	
APPROVED 5-11-1963	L. L. DANFORTH	DATE	
NO.	BY	DATE	REMARKS
REVISIONS			
BR NO.	DRG NO.	C-453	8 OF 29



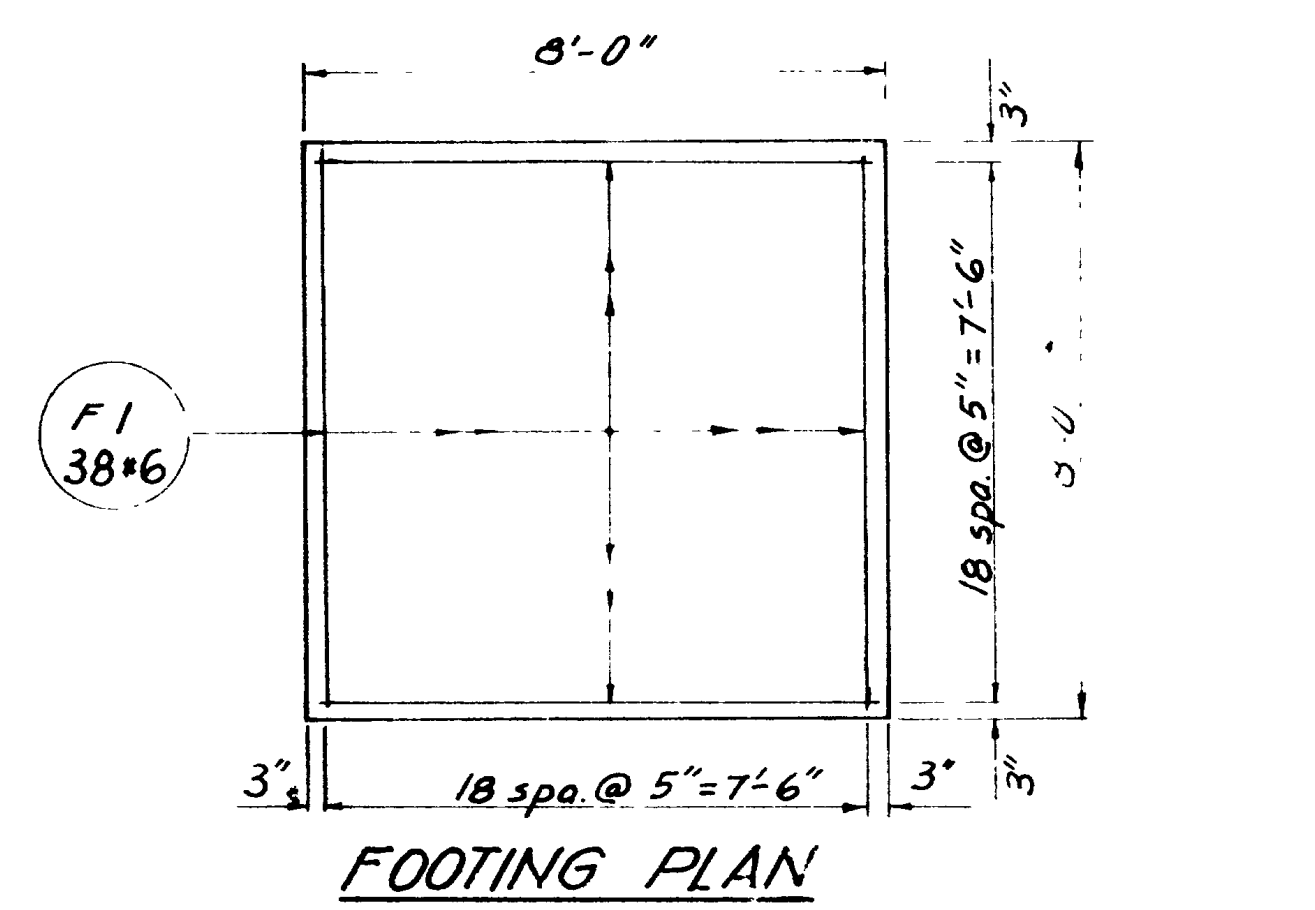
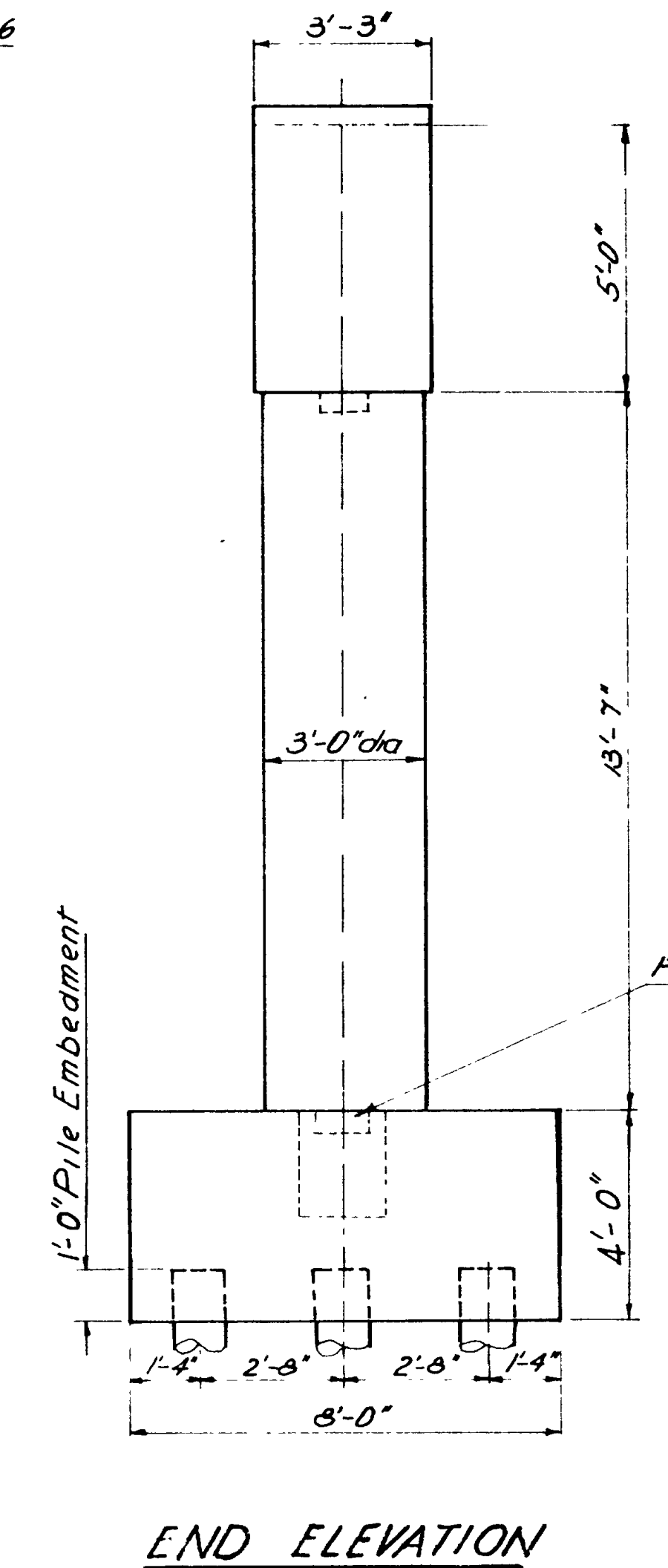
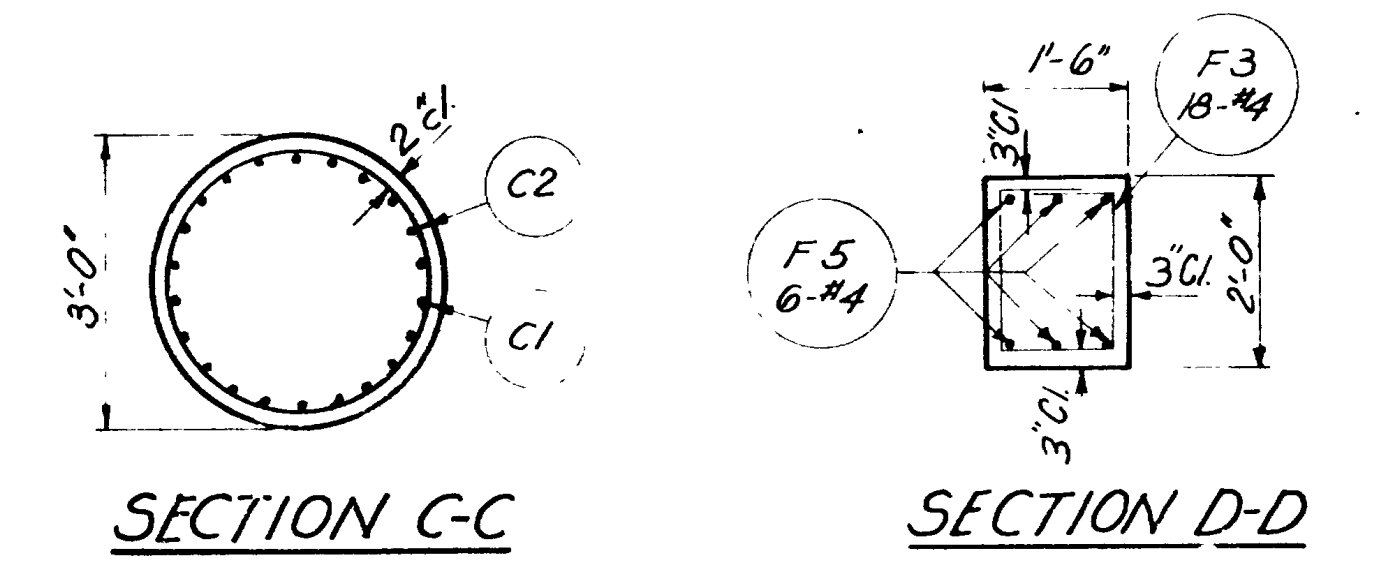
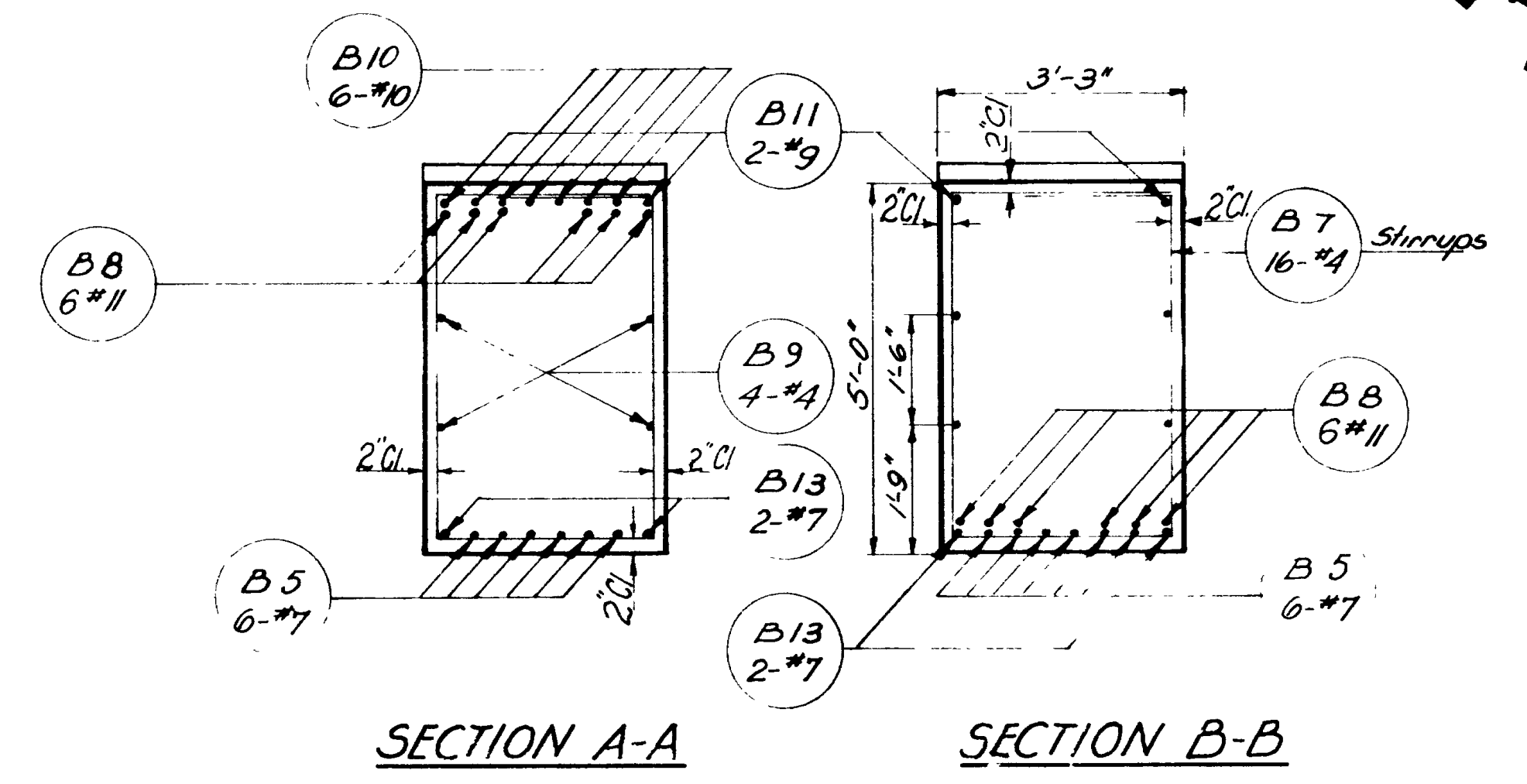
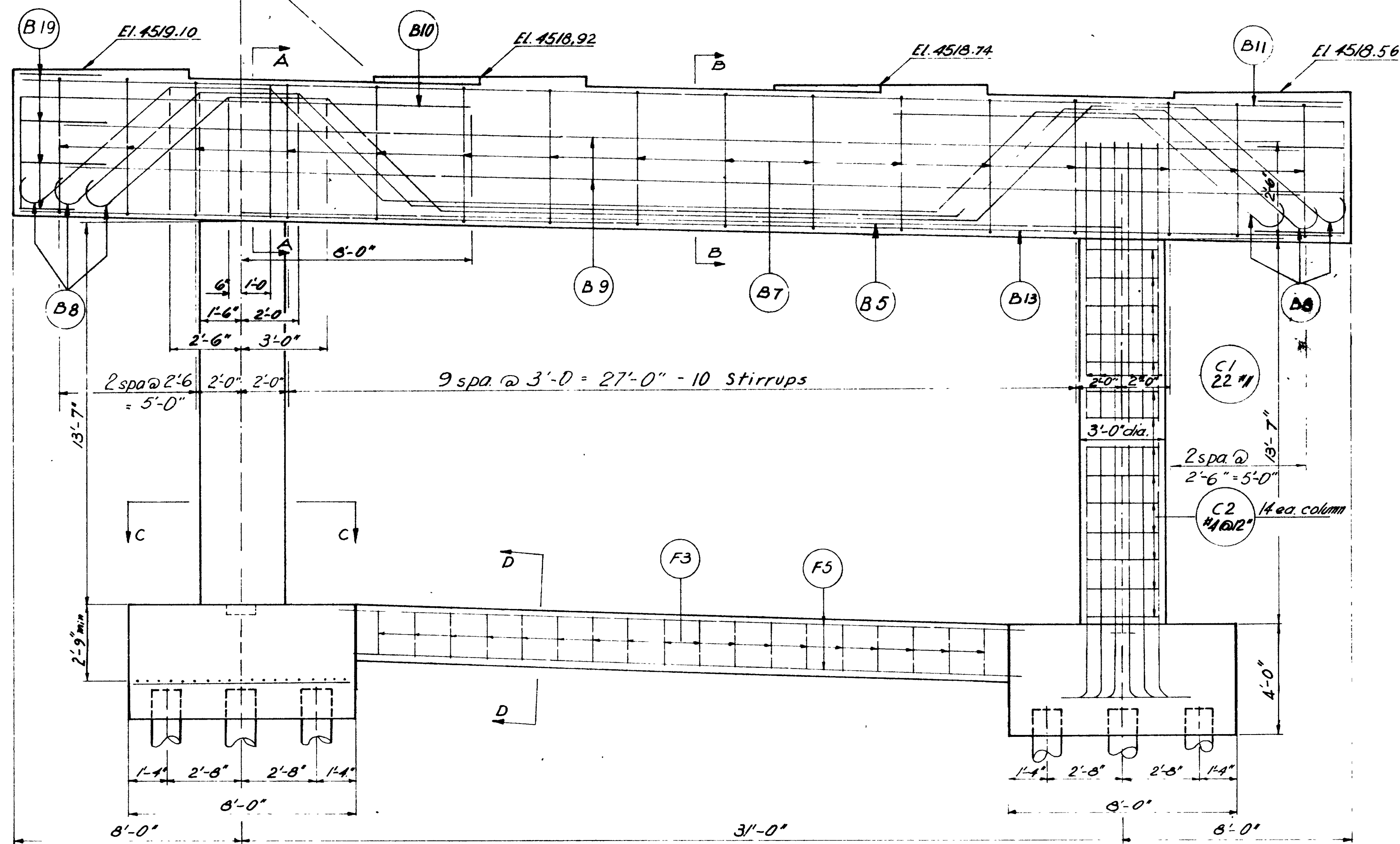
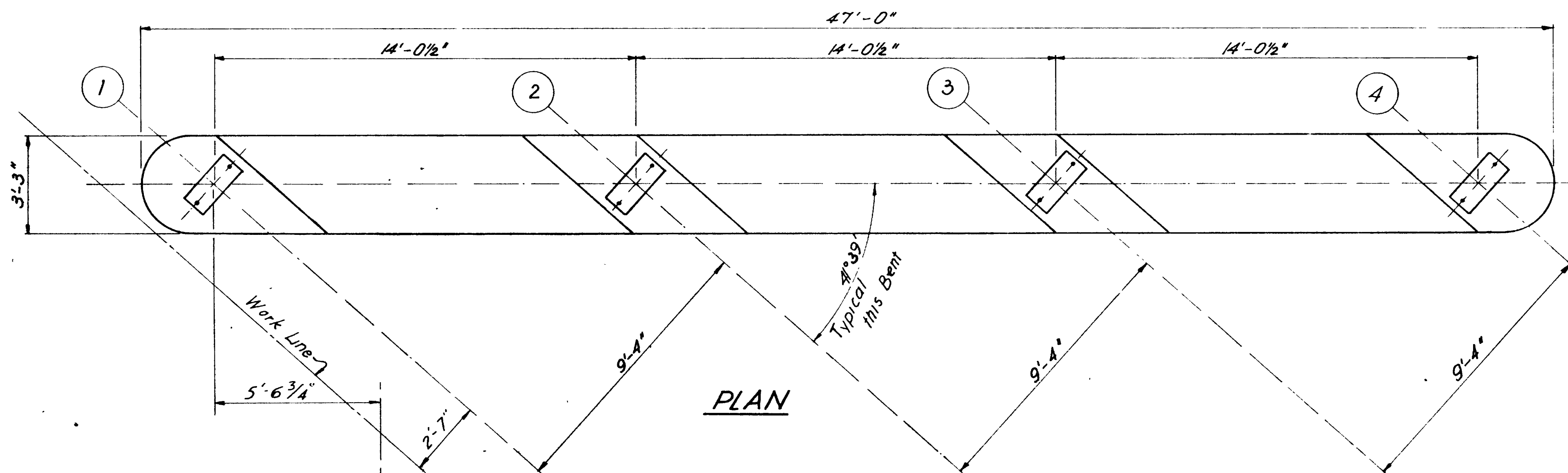
ELEVATION

END ELEVATION.

UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS WEBER CO LINE TO 4400 SOUTH RIVERDALE ROAD U'PASS			
BENT II-B DETAILS			
DESIGNED BY	RBJ	CHEKED BY	CEY
DRAWN BY	ACP	CHEKED BY	PEM
QUANTITIES BY	ACP	CHEKED BY	CEY
APPROVAL	7/19/63	Joseph E. West	904+43.8
APPROVED	DATE	BY	SECTION
			WEBER
BR NO.		FOR	C-453
			9 OF 29

NO	BY	DATE	REMARKS
REVISIONS			

REVISIONS
 NO. DATE BY
 1 10/15/53 R.B.J.
 2 11/10/53 A.C.P.
 3 11/10/53 R.B.J.
 4 11/10/53 R.B.J.
 5 11/10/53 R.B.J.
 6 11/10/53 R.B.J.
 7 11/10/53 R.B.J.
 8 11/10/53 R.B.J.
 9 11/10/53 R.B.J.
 10 11/10/53 R.B.J.

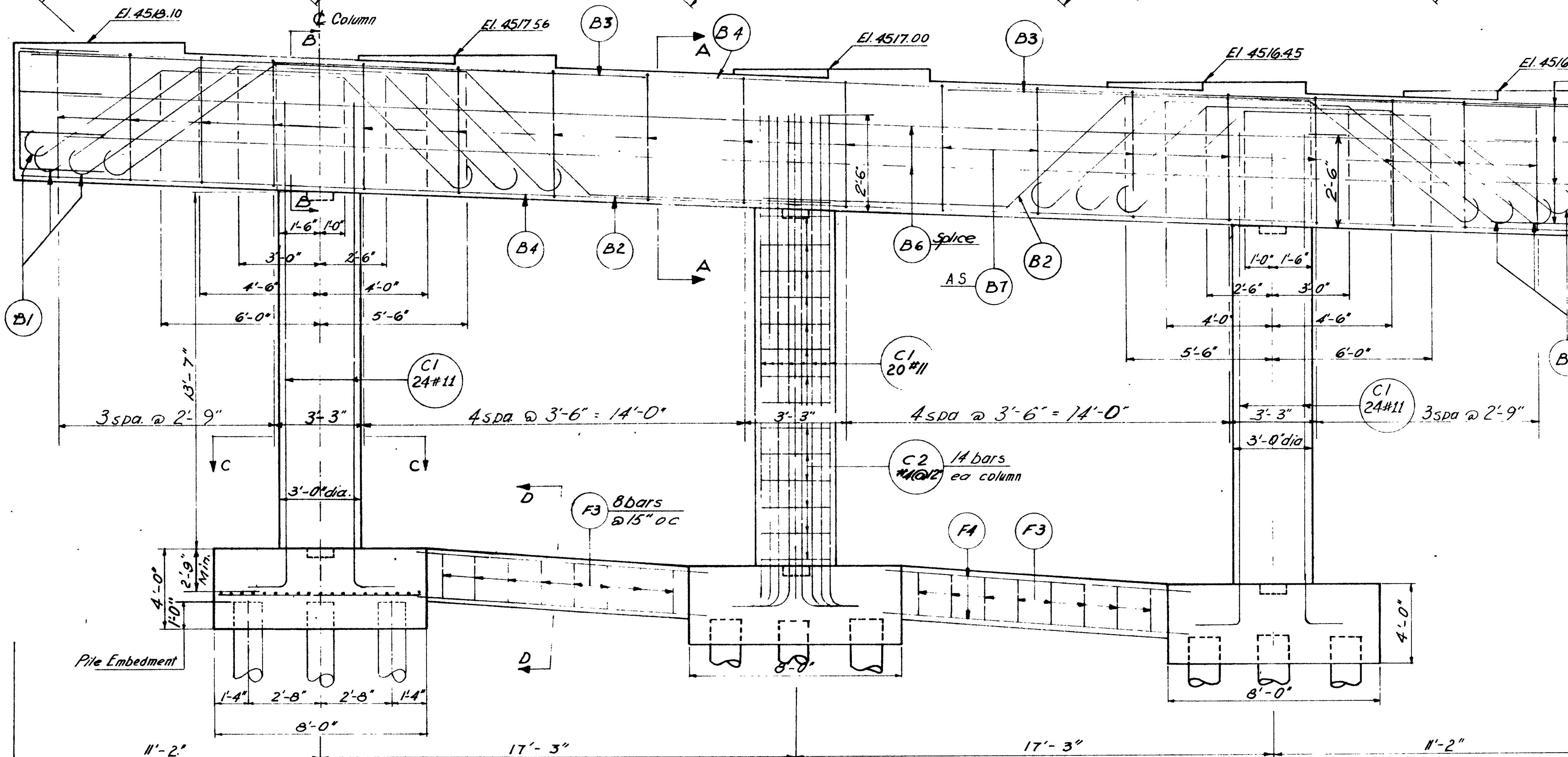
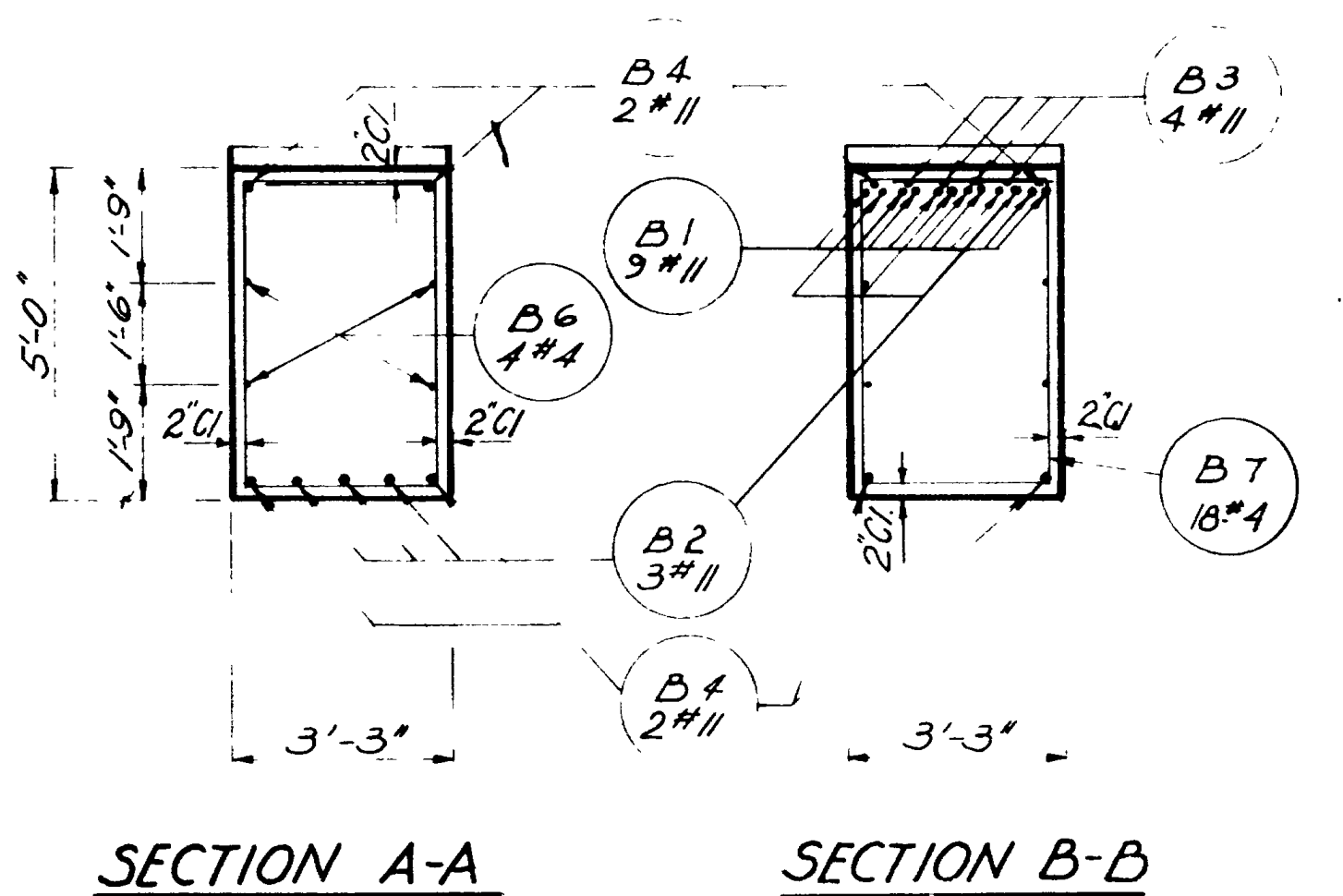
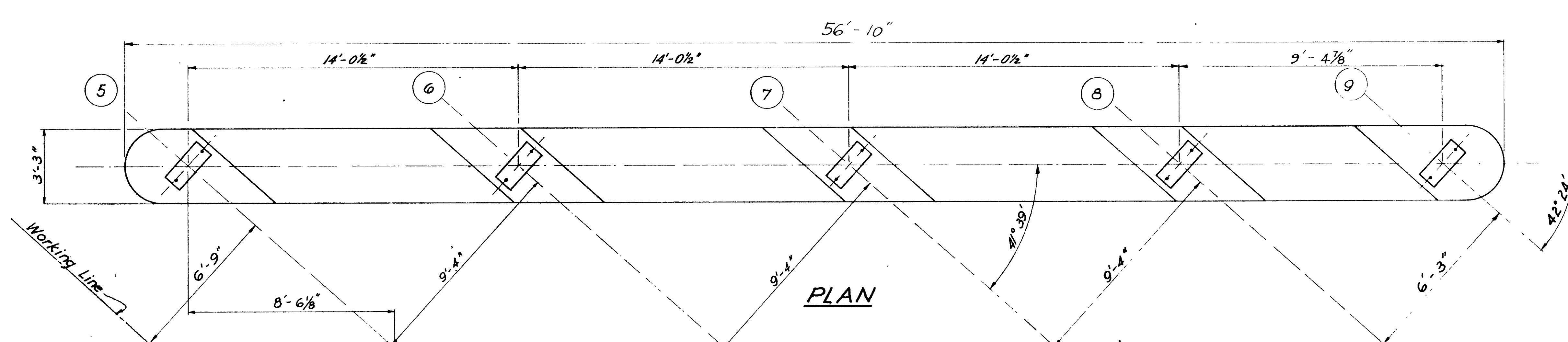


CONC. PLACING SCHEDULE
 Shaded Portion to be placed at least 7-Days after unshaded portions are placed.

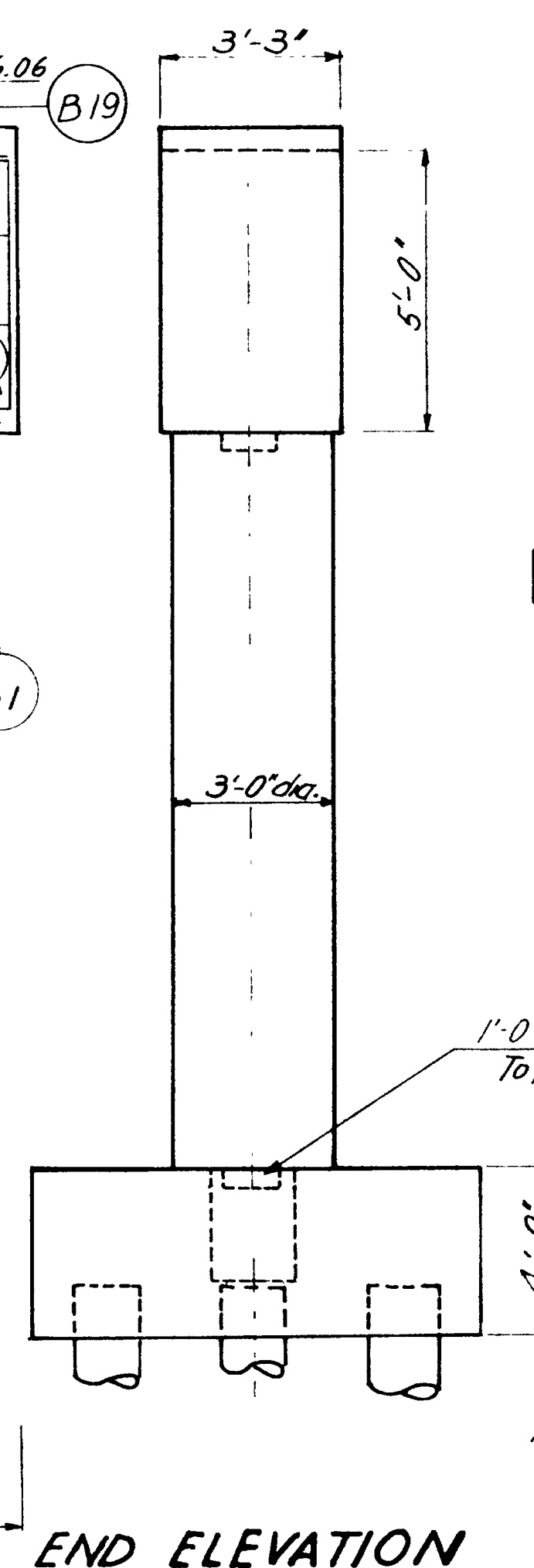
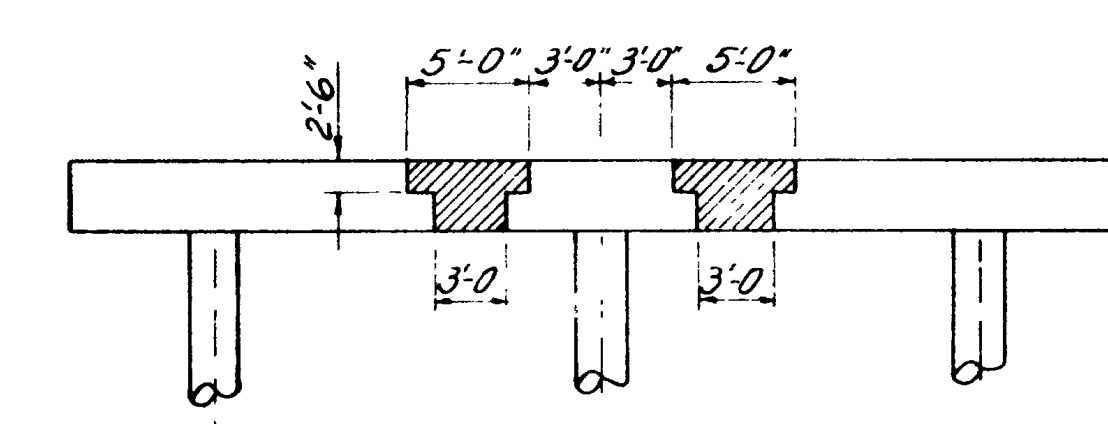
UTAH STATE DEPARTMENT OF HIGHWAYS			
SALT LAKE CITY UTAH			
STRUCTURES DIVISION			
DAVIS WEBER CO. LINE TO 4400 SOUTH			
RIVERDALE ROAD U/PASS			
BENT III-A DETAILS			
DESIGNED BY: R.B.J.	CHECKED BY: C.E.Y.	1-15-8(20)334	
DRAWN BY: A.C.P.	CHECKED BY: P.F.M.	PROJECT NUMBER	
QUANTITIES BY: A.C.P.	CHECKED BY: C.E.Y.	904+43.8	
APPROVAL RECOMMENDED BY: Joseph E. Weber	GROUP LEADER	STATION	
APPROVED DATE: 10/15/53	DATE	WEBER COUNTY	
BR NO	DRG NO: C-453	10 OF 29	

REVISIONS

NO.	DATE	BY	REASON

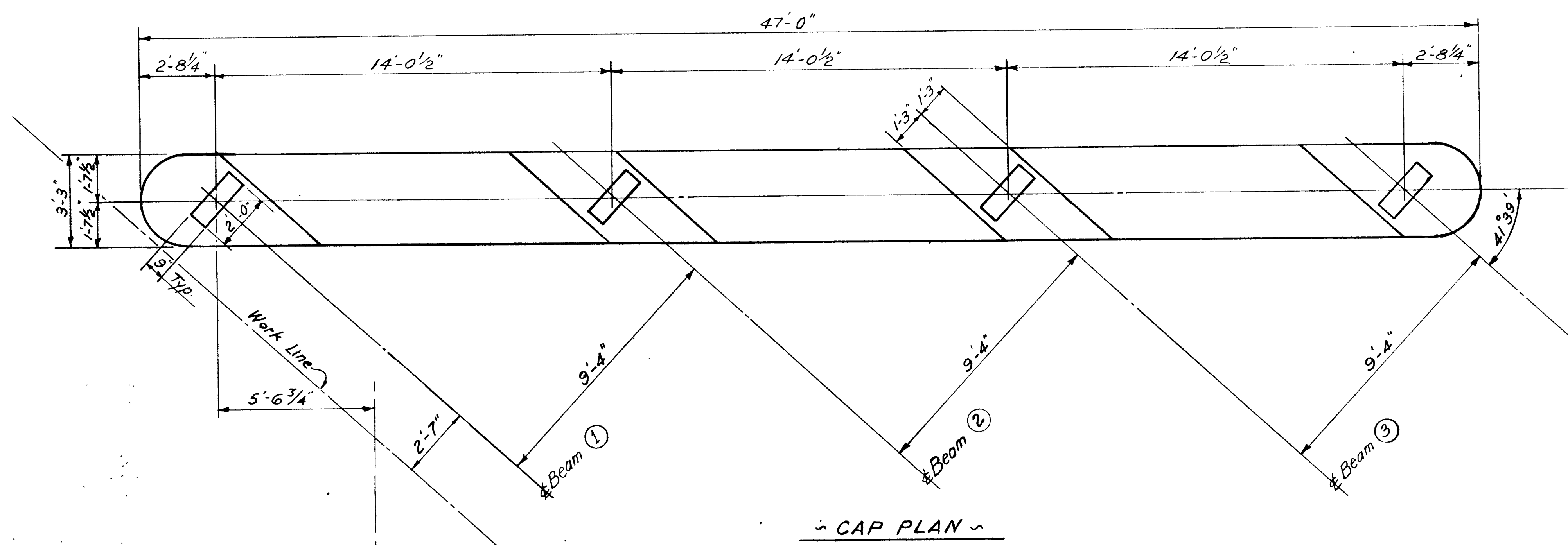


SECTION C-C
SECTION D-D & FOOTING PLAN
SEE SHEET # 10

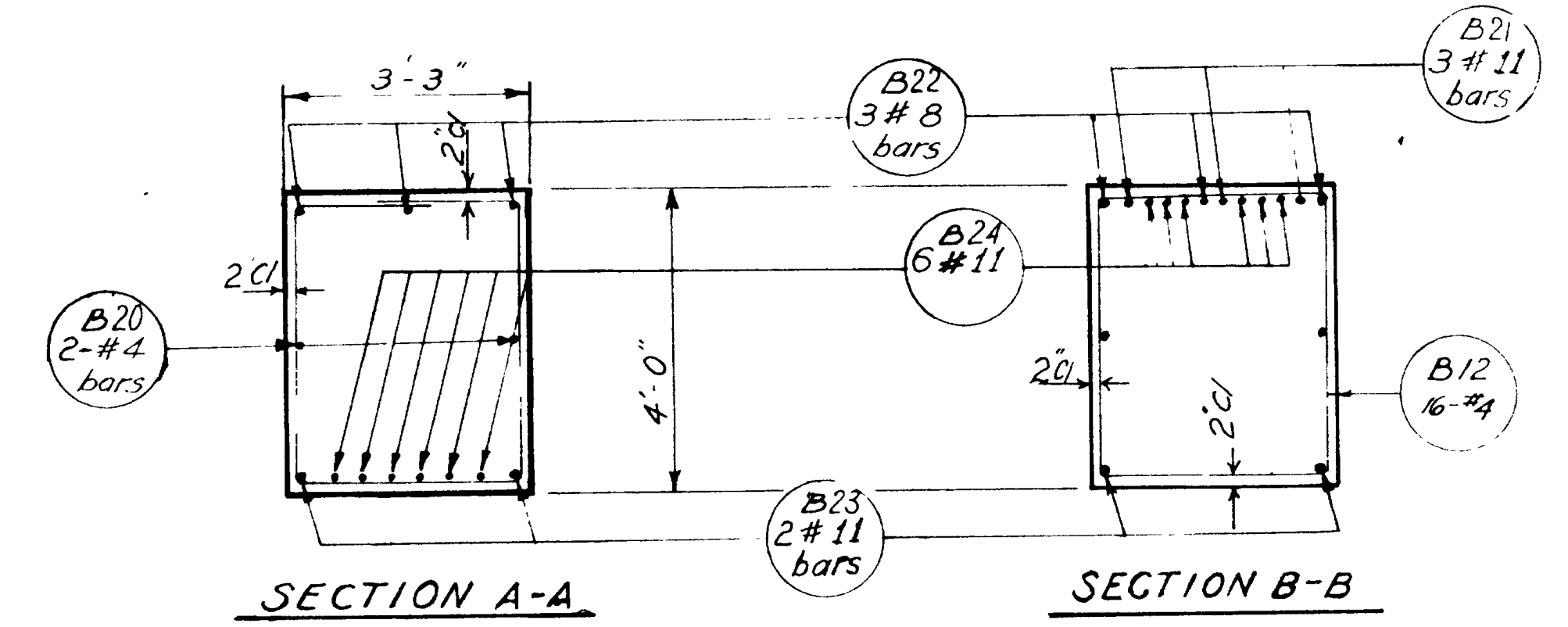


Note See Foundation plan for pile arrangement

UTAH STATE DEPARTMENT OF HIGHWAYS			
SALT LAKE CITY, UTAH			
STRUCTURES DIVISION			
DAVIS- WEBER CO. LINE TO 4400 SO.			
RIVERDALE ROAD U-PASS			
BENT - DETAIL III-B			
DESIGNED BY	CHECKED BY	PROJECT NUMBER	1-15-81(20)334
DRAWN BY	CHECKED BY	STATION	904+43.8
QUANTITIES BY	CHECKED BY	GROUP LEADER	WEBER
APPROVAL	DATE	DATE	DATE
RECOMMENDED BY	DATE	DATE	DATE
APPROVED	DATE	DATE	DATE
BR NO.	DRG NO.	11 of 29	

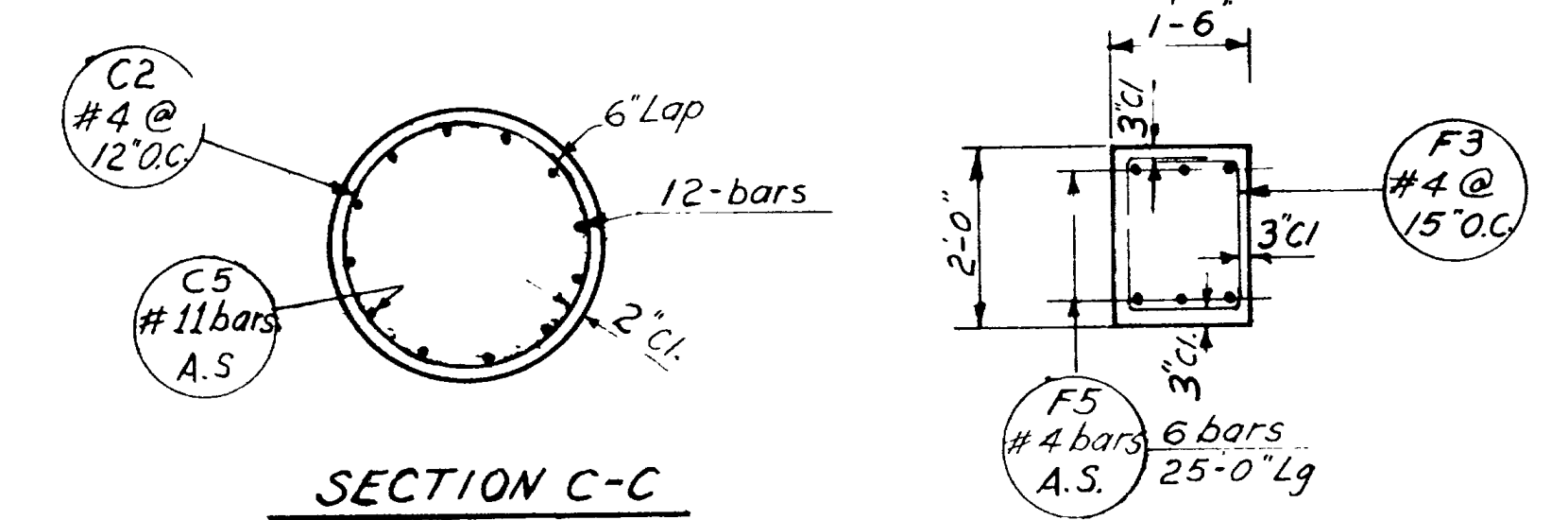


~ CAP PLAN ~



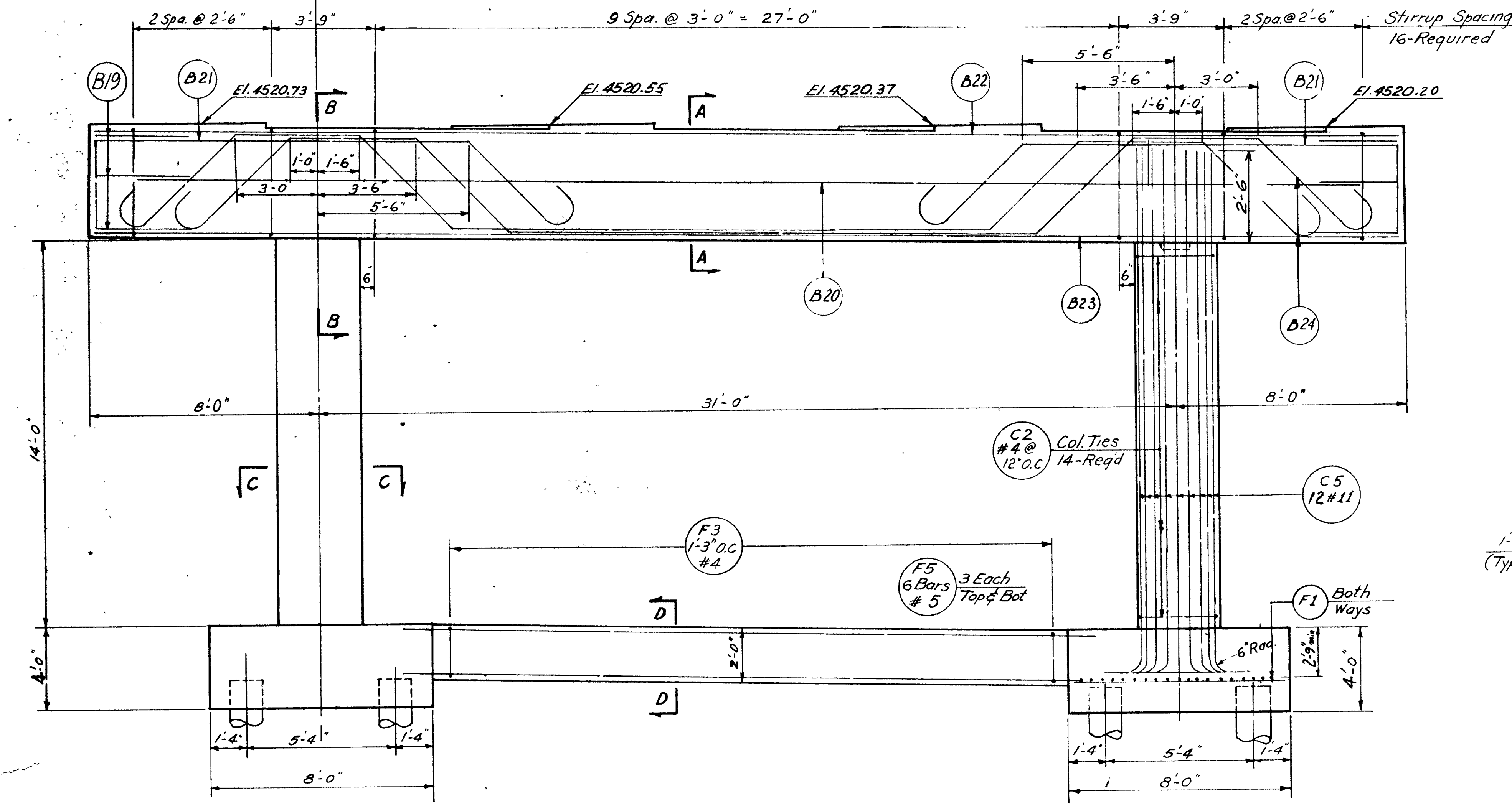
SECTION A-A

SECTION B-B

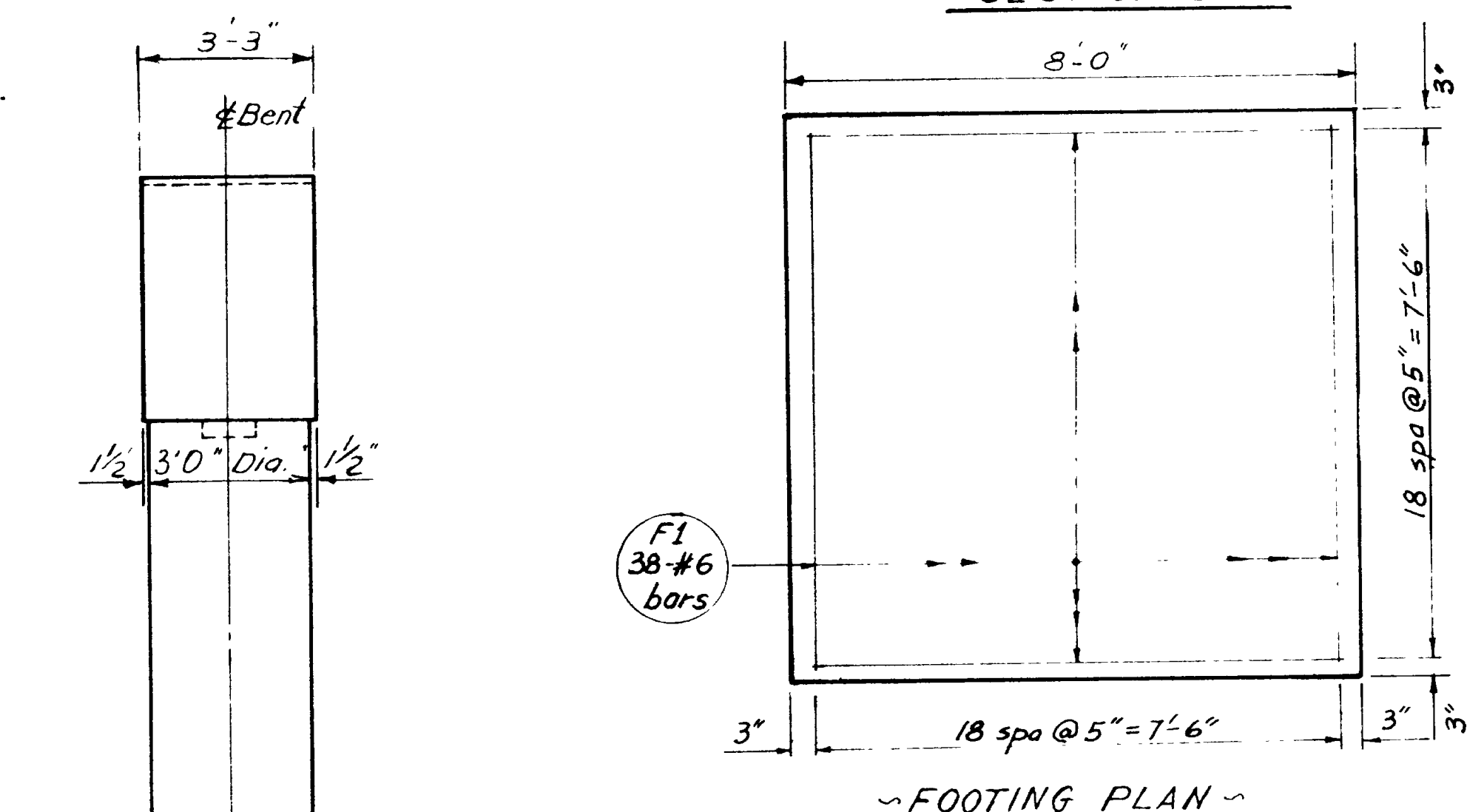


SECTION C-C

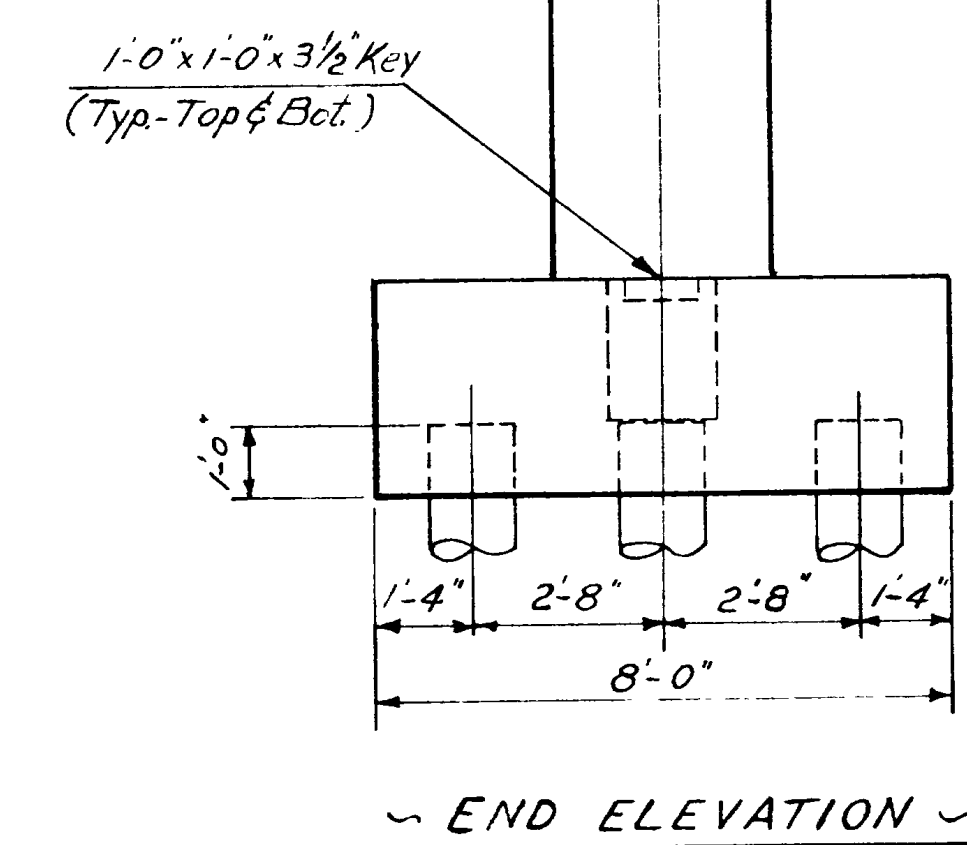
SECTION D-D



~ ELEVATION ~



~ FOOTING PLAN ~



~ END ELEVATION ~

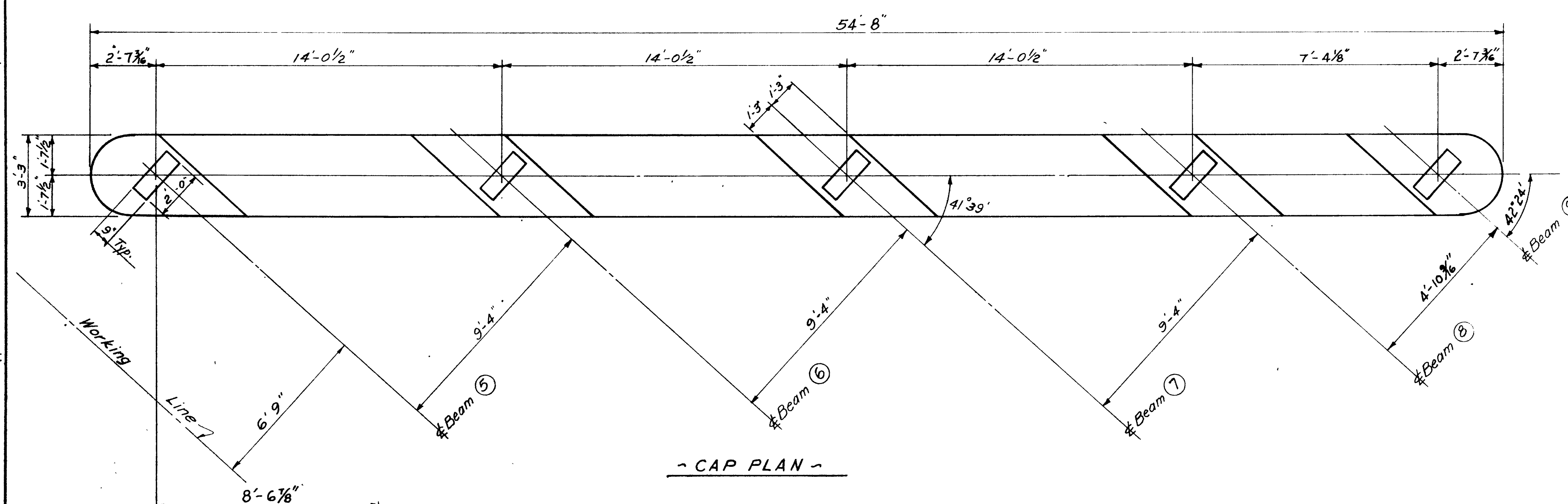
CONC. PLACING SCHEDULE
 Shaded Portion to be placed at least
 7-Days after unshaded portions are placed.

UTAH STATE DEPARTMENT OF HIGHWAYS			
SALT LAKE CITY, UTAH			
STRUCTURES DIVISION			
DAVIS-WEBER CO. LINE TO 4400 SD.			
RIVERDALE ROAD UPASS			
BENT II-A DETAILS			
DESIGNED BY RBJ	CHECKED BY CEY	1-15-8(20)33A	
DRAWN BY H.N.M.D.	CHECKED BY P.F.M.	PP 24.1 NUMBER	
QUANTITIES BY ACP	CHECKED BY CEY	904 + 438	
APPROVAL REFNO 7/14/63	APPROVED BY [Signature]	STATION	
APPROVED 5 Sept 1963	DATE	COUNTY	

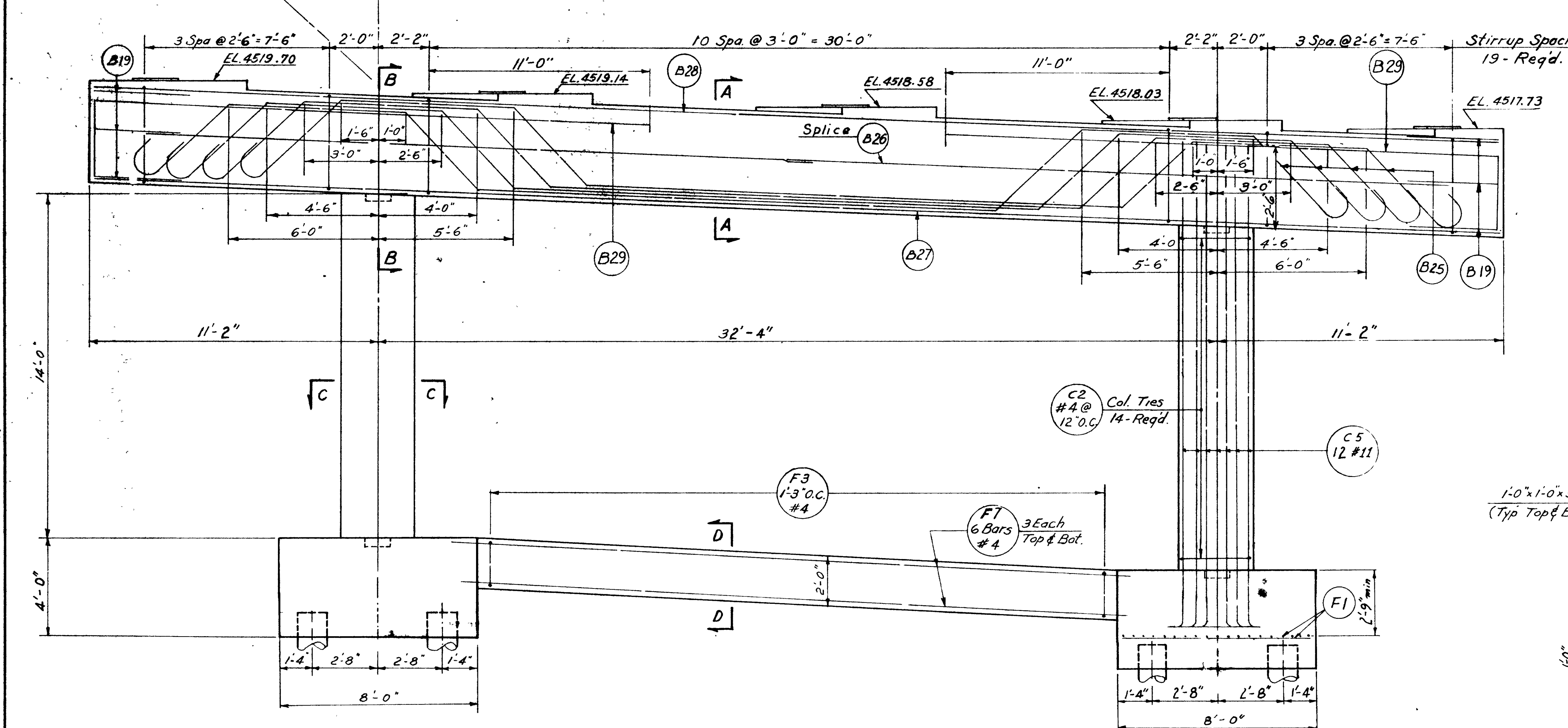
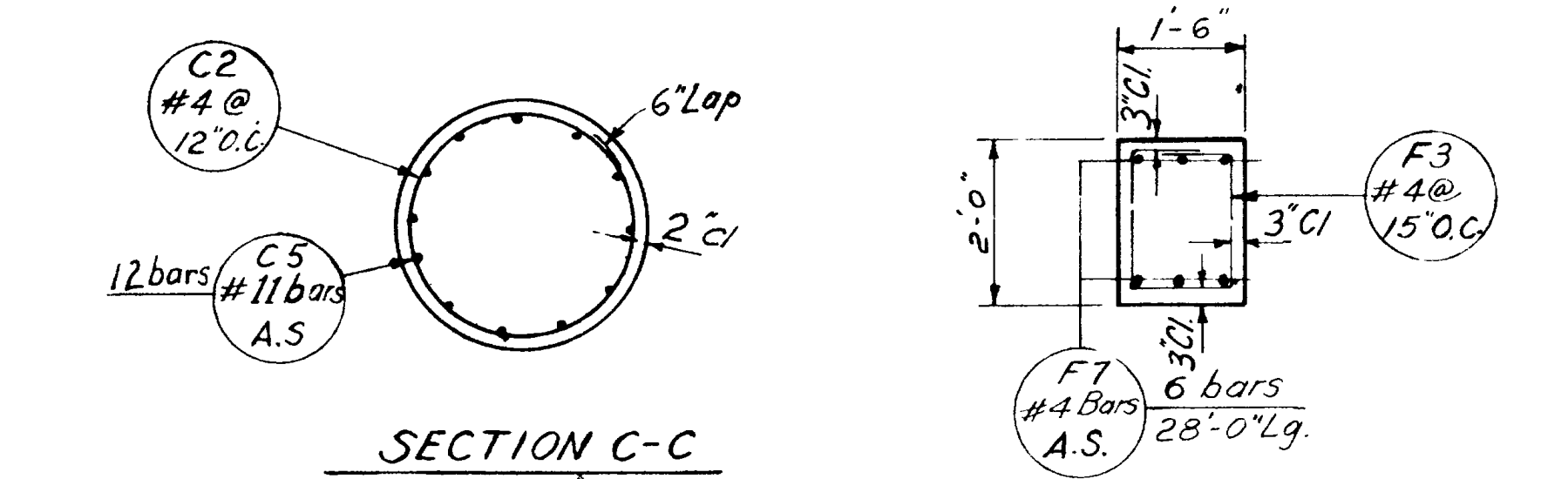
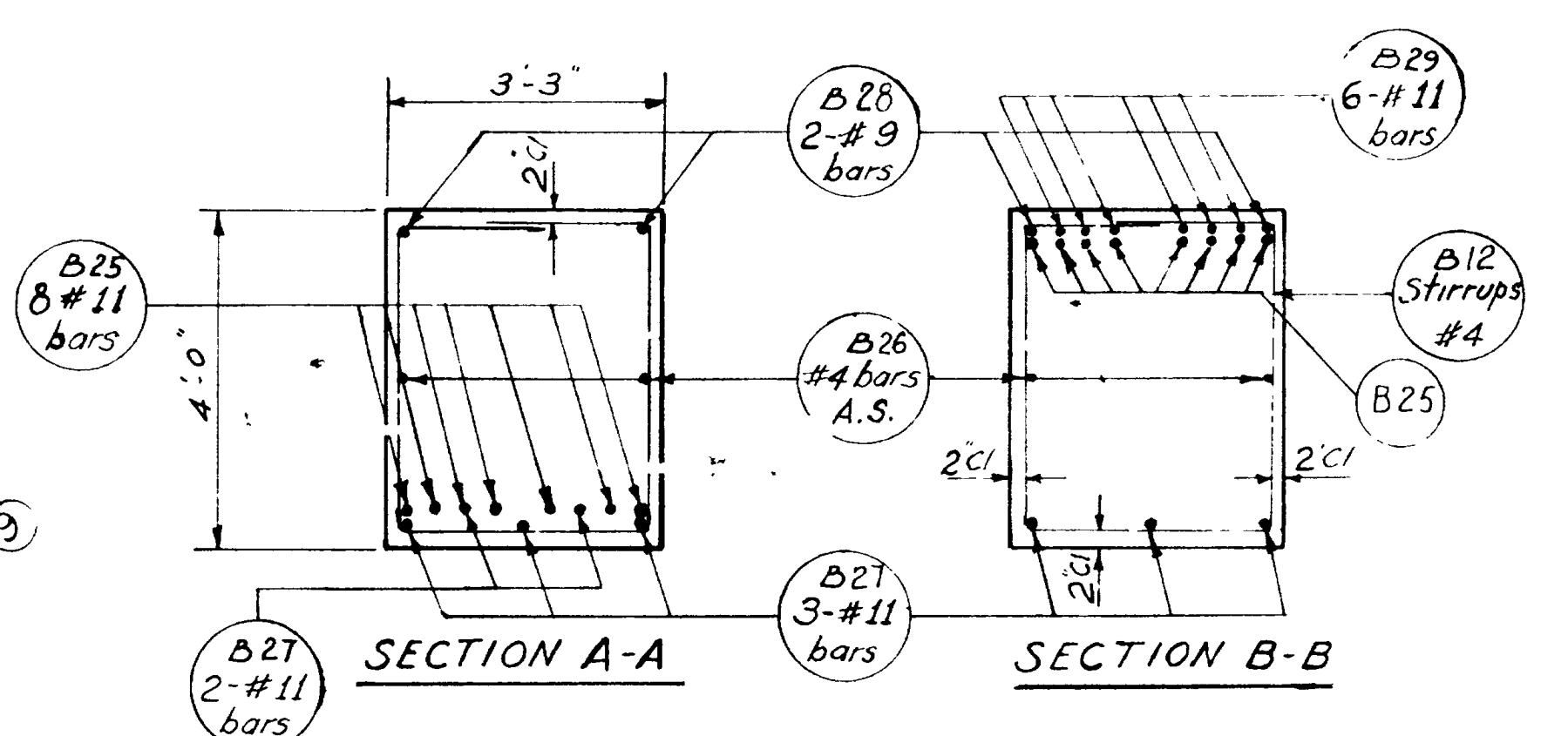
NO	BY	DATE	REMARKS

REVISIONS:

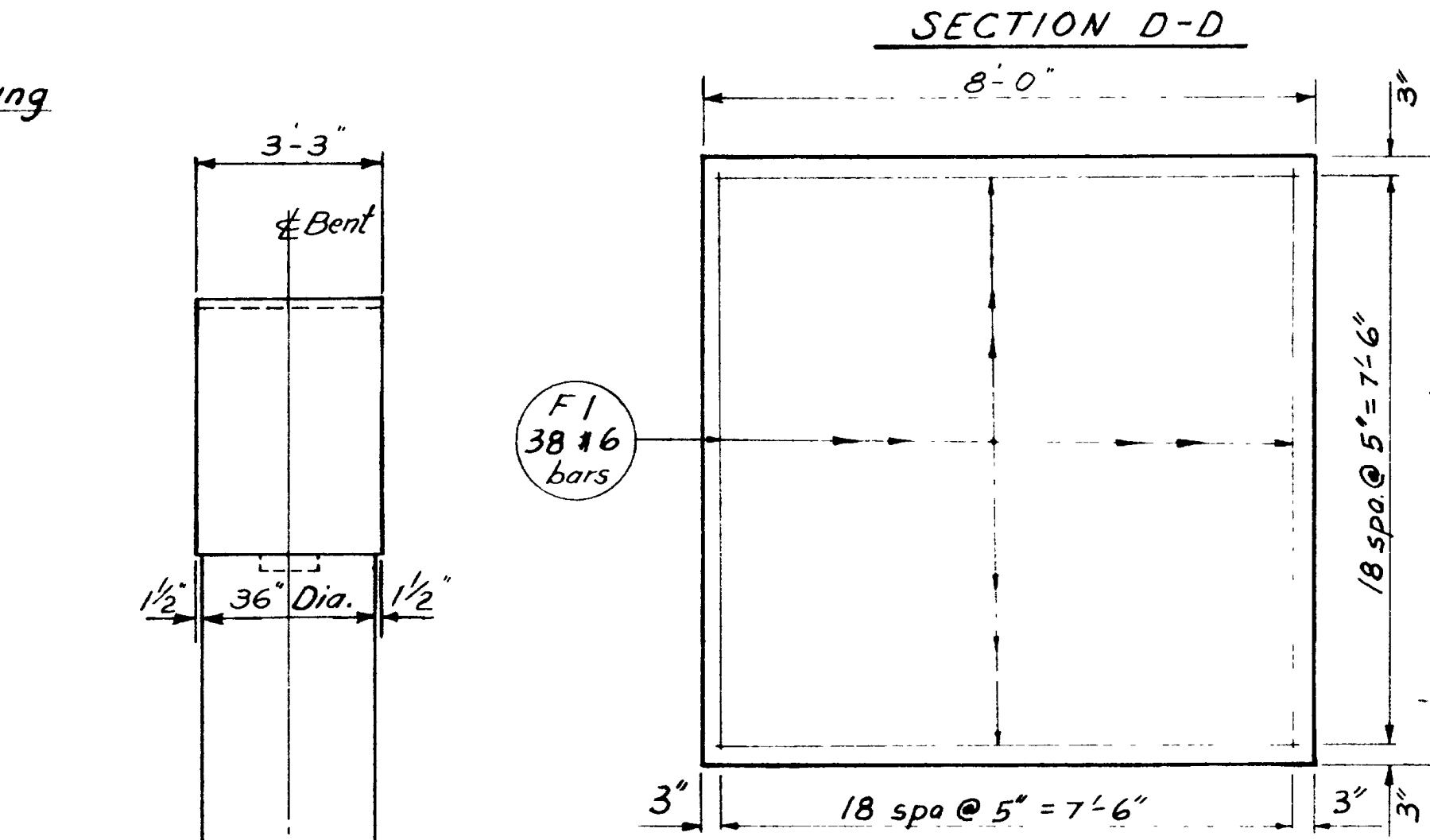
DATE	REVISIONS BY



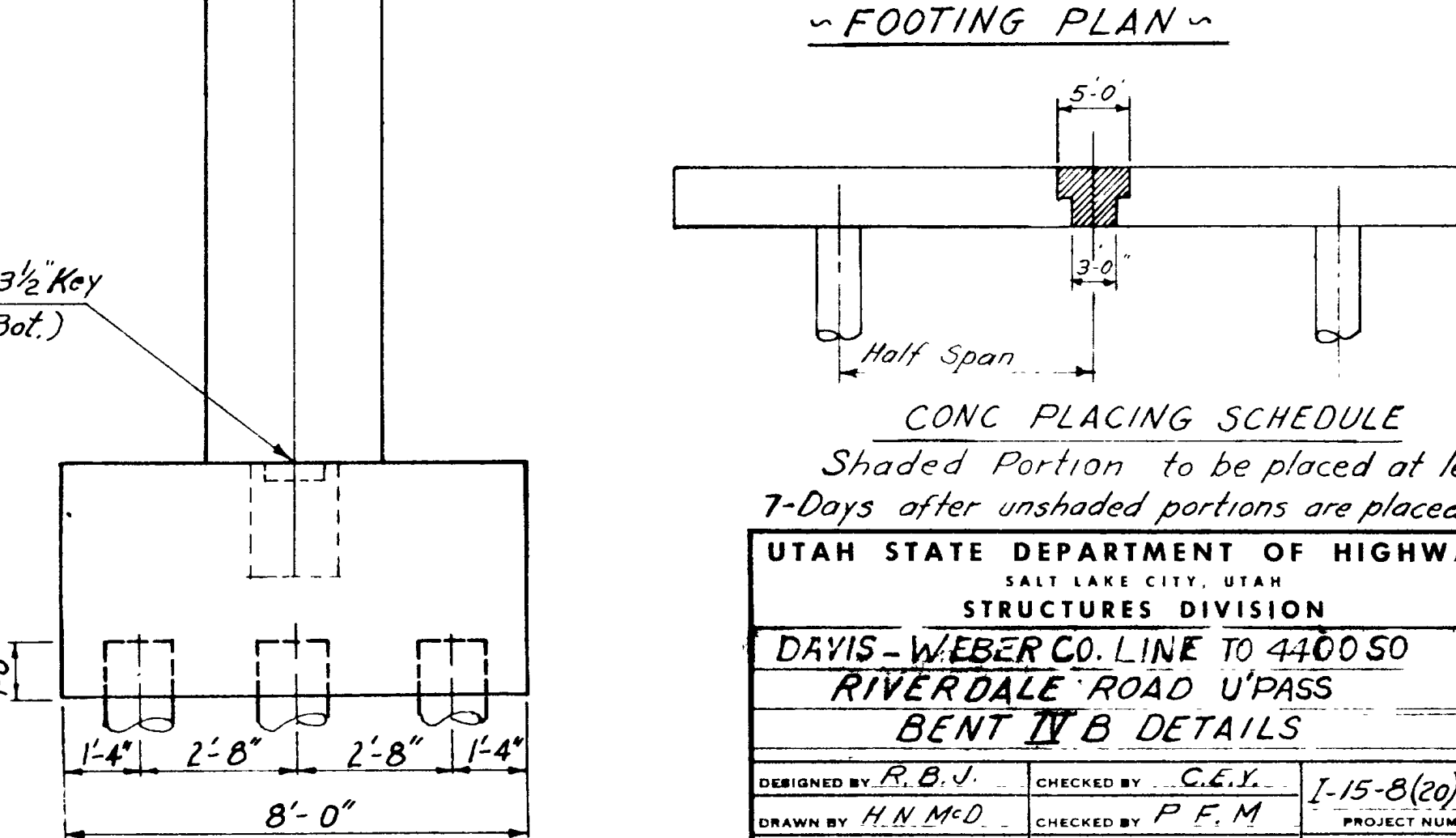
~ CAP PLAN ~



~ ELEVATION ~



~ FOOTING PLAN ~

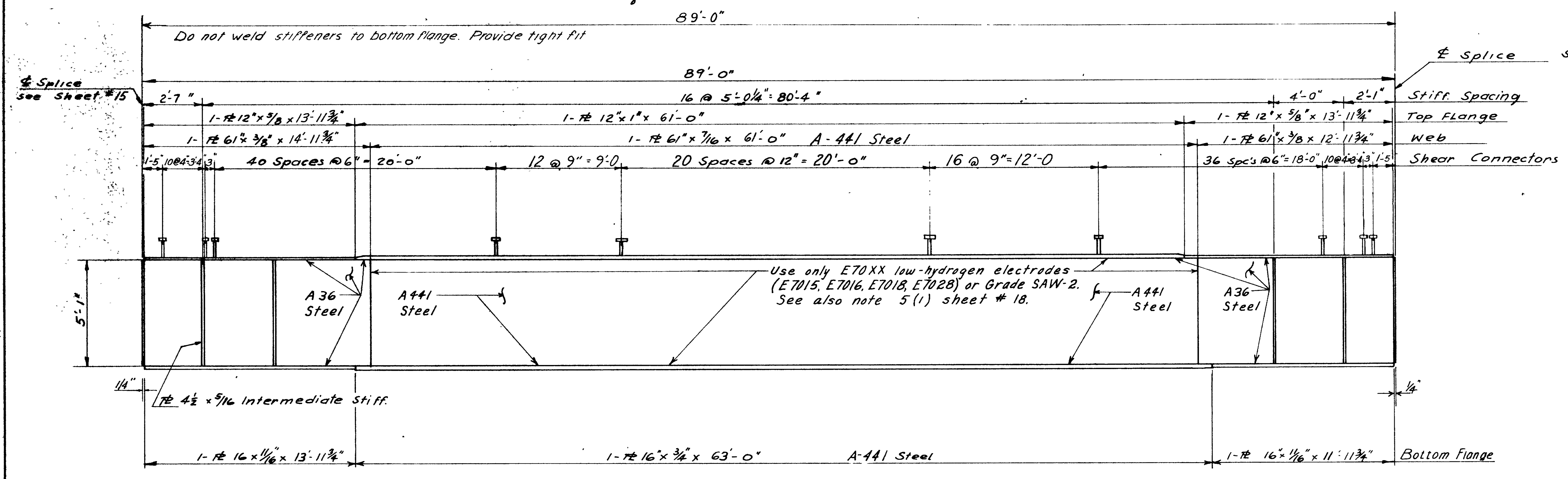
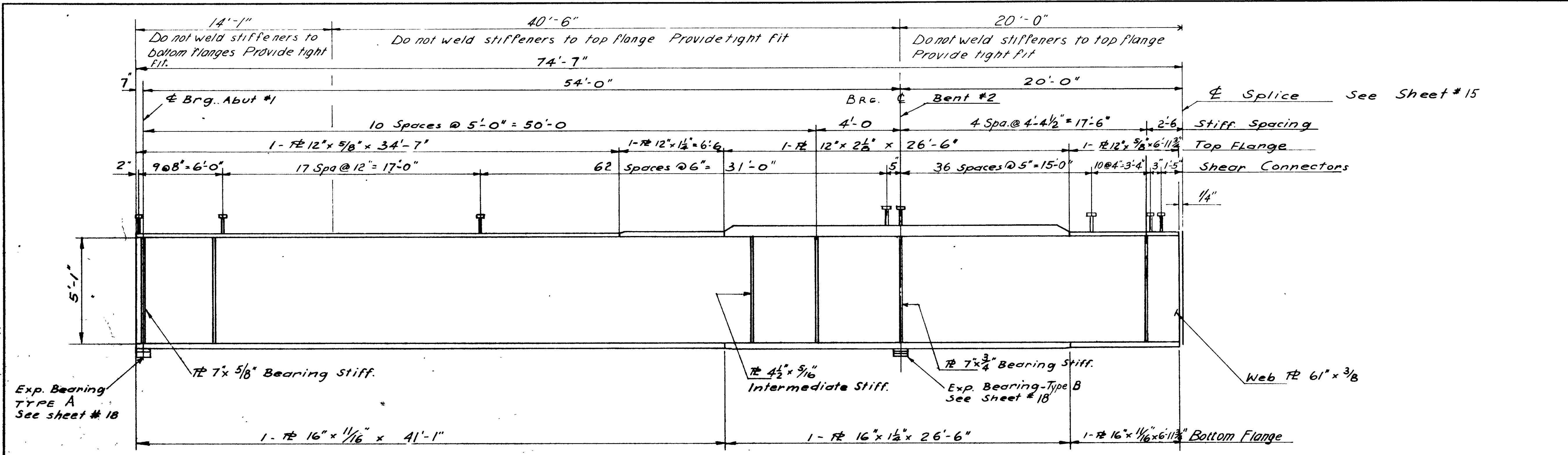


~ END ELEVATION ~

CONC PLACING SCHEDULE
 Shaded Portion to be placed at least 7-Days after unshaded portions are placed.

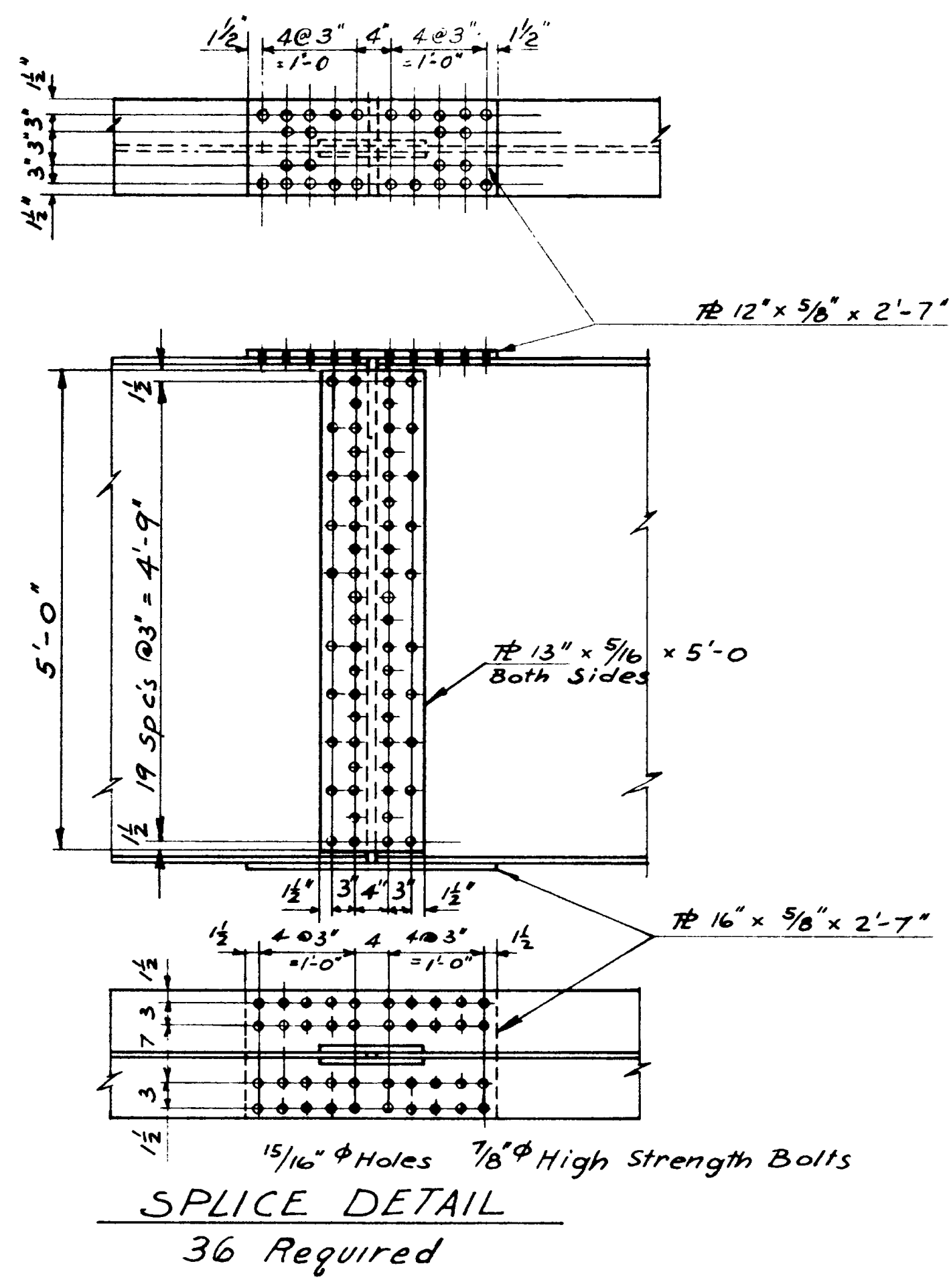
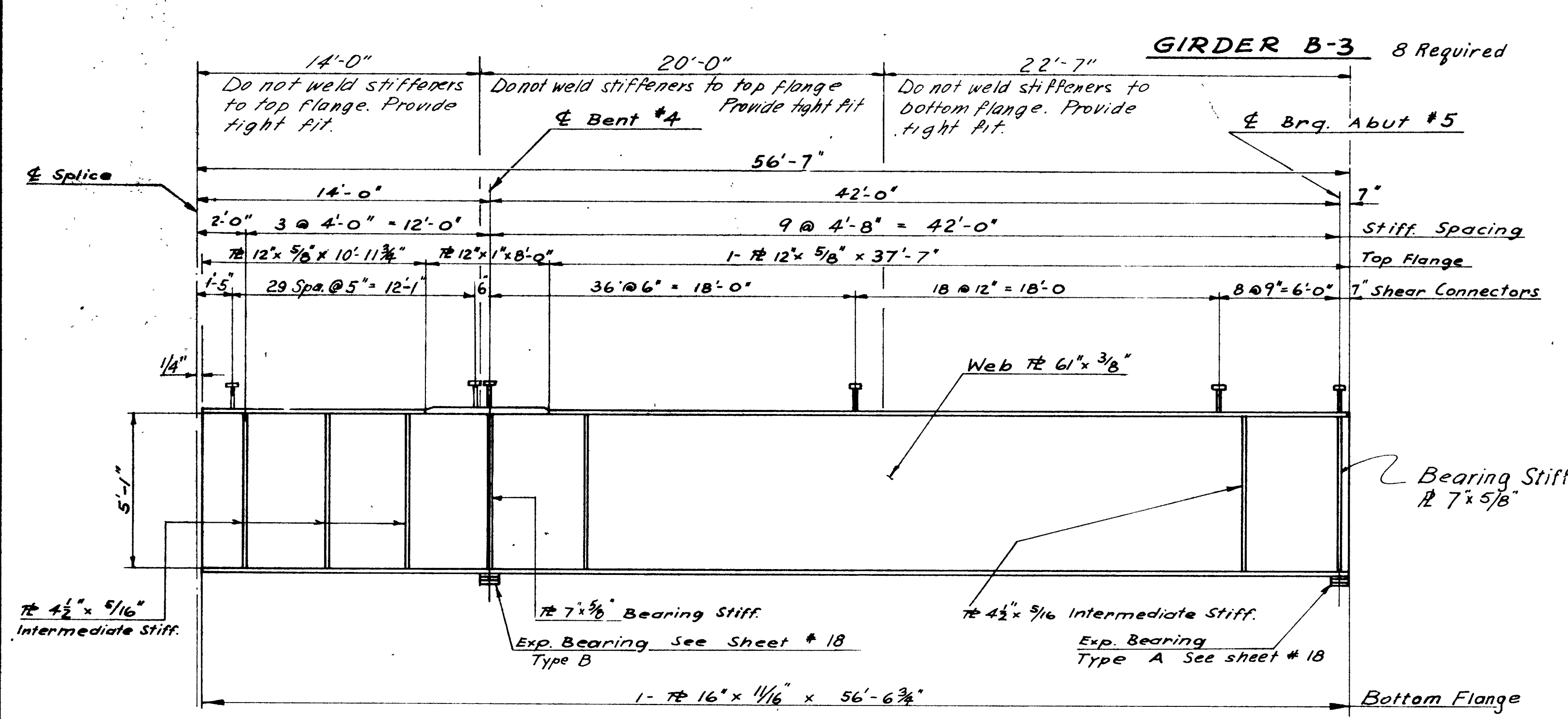
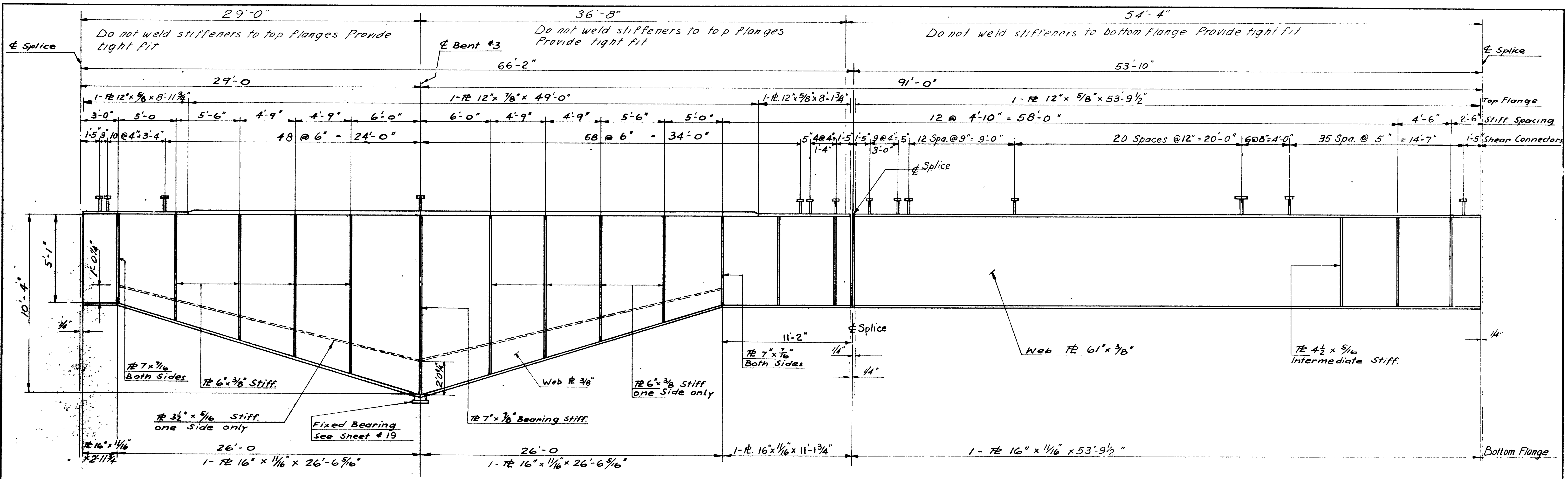
UTAH STATE DEPARTMENT OF HIGHWAYS		
SALT LAKE CITY, UTAH		
STRUCTURES DIVISION		
DAVIS-WEBER CO. LINE TO 4400 SO		
RIVERDALE ROAD U'PASS		
BENT IV B DETAILS		
DESIGNED BY R.B.V.	CHECKED BY C.E.Y.	PROJECT NUMBER I-15-8(20)334
DRAWN BY H.N.M.O.	CHECKED BY P.F.M.	STATION 904+43.8
QUANTITIES BY R.C.P.	CHECKED BY C.E.Y.	GROUP LEADER
APPROVAL RECOMMENDED BY Joseph E. Wray	DATE	COUNTY Weber
APPROVED	DATE	COUNTY

REVISIONS
 DATE
 REVISOR
 DATE
 REVISOR
 DATE
 REVISOR
 DATE
 REVISOR
 DATE
 REVISOR
 DATE



NOTE
 For Diaph Stiff. spacing see Framing Plan
 All intermediate Stiffeners
 1- \angle 4 $\frac{1}{2}$ " x $\frac{5}{16}$ " on one side only, except
 As Noted.
 All plates A-36 Steel except as noted.
 All bearing stiffeners on both sides.

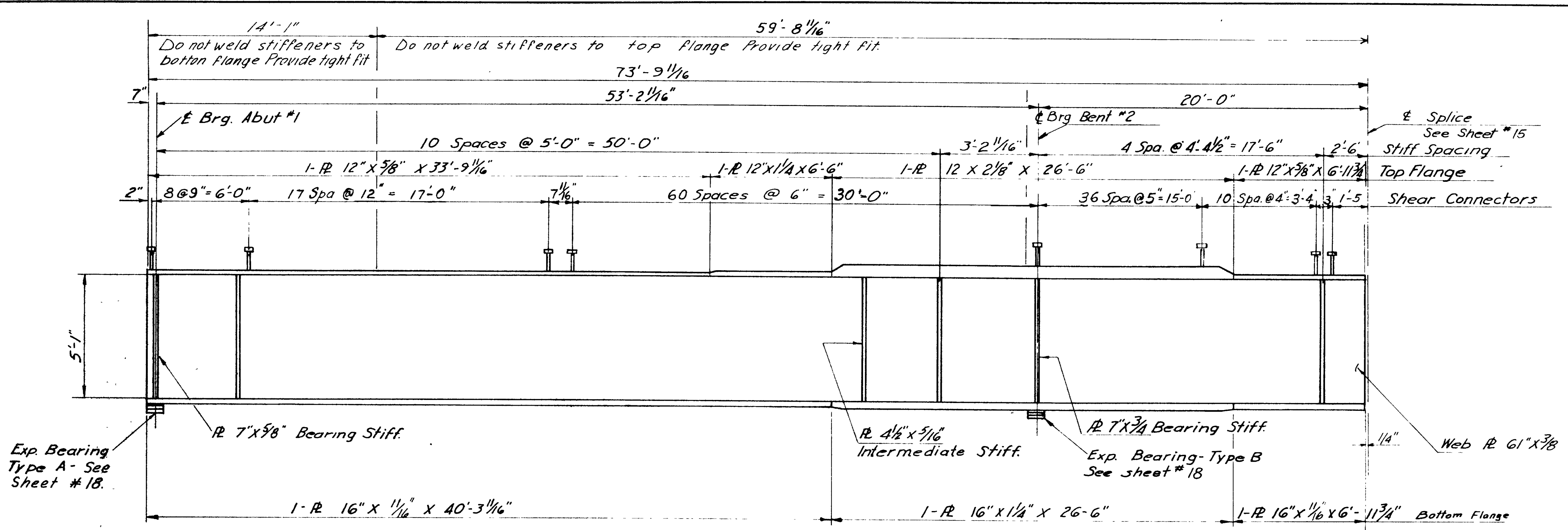
UTAH STATE DEPARTMENT OF HIGHWAYS		
SALT LAKE CITY, UTAH		
STRUCTURES DIVISION		
DAVIS - WEBER CO. LINE TO 4400 SOUTH		
RIVERDALE ROAD U'PASS		
GIRDER DETAILS		
DESIGNED BY R.B.V.	CHECKED BY C.E.Y.	PROJECT NUMBER I-15-B(20)334
DRAWN BY M.M.G.	CHECKED BY H.N.M-D	PROJECT NUMBER
QUANTITIES BY ACP	CHECKED BY C.E.Y.	STATION 904+43.8
APPROVAL RECOMMENDED BY Joseph E. West	GROUP LEADER	STATION
APPROVED S-10-11-03	DATE	Webster COUNTY
BR NO	DRG NO C-453	14 OF 29



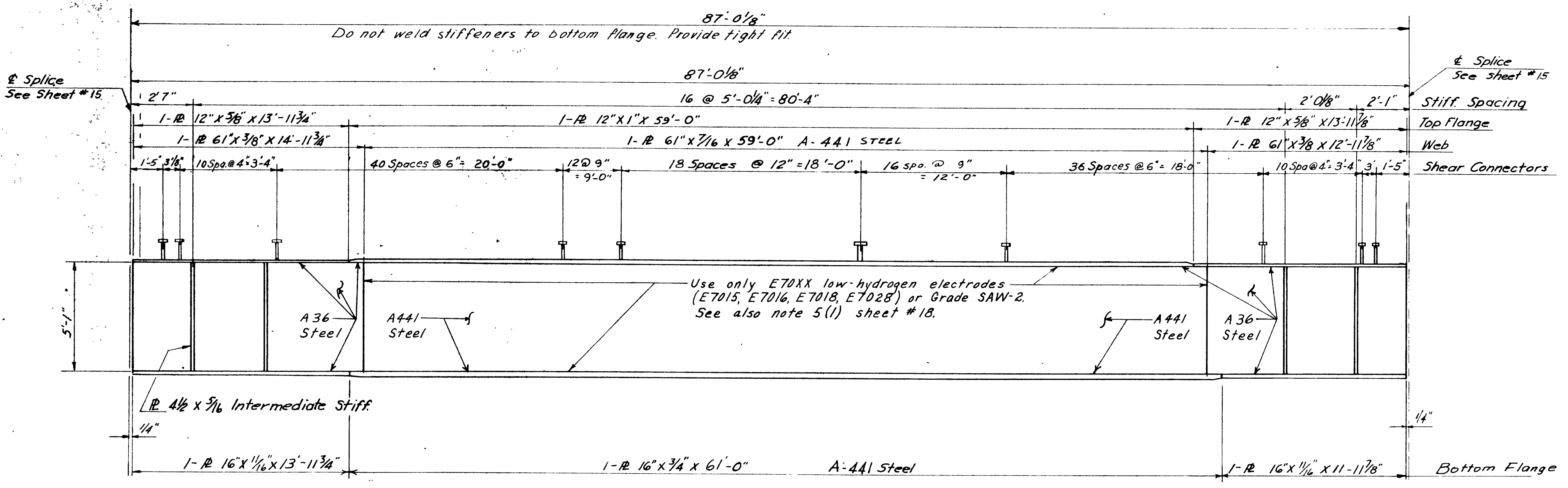
NOTE
 For Diaph Stiff. Spacing see Framing Plan
 All intermediate stiffeners
 1-# 4 1/2 x 5/16 on one side only, Except
 As Noted.
 All bearing stiffeners on both sides

UTAH STATE DEPARTMENT OF HIGHWAYS			
SALT LAKE CITY, UTAH			
STRUCTURES DIVISION			
DAVIS-WEBER CO. LINE TO 4400 SOUTH			
RIVERDALE ROAD, U.PASS			
GIRDER DETAILS			
DESIGNED BY R.B.U.	CHECKED BY C.E.Y.	I-15-B(20)33A	
DRAWN BY W.M.G.	CHECKED BY H.N.M.D.	PROJECT NUMBER	
QUANTITIES BY ACP	CHECKED BY C.E.Y.	904+43.8	
APPROVAL	RECOMMENDED BY	GROUP LEADER	STATION
APPROVED Sep 10, 1963	D.C. [Signature]	CHIEF STRUCTURAL ENG.	Weber
BR NO	DRG NO C-453	15 OF 29	

REVISIONS:
 1. DATE
 2. DATE
 3. DATE
 4. DATE
 5. DATE



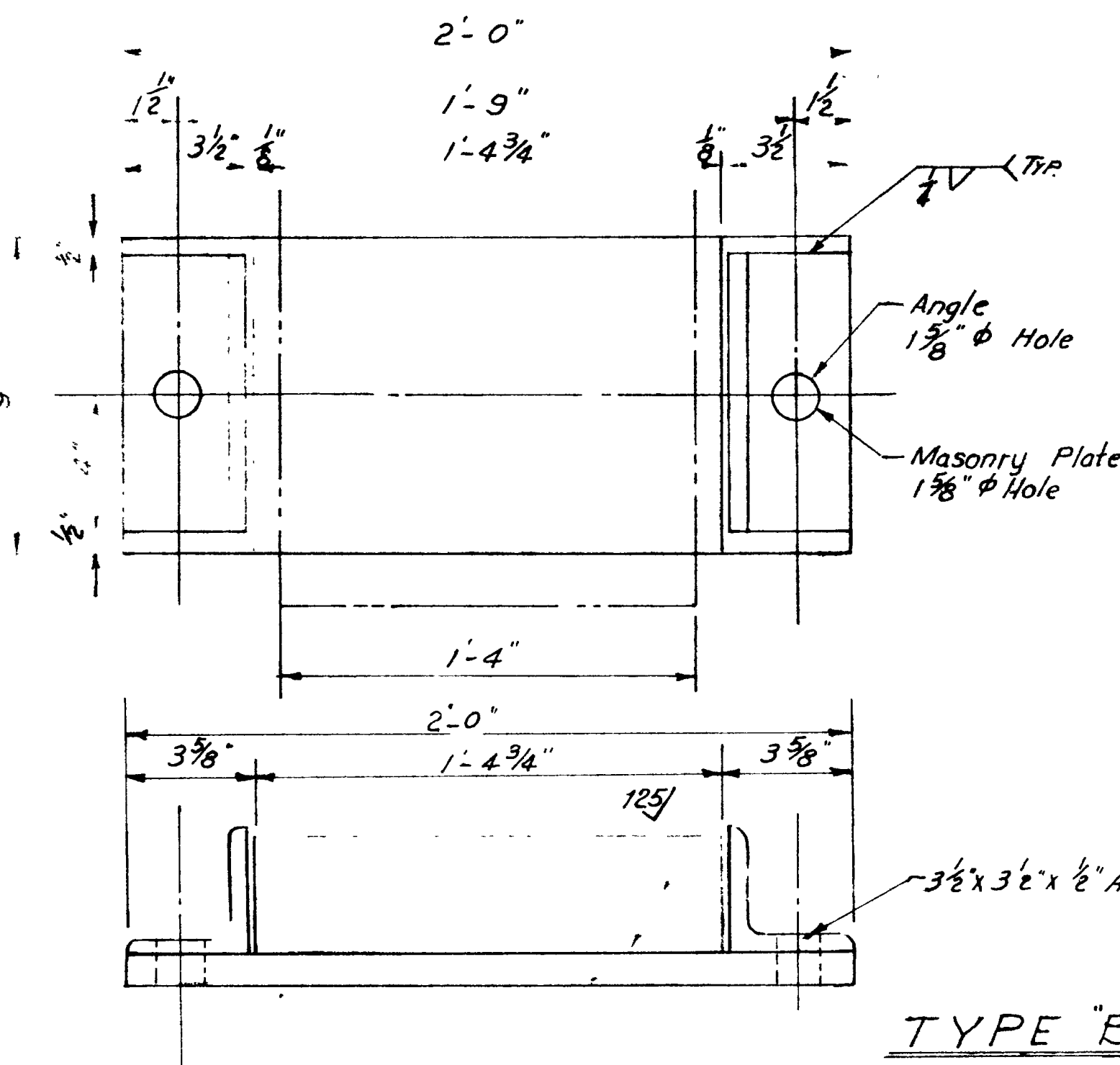
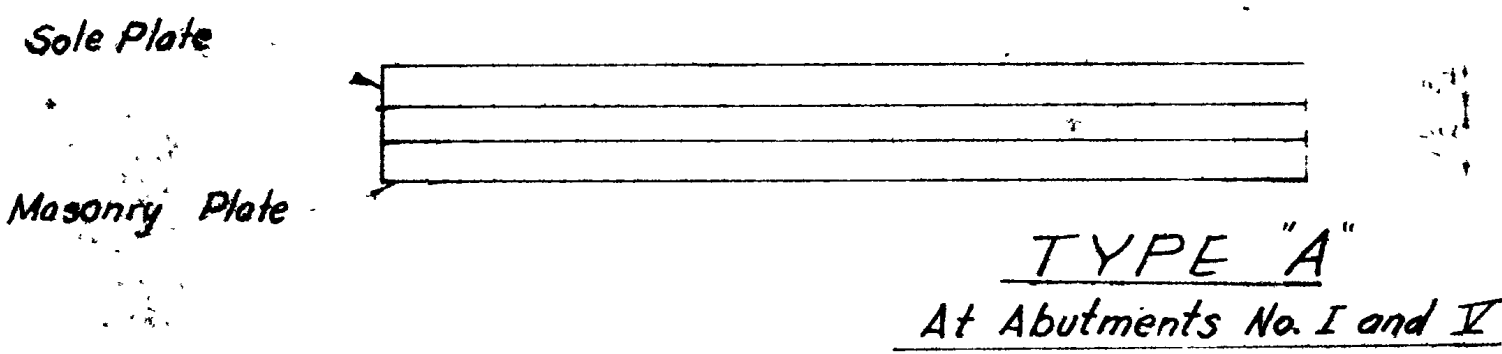
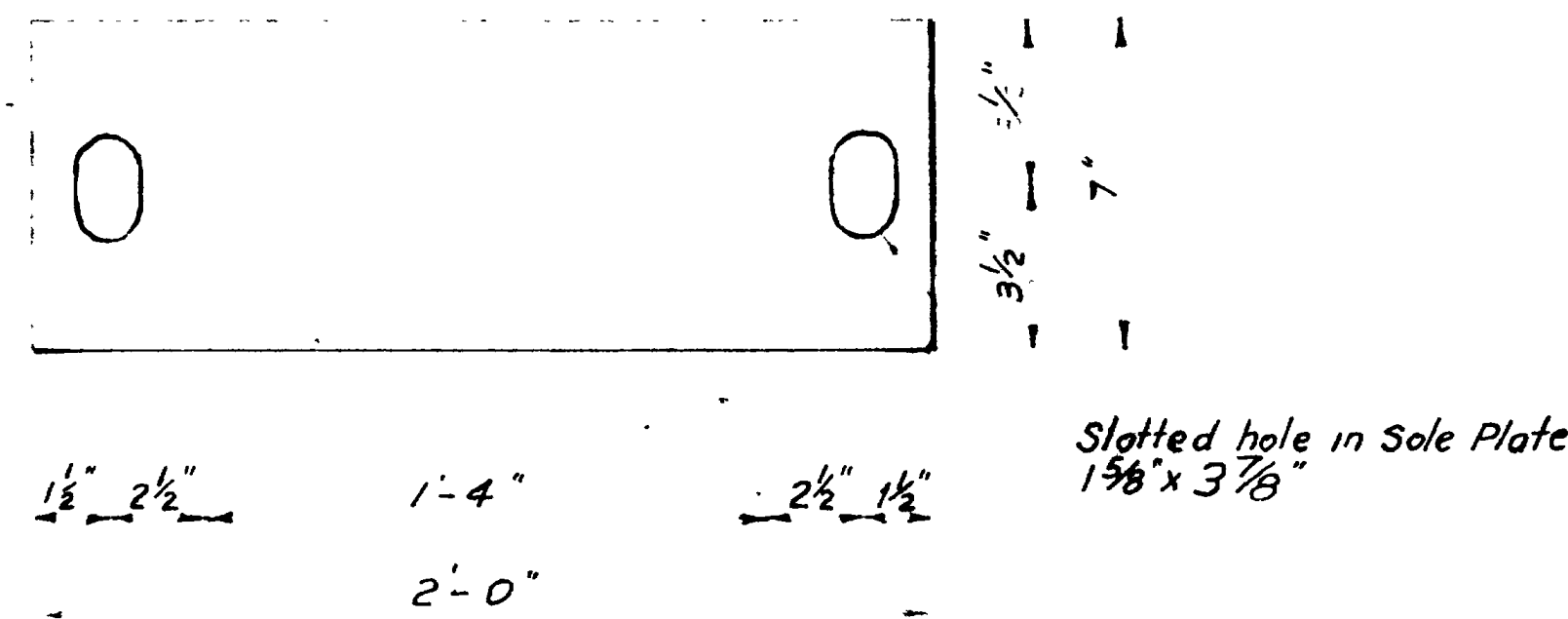
GIRDER B-1A
1 Required



GIRDER B-2A
1 Required

NOTE:
For Diaph. Conn. spacing see Framing Plan. All intermediate stiffeners 1 R 4 1/2" x 5/16" on one side only except as noted.
All bearing stiffeners on both sides.
All plates A-36 STEEL except as noted

UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS-WEBER Co LINE to 4400 So. RIVERDALE ROAD U' PASS GIRDER DETAILS			
DESIGNED BY RBJ	CHECKED BY C.E.Y.	PROJECT NUMBER 1-15-8(20)334	
DRAWN BY DCA	CHECKED BY H.N.McD	STATION 904+438	
QUANTITIES BY A.C.P.	CHECKED BY C.E.Y.	COUNTY Weber	
APPROVAL REF. CHG. 7/19/63	DATE 7/19/63	BY Joseph E. West	DATE
APPROVED Sept 10 1963	DATE	BY D.L. Sanzani	DATE
NO.	BY	DATE	REMARKS
REVISIONS			
BR NO.	DR. NO.	C-453	16 OF 29



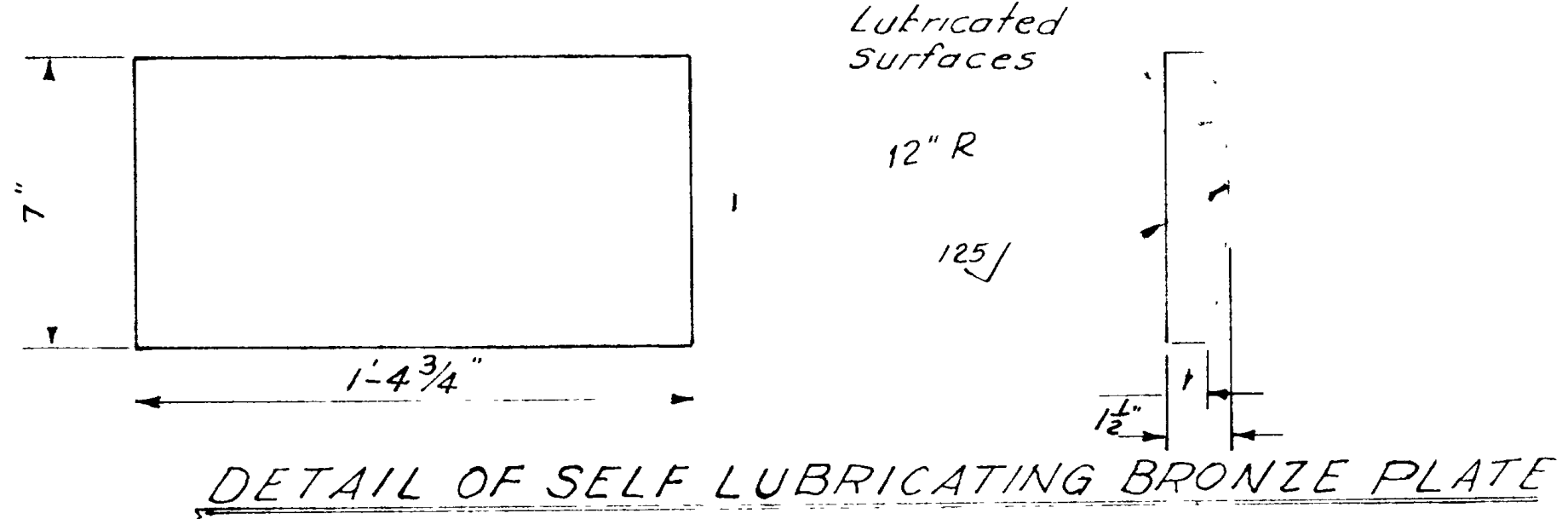
Note:
Type "B" bearing unit shall be fabricated from Hi-Tin Cast Bronze conforming to A.S.T.M. B 22, Alloy B or Rolled Bronze Alloy conforming to A.S.T.M. B 100, Alloy 1.

AT TYPE "A" BEARING

AT TYPE "B" BEARING

ANCHOR BOLT

NOTE Bridge Seat Bearing Areas (2" larger than Masonry Plate) shall be finished high and rubbed or ground to grade within 1/16".
No grouting.



See
Beam Sheets
for welding
instructions

STRUCTURAL STEEL NOTES

- 1- Structural steel for welding shall conform to the Specifications for Structural Steel for Welding of A.S.H.O. M-165 (ASTM. A-441) or M-183 (ASTM A-36)
- 2- The methods of joint preparation for welding shown on the plans are based on the use of manually shielded arc welding. In the event that automatic or semi-automatic submerged arc processes are used, the Contractor may submit alternative methods of joint preparation for approval of the Engineer.
- 3- Strength Fillet weld or alternate shall be the minimum sized welds required by the Standard Specifications of the A.W.S. for thickness of the material joined, unless otherwise specified.
- 4 All seal weld shall be minimum 3/16" fillet.
- 5- For welding ASTM A36, carbon steel, electrodes conforming to either the E60XX or the E70XX series of Tentative Specifications for Mild Steel Arc-Welding Electrodes (AWS Designation A5.1; ASTM Designation A233) shall be used for A36 steel in thickness of 3/4" or less. Only E70XX low-hydrogen electrodes shall be used on thicknesses of A36 steel over 3/4".

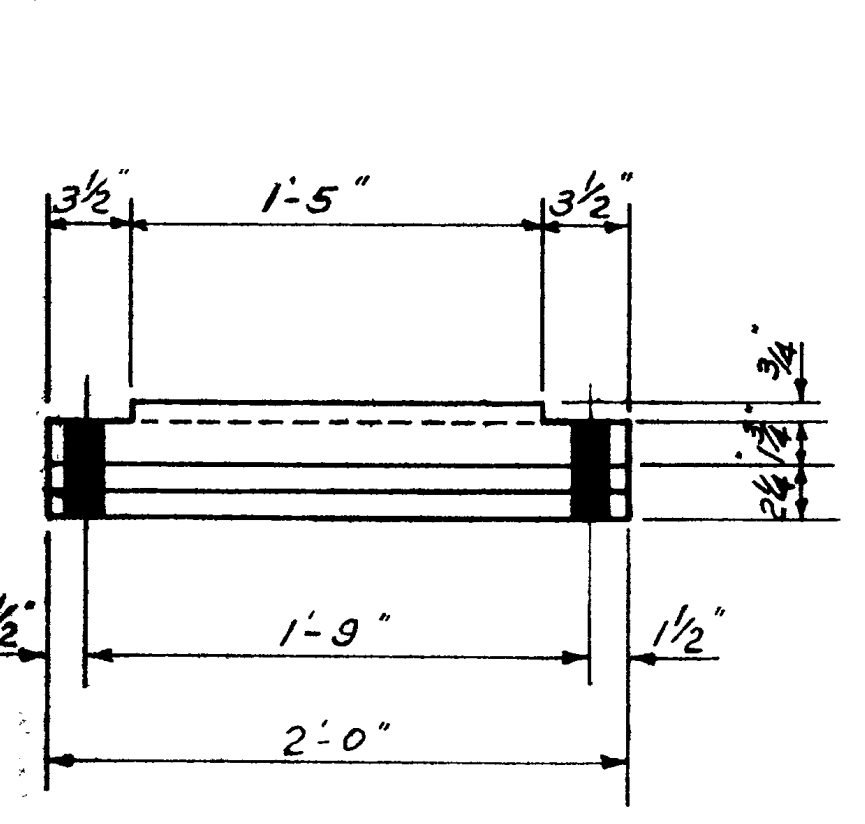
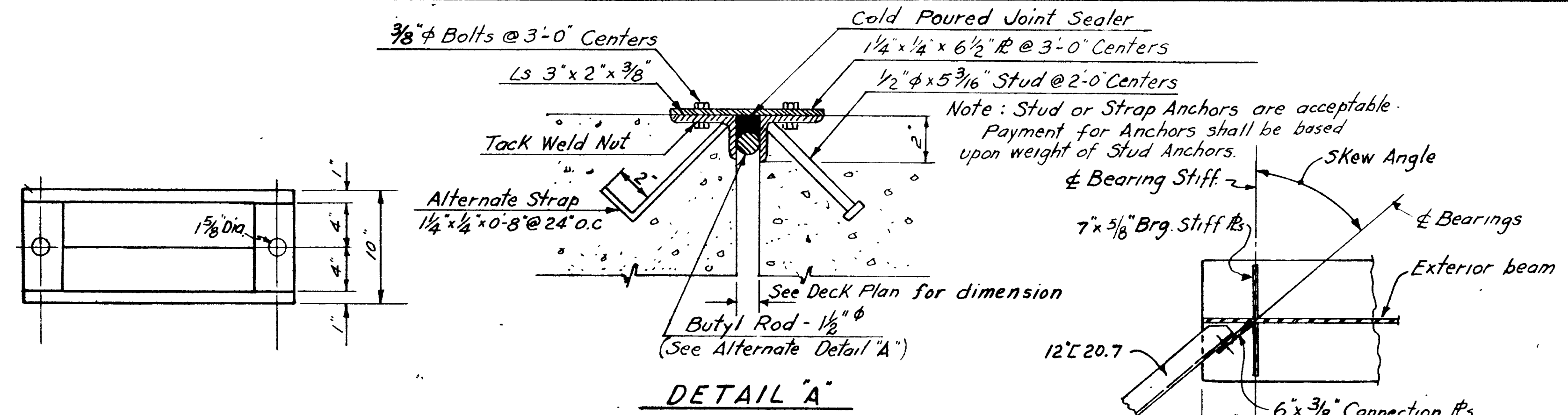
Thickness of thickest part at point of welding	Minimum Preheat and Interpass Temperature (F°)	
	Shielded Metal-Arc (other than low-hydrogen electrodes)	Shielded Metal-Arc (low-hydrogen electrode and submerged-Arc)
To 3/4" incl	200	None (1)
Over 3/4" to 2" incl	Not permitted	50 (2)
Over 2"	Not permitted	150 (2)

(1) When the base metal temperature is below 32°F, preheat the base metal to at least 70°F and maintain during welding.
(2) Using only E70XX low-hydrogen electrodes.

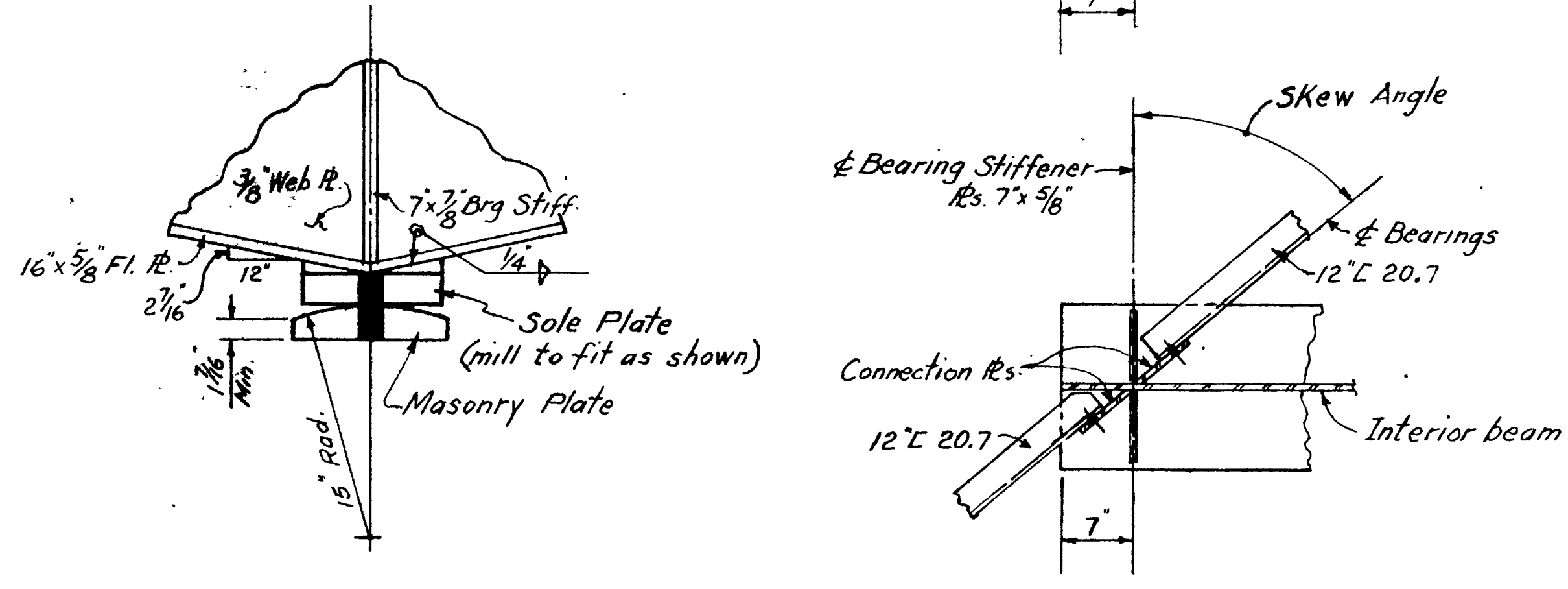
DAVIS-WEBER CO. LINE TO 4400 SO.
RIVERDALE ROAD U'PASS
STEEL DETAILS

R.B.V. CEY I-15-B(20)334
H.N.M.D. J.E.W.
ACP CEY 904+43.B
Joseph Weber
Weber

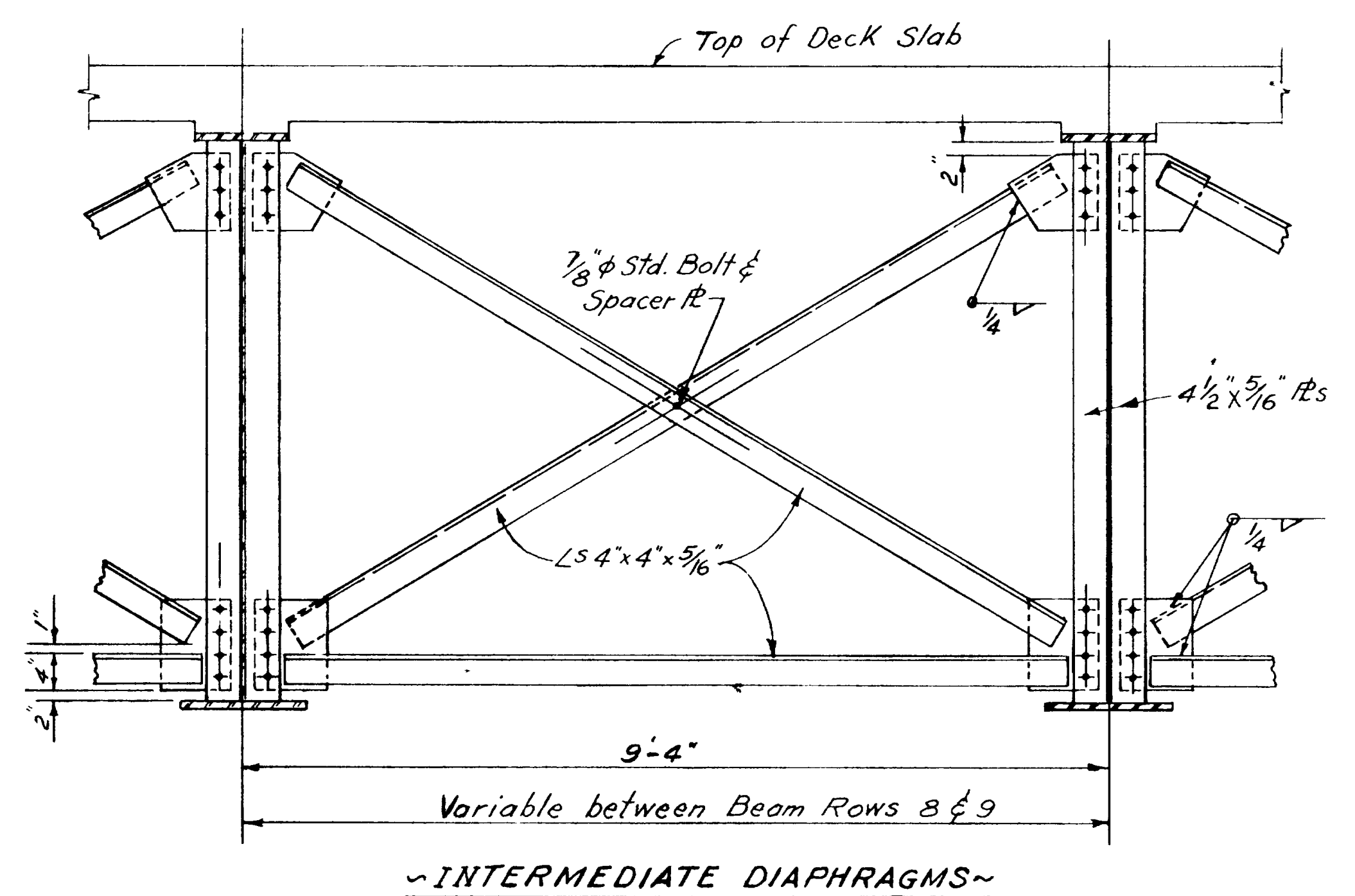
C-453 18 29



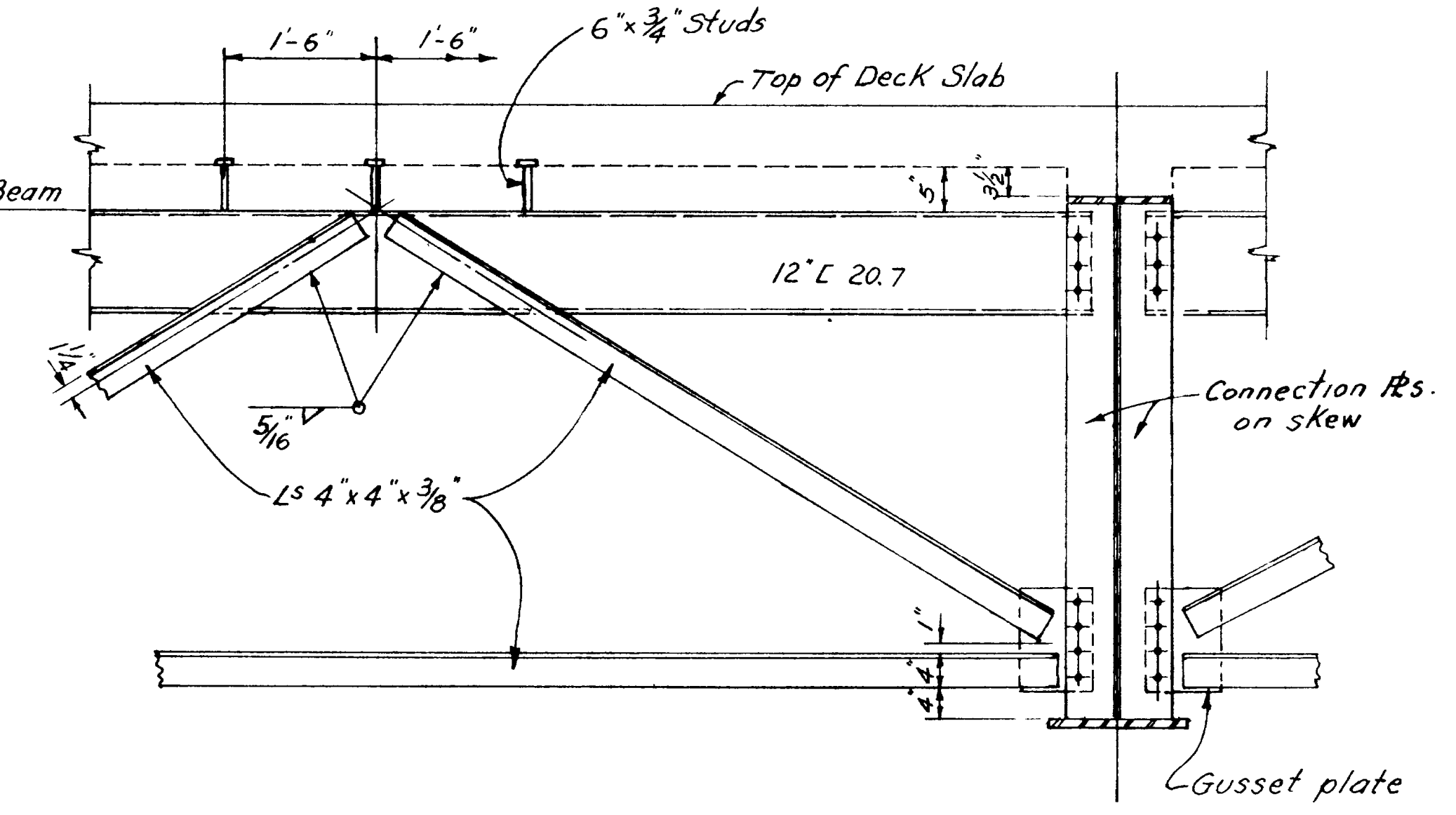
~ BEARING UNIT AT CENTER BENT ~



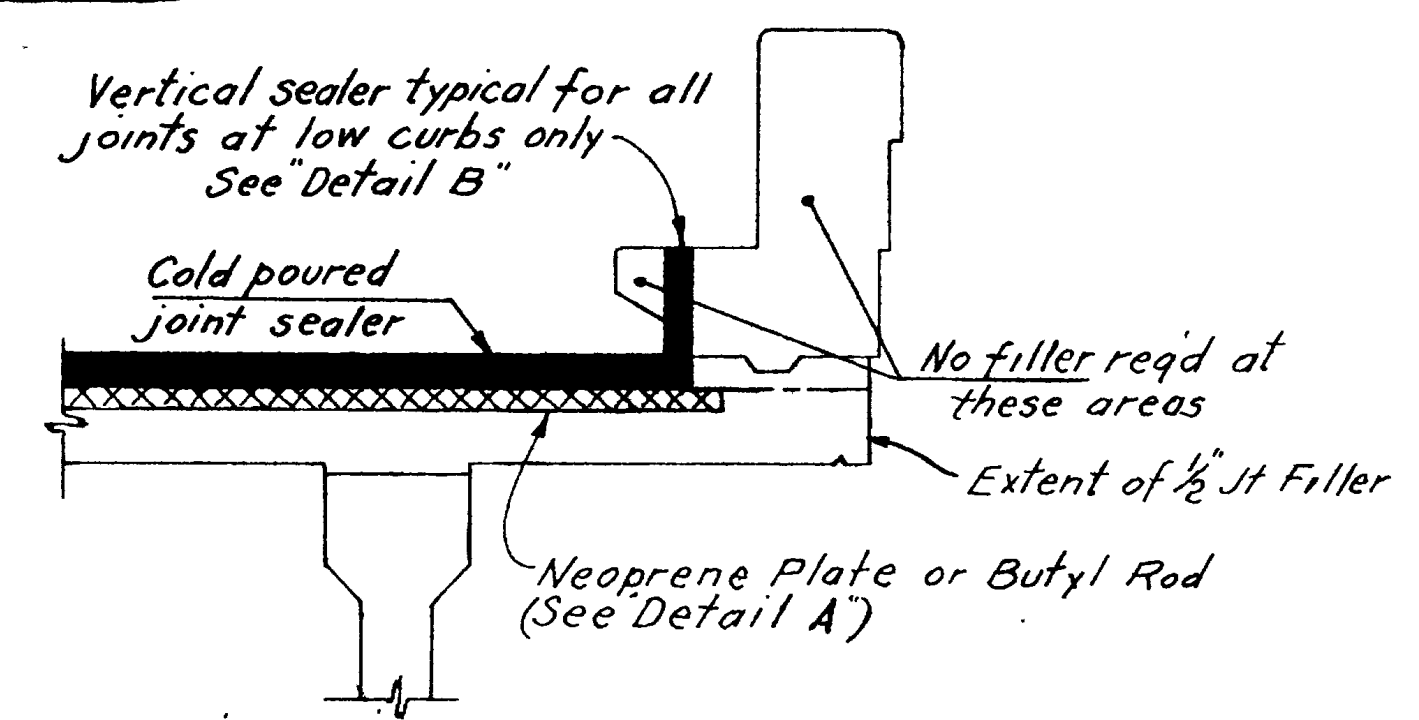
~ END DIAPHRAGM PLANS ~



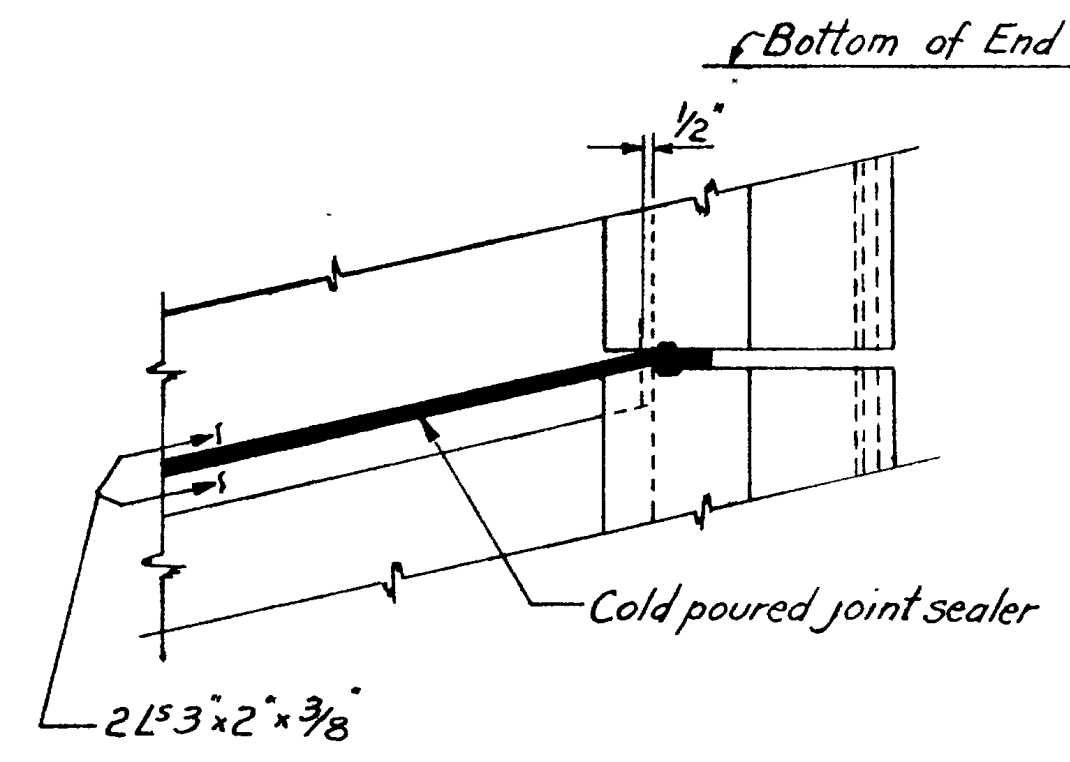
~ INTERMEDIATE DIAPHRAGMS ~



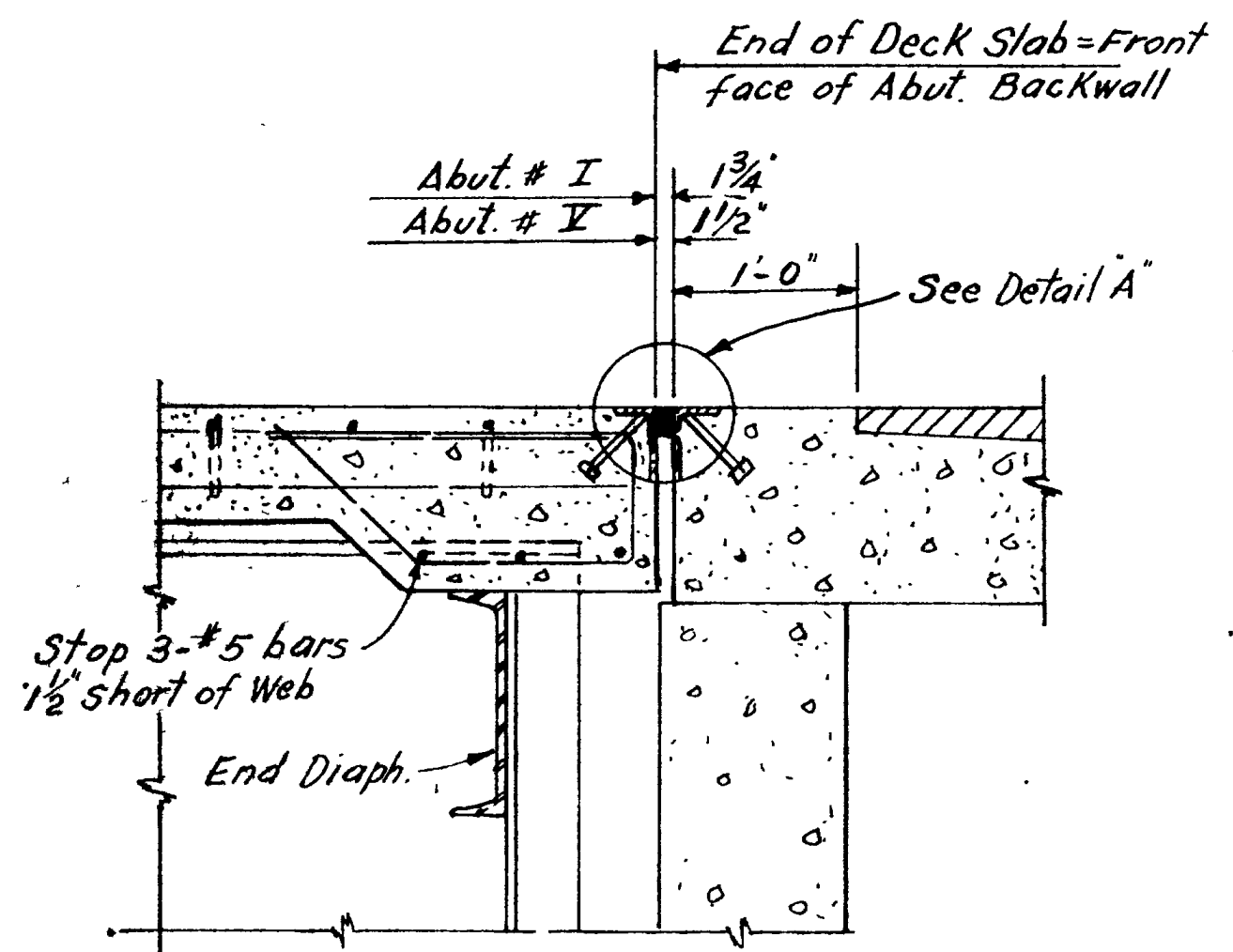
~ END DIAPHRAGMS AT ABUTMENTS ~



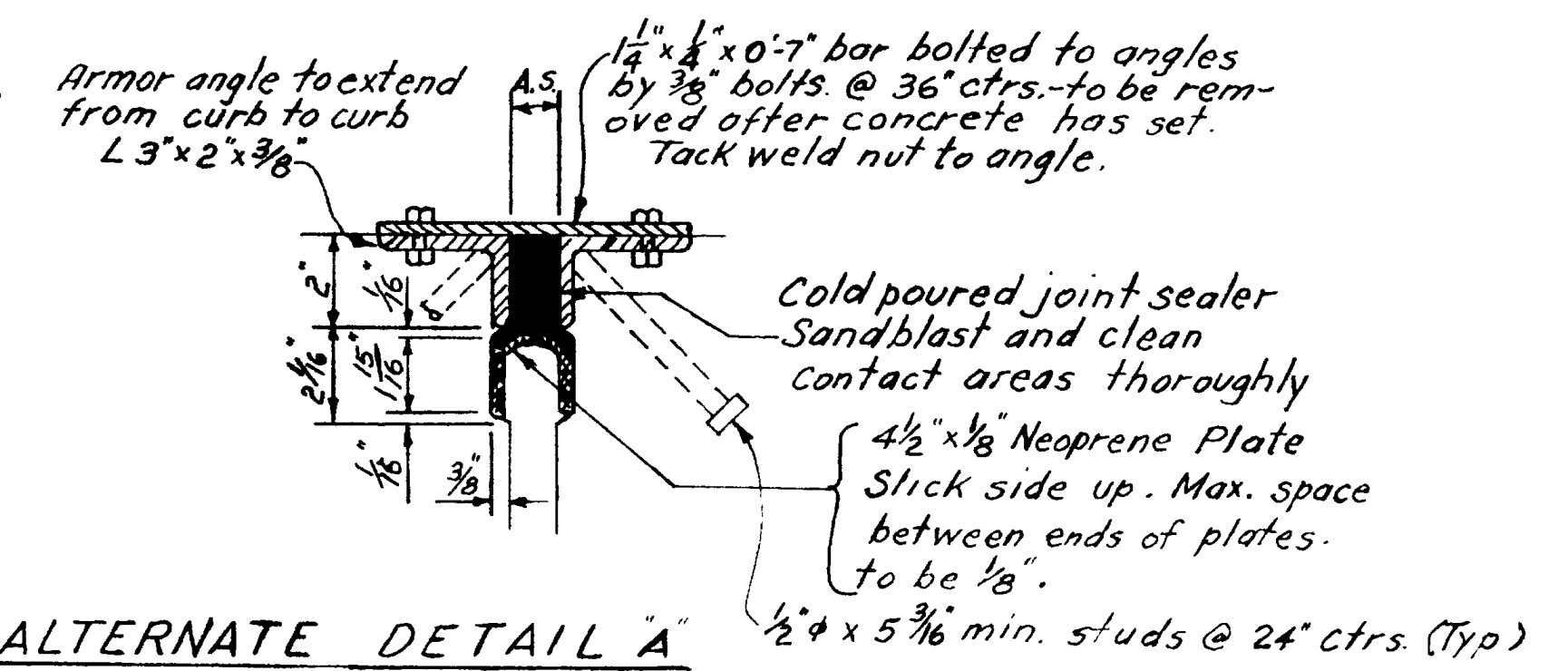
SECTION AT JOINT



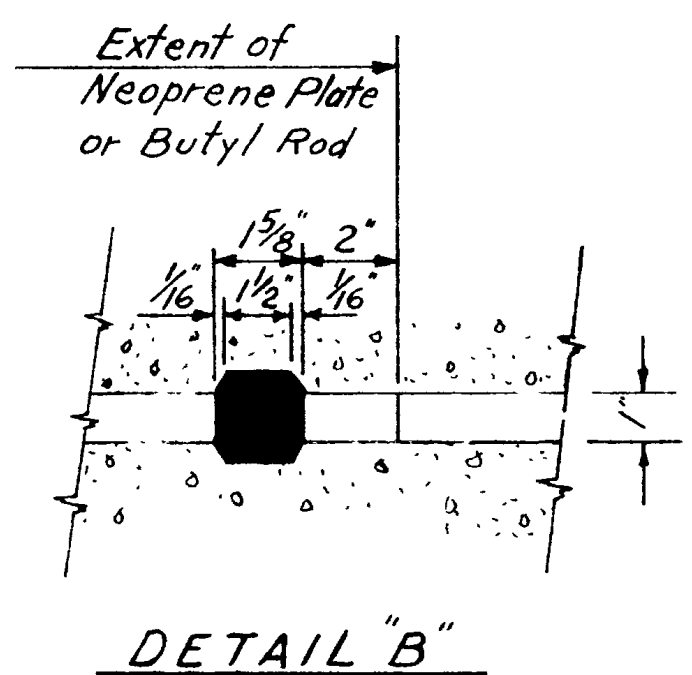
PLAN AT JOINT
(Typical Both Curbs)



SECTION AT ABUTMENTS
EXPANSION BEARING
(Normal to Backwall & Diaphragm)

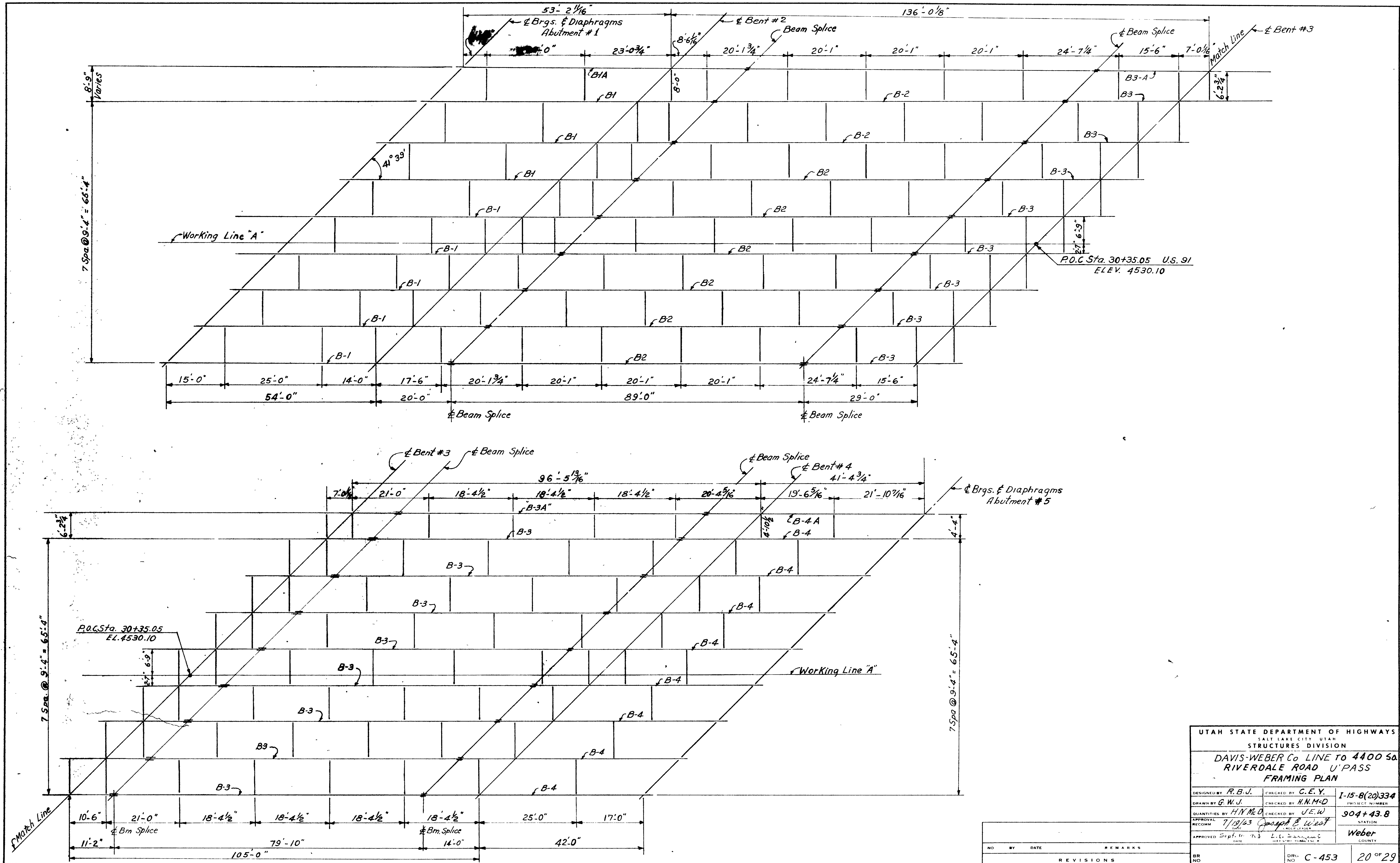


ALTERNATE DETAIL A



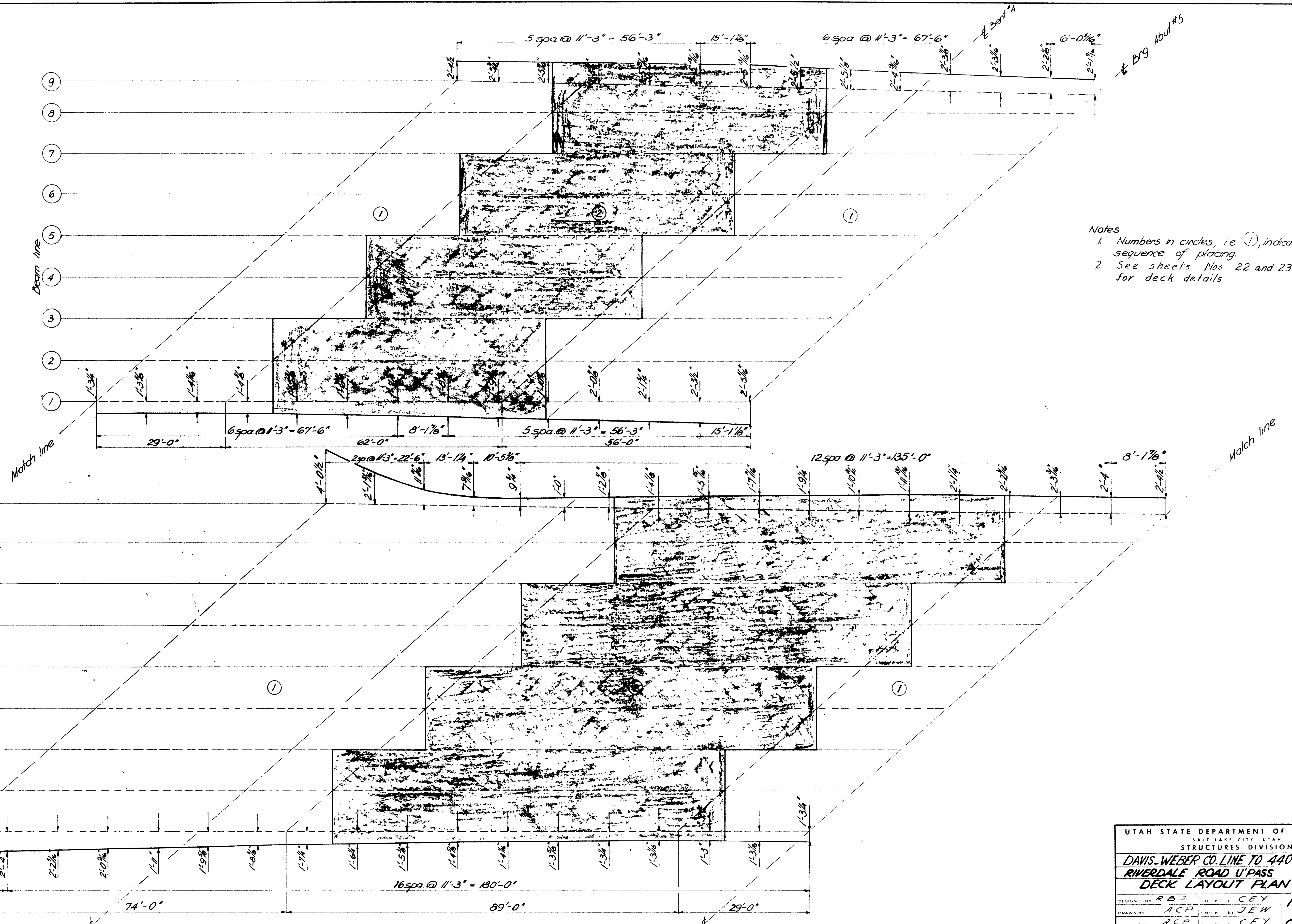
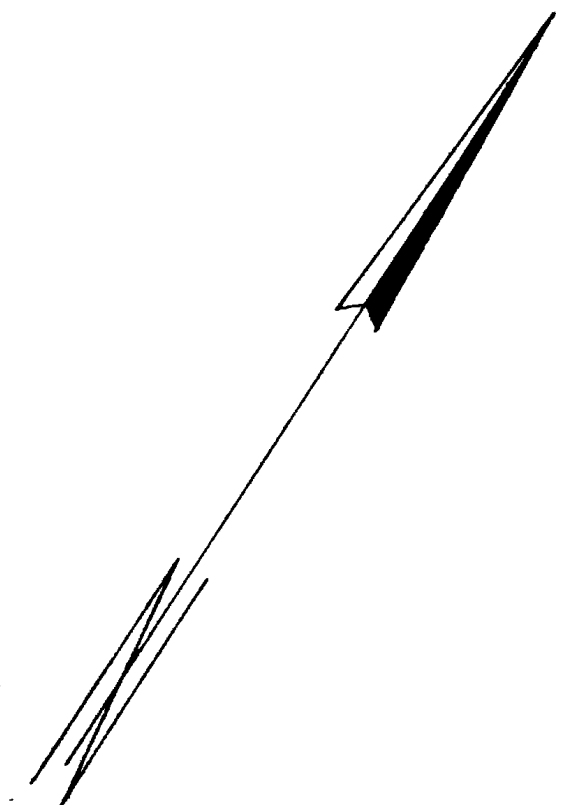
DETAIL B

UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS-WEBER Co LINE TO 4400 SO. RIVERDALE ROAD STEEL DETAILS			
DESIGNED BY R.B.J.	CHECKED BY C.E.Y.	PROJECT NUMBER I-15-B(20)334	
DRAWN BY H.N.McD.	CHECKED BY J.E.W.	STATION 904+43.8	
QUANTITIES BY ACP	CHECKED BY C.E.Y.	DRAWN BY Joseph E. West	
APPROVAL 7/10/63	D.L. Sargent	Weber COUNTY	
APPROVED Sept 10, 1963	D.L. Sargent	DATE	
NO.	BY	DATE	REMARKS
REVISIONS			
BR NO.	DR. NO.	C-453	19 OF 29



UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION		
DAVIS-WEBER CO LINE TO 4400 SA RIVERDALE ROAD U'PASS FRAMING PLAN		
DESIGNED BY R.B.J.	CHECKED BY C.E.Y.	PROJECT NUMBER I-15-8(20)334
DRAWN BY G.W.J.	CHECKED BY H.N.M.O	STATION 904+43.8
QUANTITIES BY H.N.M.O	CHECKED BY J.E.W	COUNTY Weber
APPROVAL RECOMM 7/19/63	DATE 7/19/63	DR. NO. C-453
APPROVED Sept. 11, 1963		20 OF 29

NO	BY	DATE	REMARKS
REVISIONS			



Notes
 1. Numbers in circles, i.e. ①, indicate sequence of placing.
 2. See sheets Nos 22 and 23 for deck details

Abut. #1

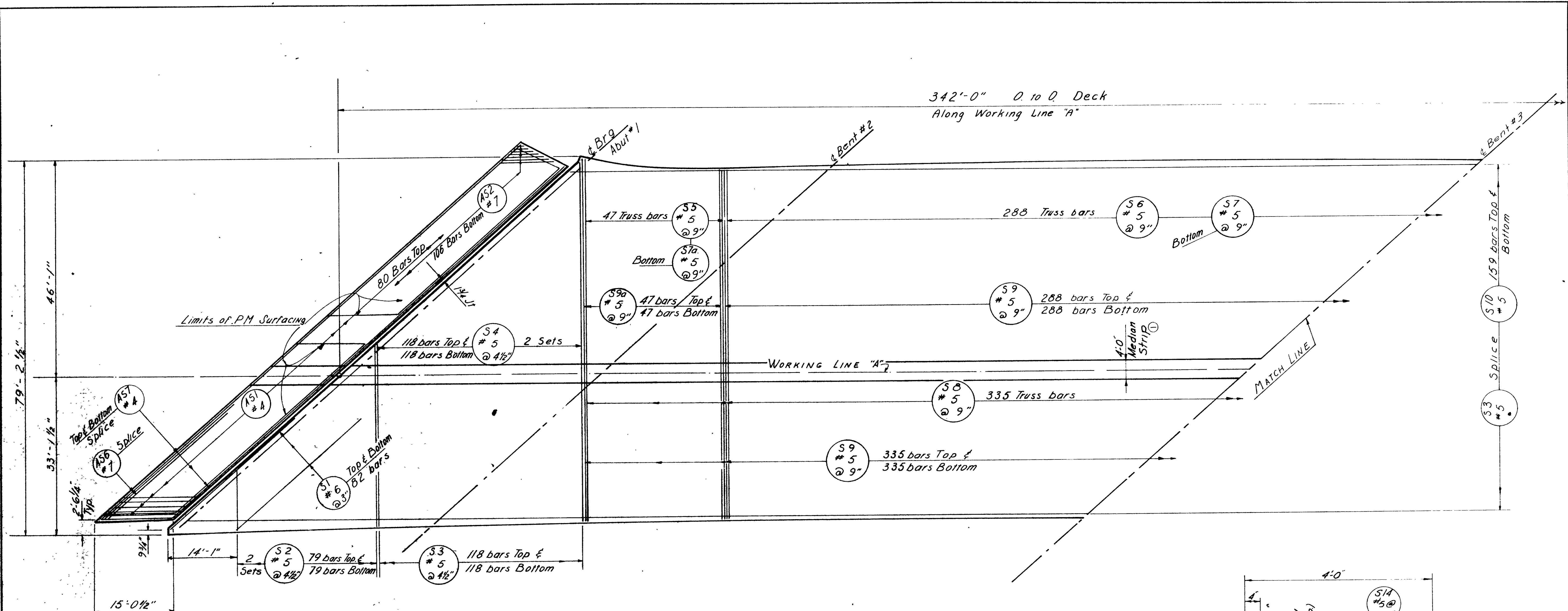
Pier #2

Pier #3

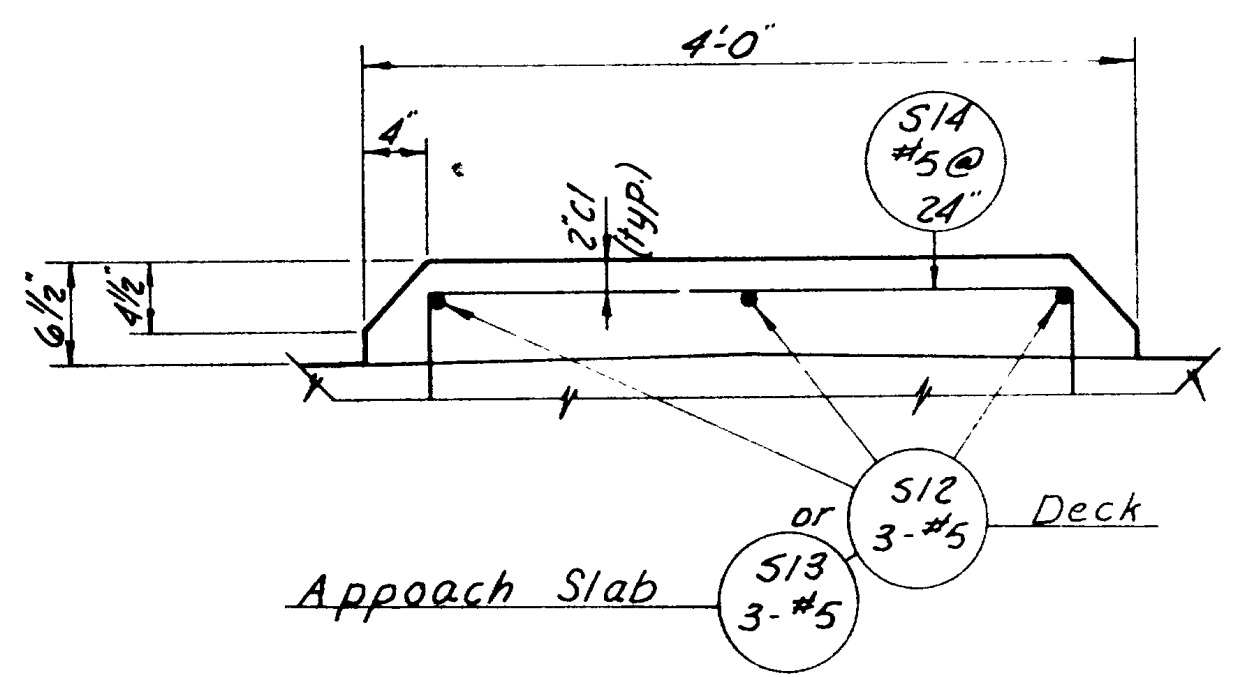
UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS, WEBER CO. LINE TO 4400 SO. RIVERDALE ROAD U'PASS DECK LAYOUT PLAN			
DESIGNED BY RBJ	CHECKED BY CEY	PROJECT NUMBER	1-15-8(20)334
DRAWN BY ACP	IN CHARGE BY JEW	FIG. NO.	904-43.8
QUANTITIES BY ACP	IN CHARGE BY CEY	DATE	1-15-8
APPROVAL	7/9/63 Joseph & West	DATE	7/9/63
APPROVED	Sept 10 1963 D. C. [Signature]	DATE	9/10/63

NO.	BY	DATE	REMARKS

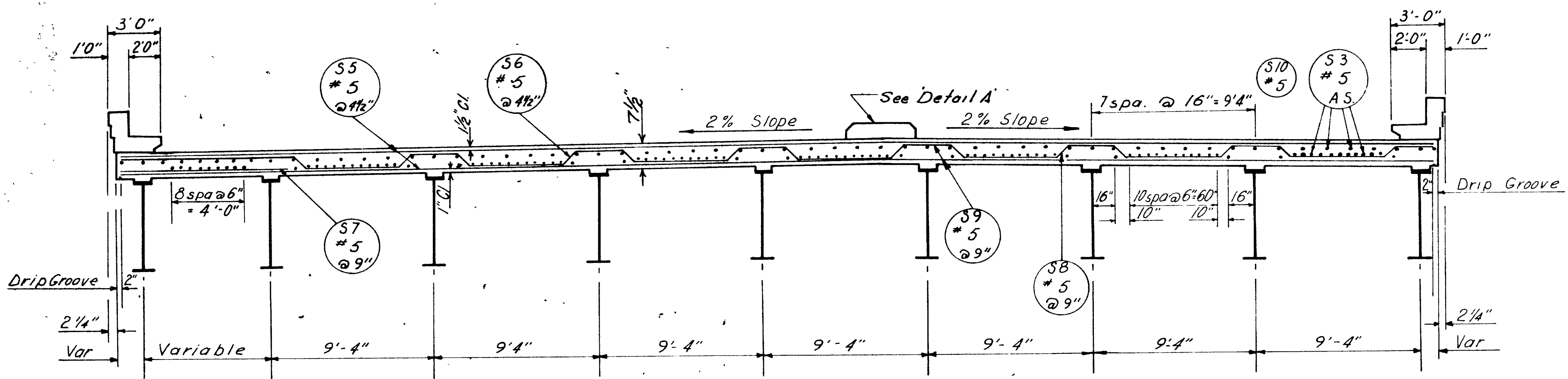
BR NO. [] DRG. NO. C-453 21 OF 29



PLAN



DETAIL A



TYPICAL SECTION

UTAH STATE DEPARTMENT OF HIGHWAYS
SALT LAKE CITY, UTAH
STRUCTURES DIVISION

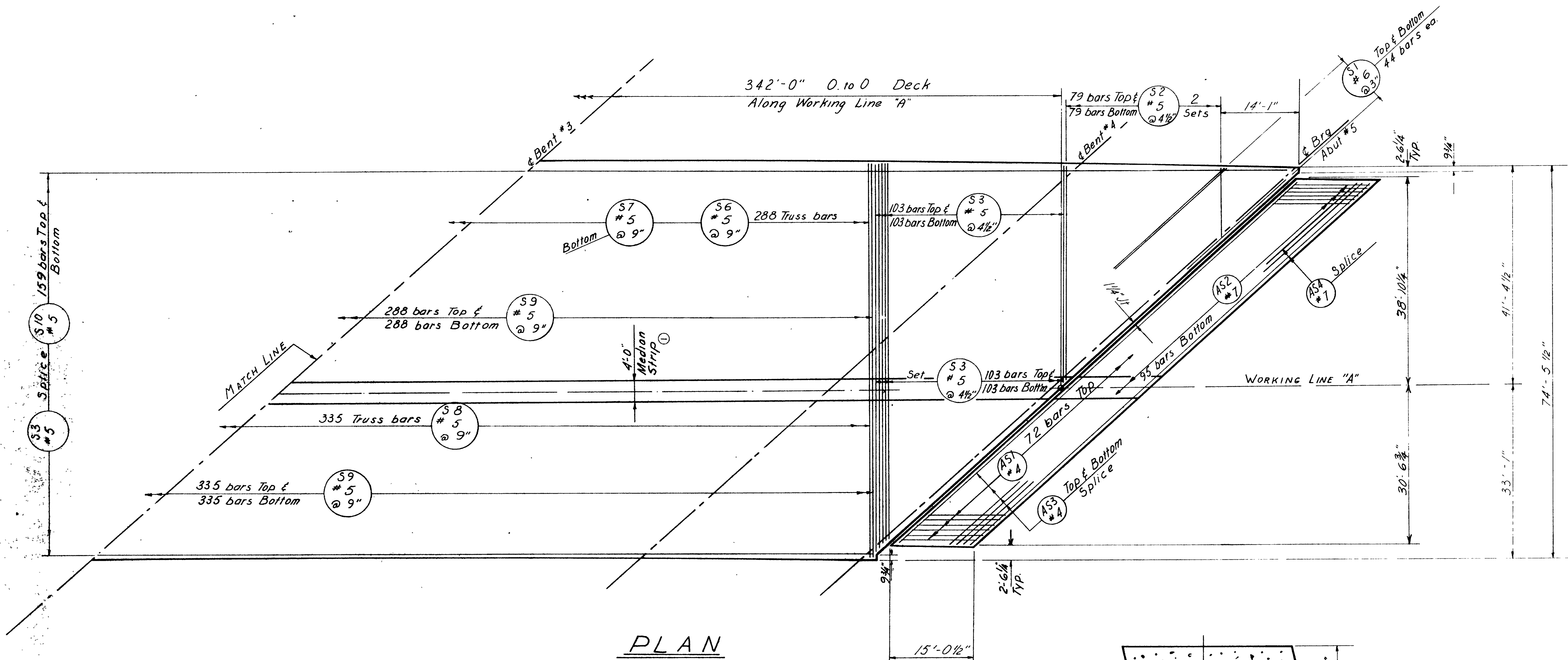
**DAVIS-WEBER CO. LINE TO 4400 SO
RIVERDALE ROAD UPASS
DECK DETAILS**

DESIGNED BY	RBV	CHECKED BY	CEY	PROJECT NUMBER I-15-B(20)334
DRAWN BY	JCM	CHECKED BY	JEW	
QUANTITIES BY	ACP	CHECKED BY	CEY	STATION 904+43.8
DATE	7/9/65	APPROVED BY	Joseph E. West	COUNTY Weber
APPROVED	Sept. 10, 1965	D. G. ...		

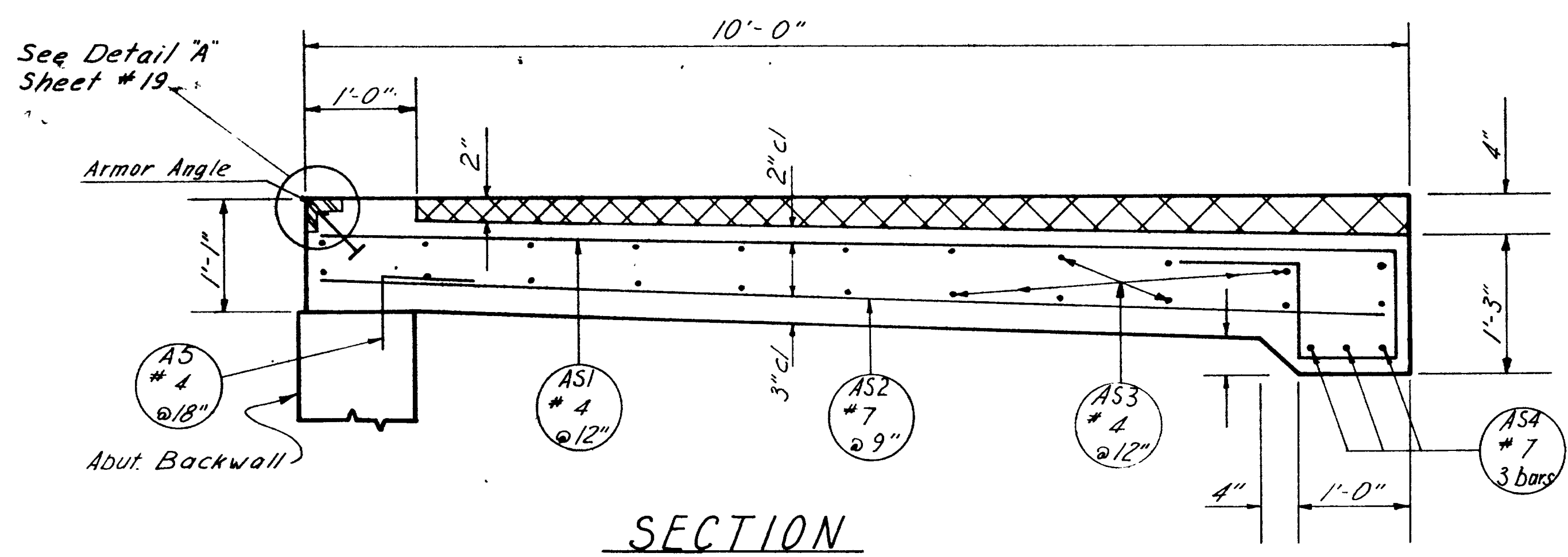
① DVB 9-20-63 Added Median Strip Detail A.

NO.	BY	DATE	REVISIONS

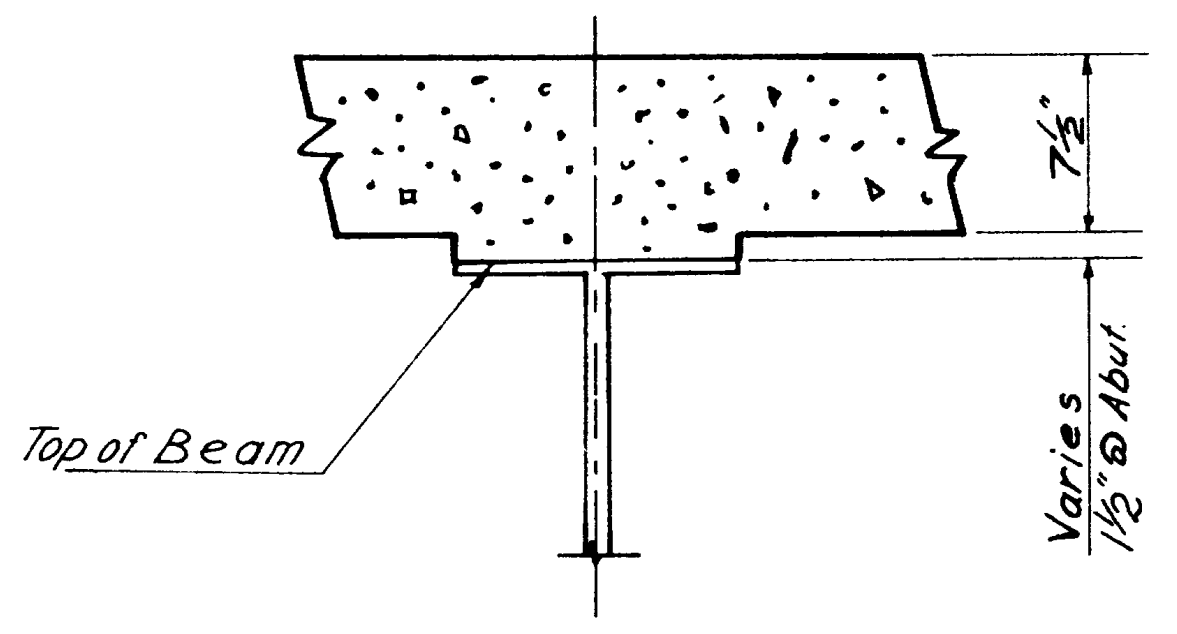
BR NO.	DWG NO.	C-453
		22 OF 29



PLAN



SECTION
APPROACH SLAB



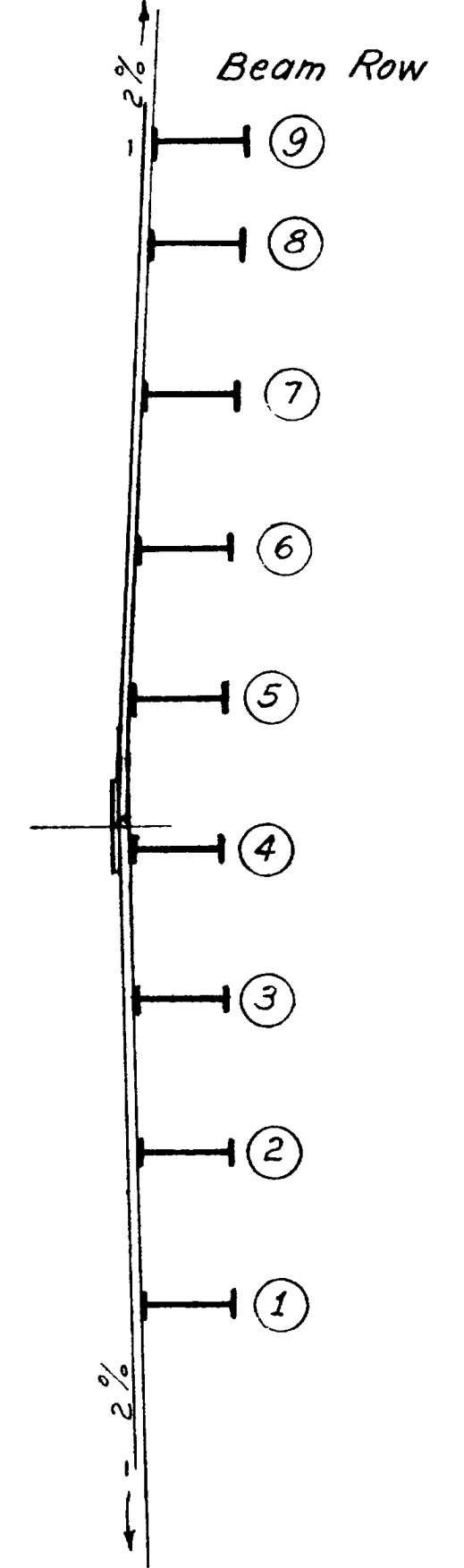
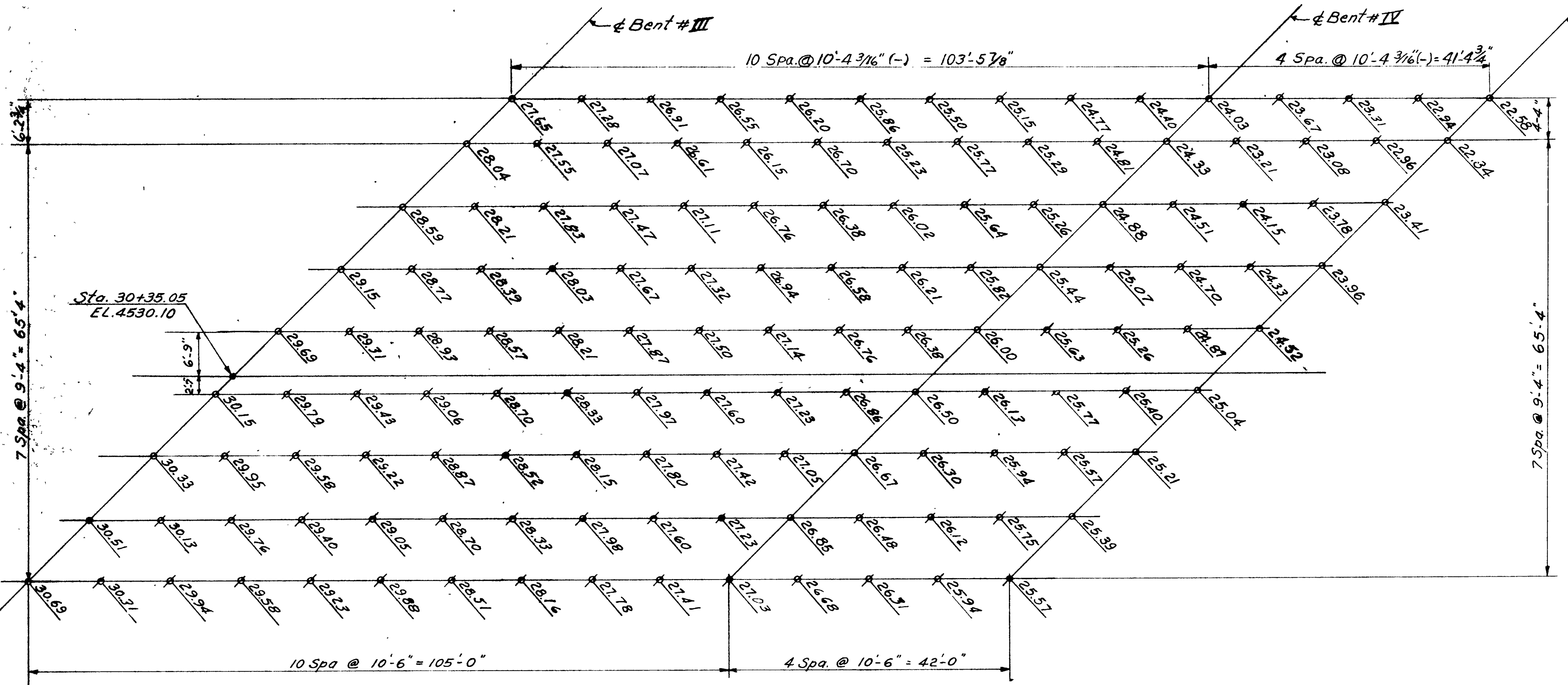
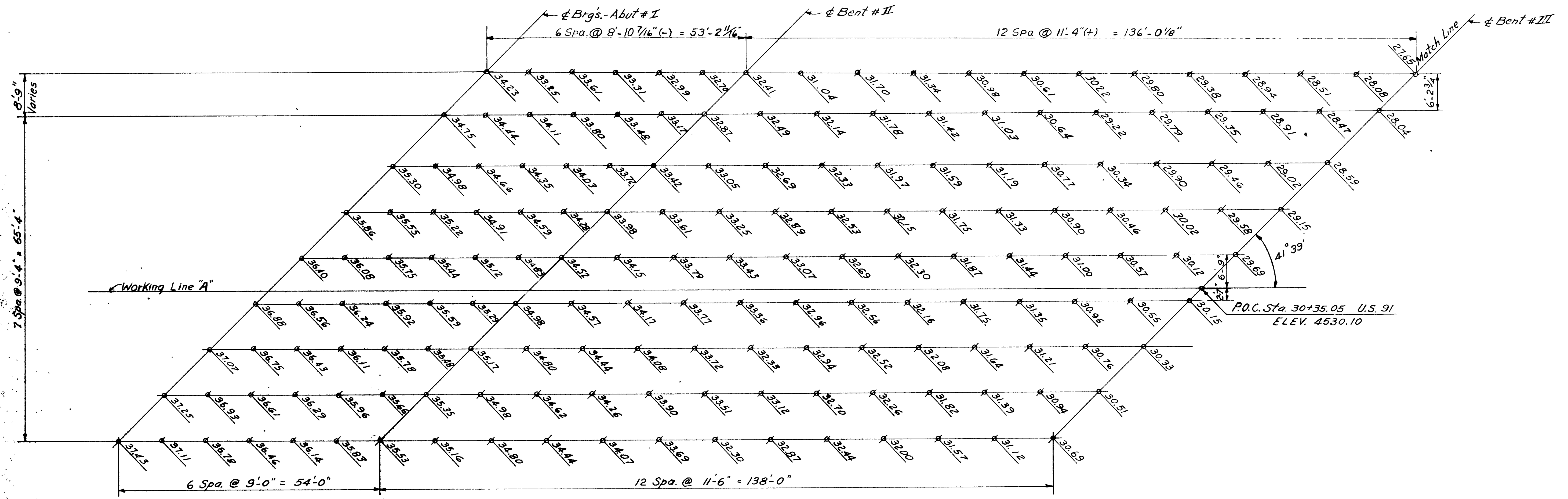
CAMBER STRIP DETAIL

UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS-WEBER Co LINE to 4400 So RIVERDALE ROAD U'PASS DECK DETAILS			
DESIGNED BY RBJ	CHECKED BY CEY	I-15-8(20)334	
DRAWN BY JCM	CHECKED BY JEW	PROJECT NUMBER	
QUANTITIES BY ACP	CHECKED BY CEY	904+43 B	
APPROVAL 7/19/43	DATE	STATION	
APPROVED Sept 10, 1943	DATE	Weber COUNTY	

① DVB 9-20-43 Added Median Strip

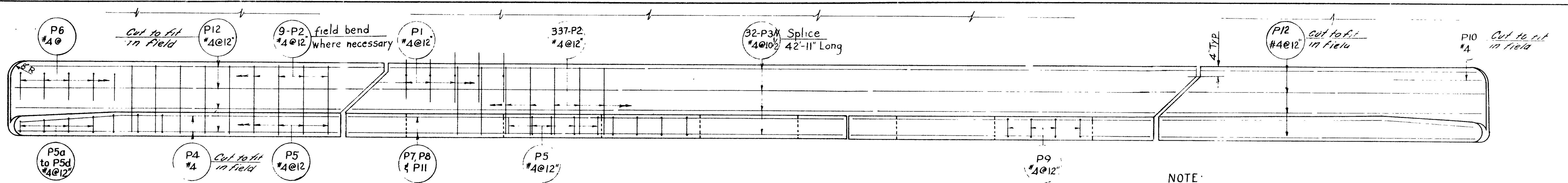
NO	BY	DATE	REMARKS
REVISIONS			

REVISOR BY DATE
 REVISOR BY DATE
 REVISOR BY DATE
 REVISOR BY DATE



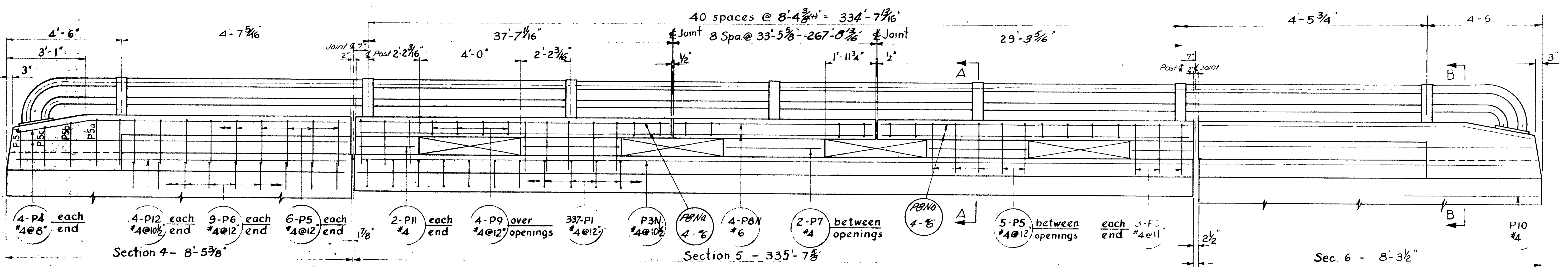
Elevations given are profile grade plus 1.25 x Slab D.L. Deflection.

UTAH STATE DEPARTMENT OF HIGHWAYS			
SALT LAKE CITY, UTAH			
STRUCTURES DIVISION			
DAVIS, WEBER CO. LINE TO 4400 SO			
RIVERDALE ROAD U/PASS			
SCREED ELEVATIONS			
DESIGNED BY R.B.V.	CHECKED BY C.E.Y.	PROJECT NUMBER 1-15-8(20)334	
DRAWN BY H.N.McD	CHECKED BY A.C.P.	STATION 904+43.8	
QUANTITIES BY H.N.McD	CHECKED BY J.E.W.	GROUP LEADER	
APPROVAL RECOMMENDED BY Joseph E. West	DATE 10/19/63	CHEF STRUCTURAL ENG.	Webster COUNTY
BR NO	DRG NO C-453	24 of 29	

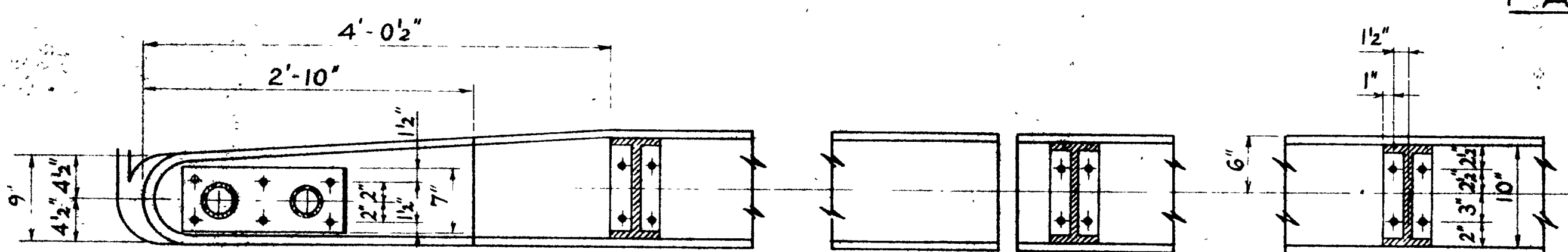


PARAPET PLAN

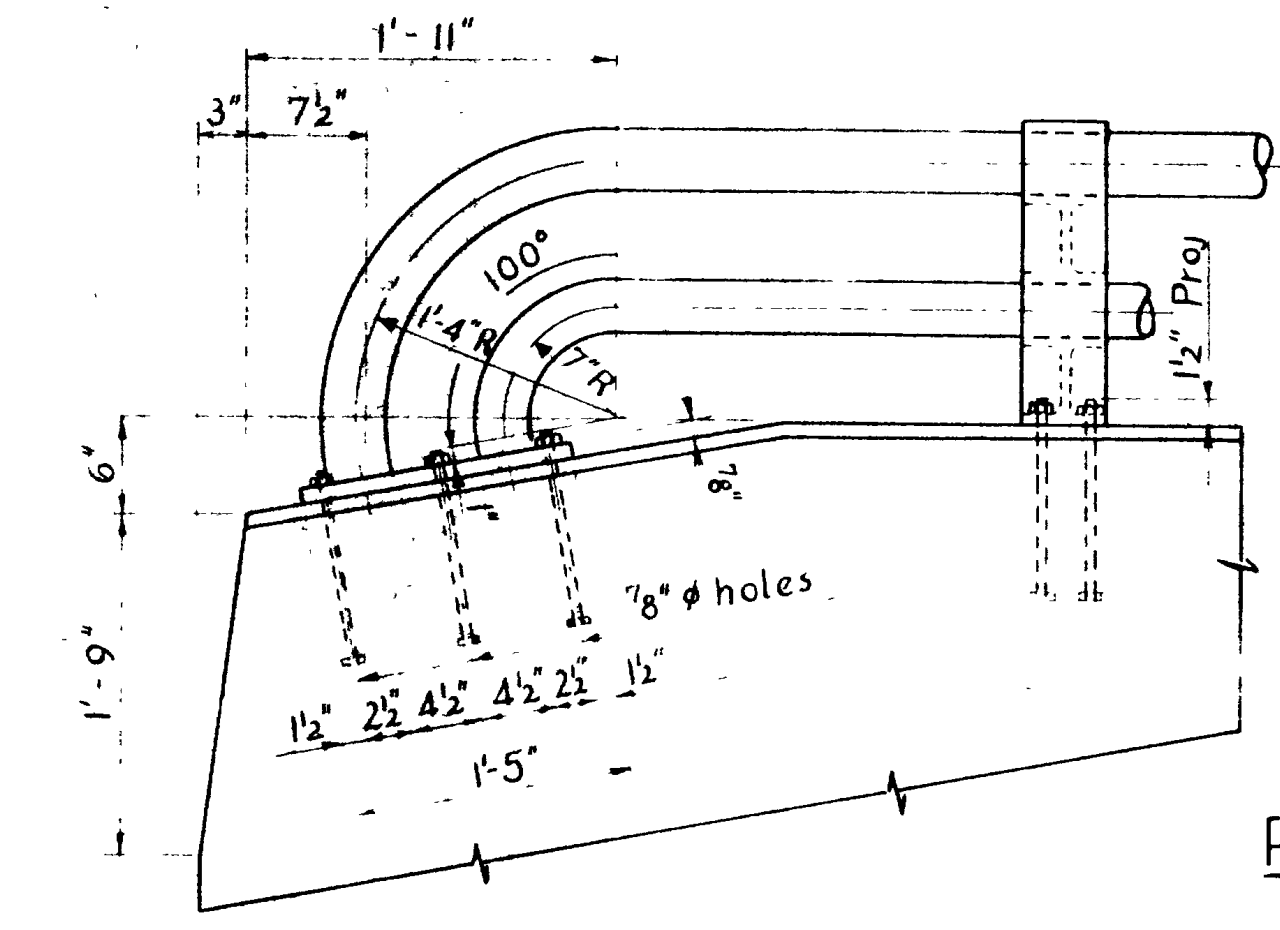
NOTE:
 1. The curb and parapet shall not be poured until at least 7 days after the slab has been poured and until all shoring has been removed.
 2. See Sheet MR-1 for Detail of Rail Installation.



PARAPET ELEVATION

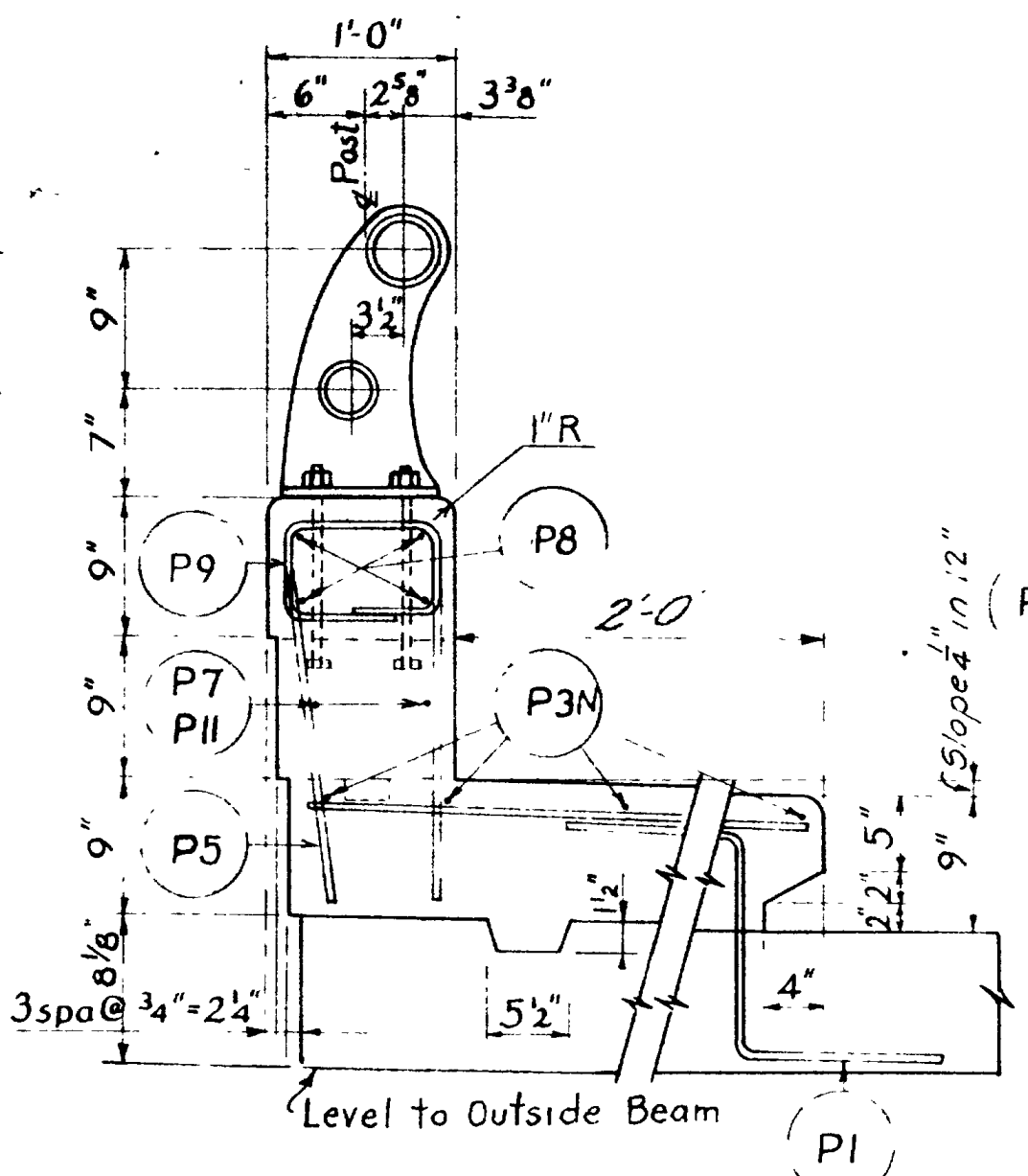


RAILING PLAN

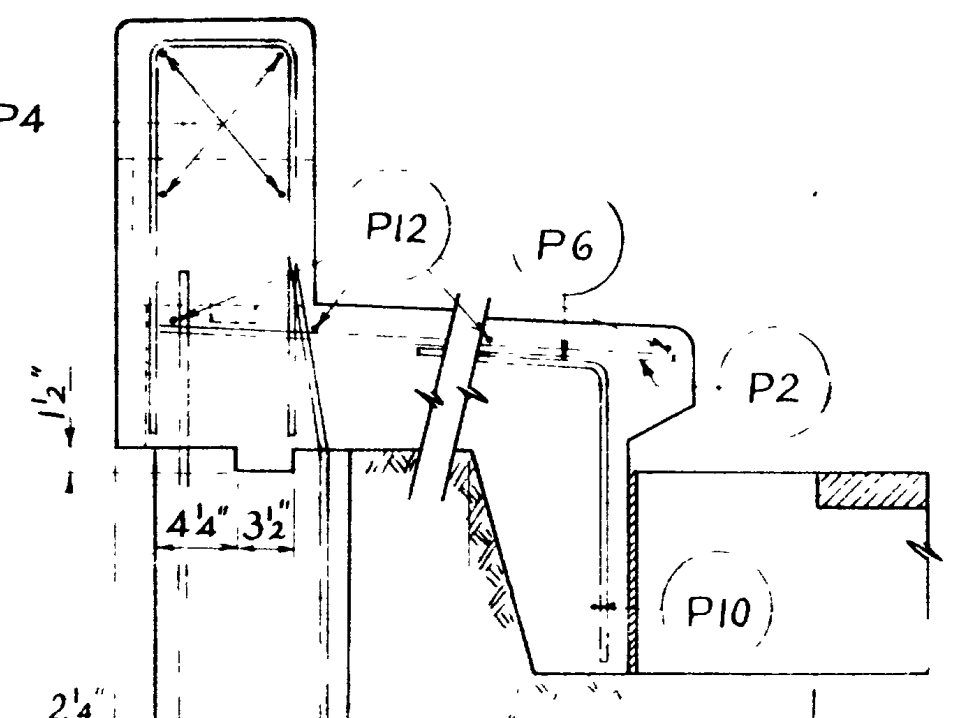


RAILING ELEVATION

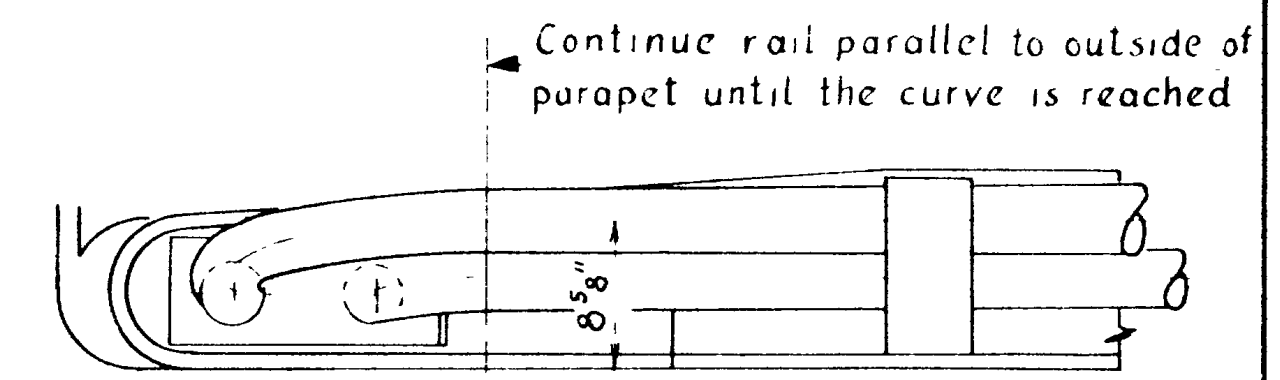
RAILING DETAILS



SECTION A-A



SECTION B-B

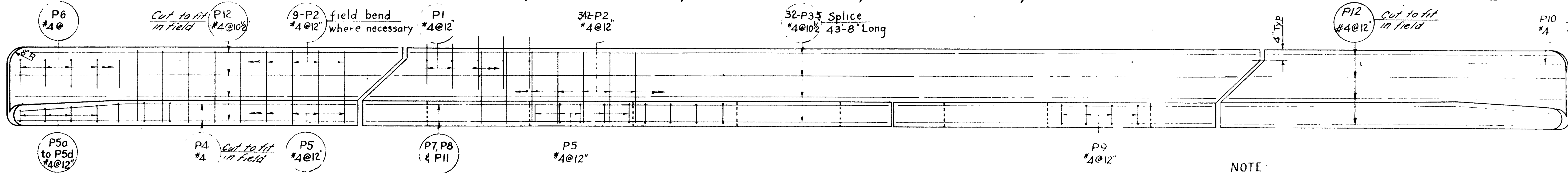


DETAIL AT RAIL END

See Schematic Plan on Sheet # 25
 North Parapet

UTAH STATE DEPARTMENT OF HIGHWAYS			
STRUCTURES DIVISION			
DAVIS-WEBER Co LINE TO 4400 SO. RIVERDALE ROAD U-PASS NORTH PARAPET DETAILS			
DESIGNED BY	R.B.J.	CHECKED BY	CEY
DRAWN BY	H.N.McD	IN CHARGE	P.E.M.
QUANTITY BY	H.N.McD	ESTIMATED BY	P.E.M.
APPROVED BY	Joseph E. West	DATE	Sept. 14, 1963
PROJECT NO.	904+438	SCALE	AS SHOWN
DATE	Sept. 14, 1963	BY	WEBER

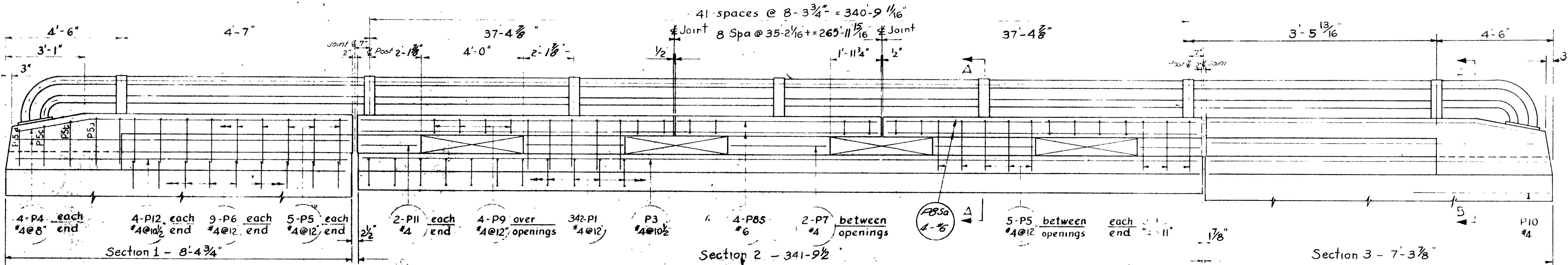
C-453 25-29



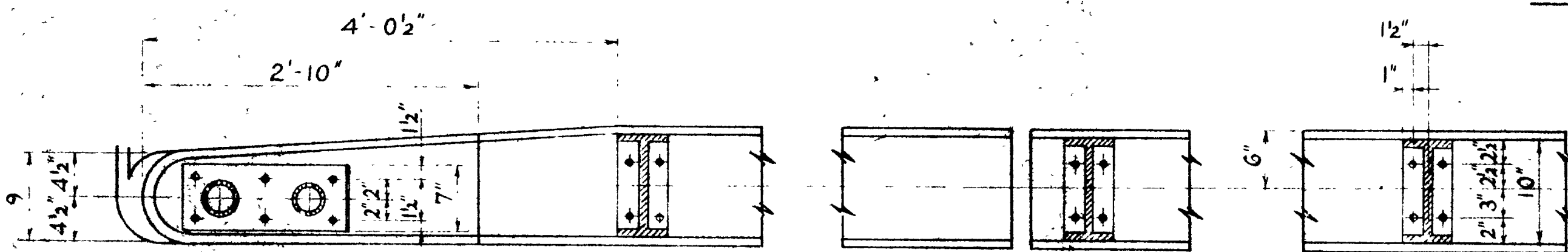
PARAPET PLAN

NOTE:

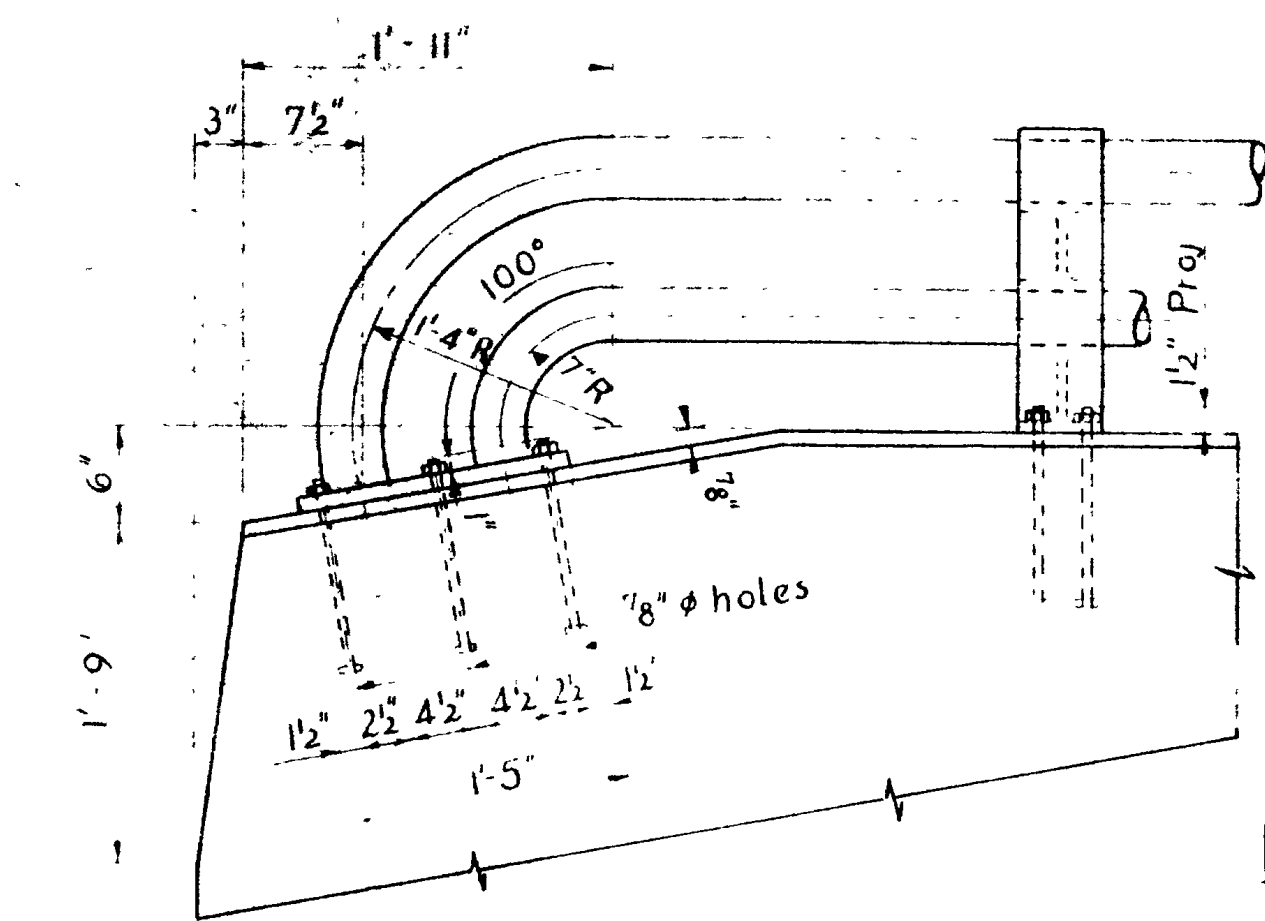
1. The curb and parapet shall not be poured until at least 7 days after the slab has been poured and until all shoring has been removed.
2. See Sheet MR-1 for Detail of Rail Installation.



PARAPET ELEVATION

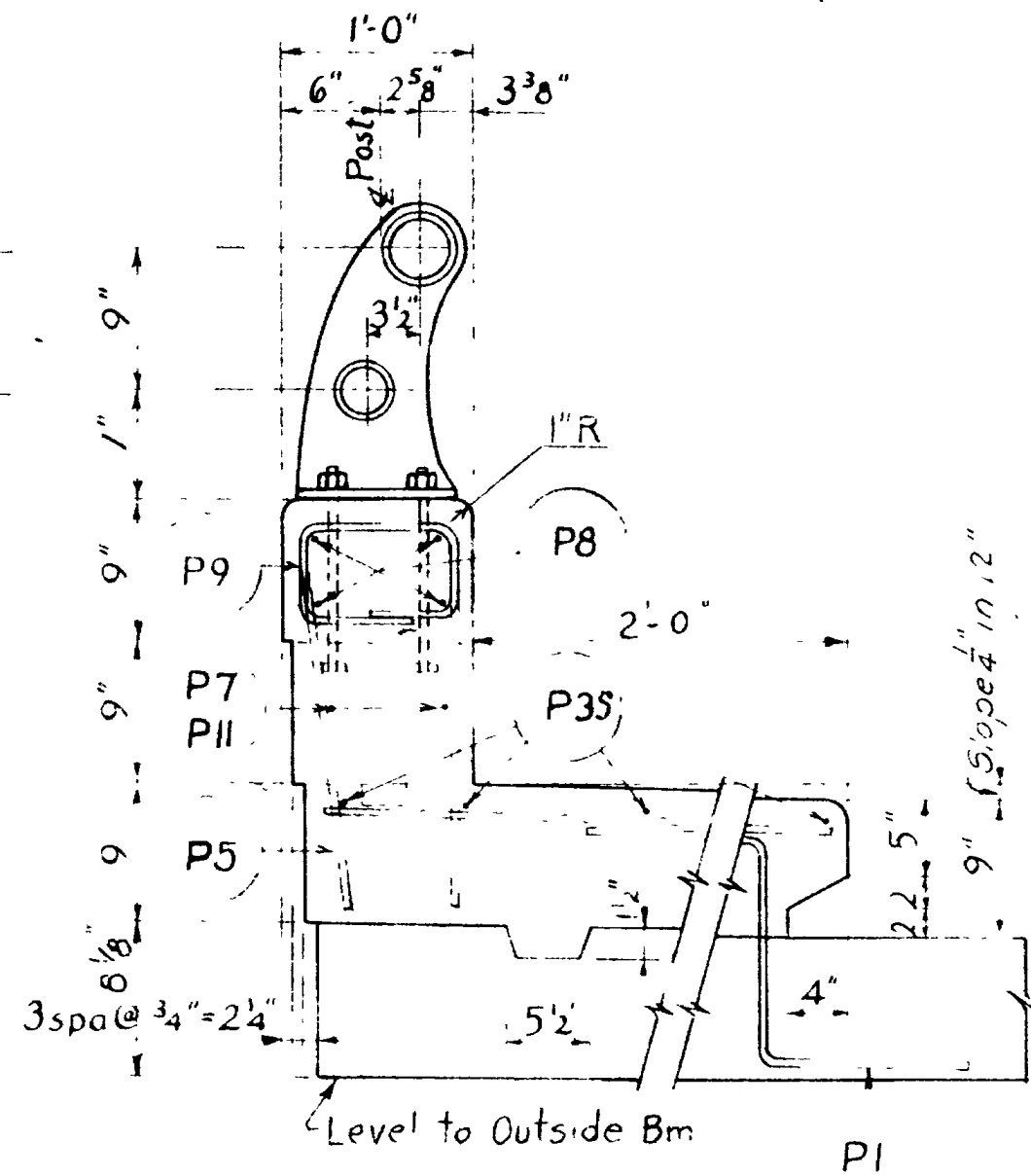


RAILING PLAN

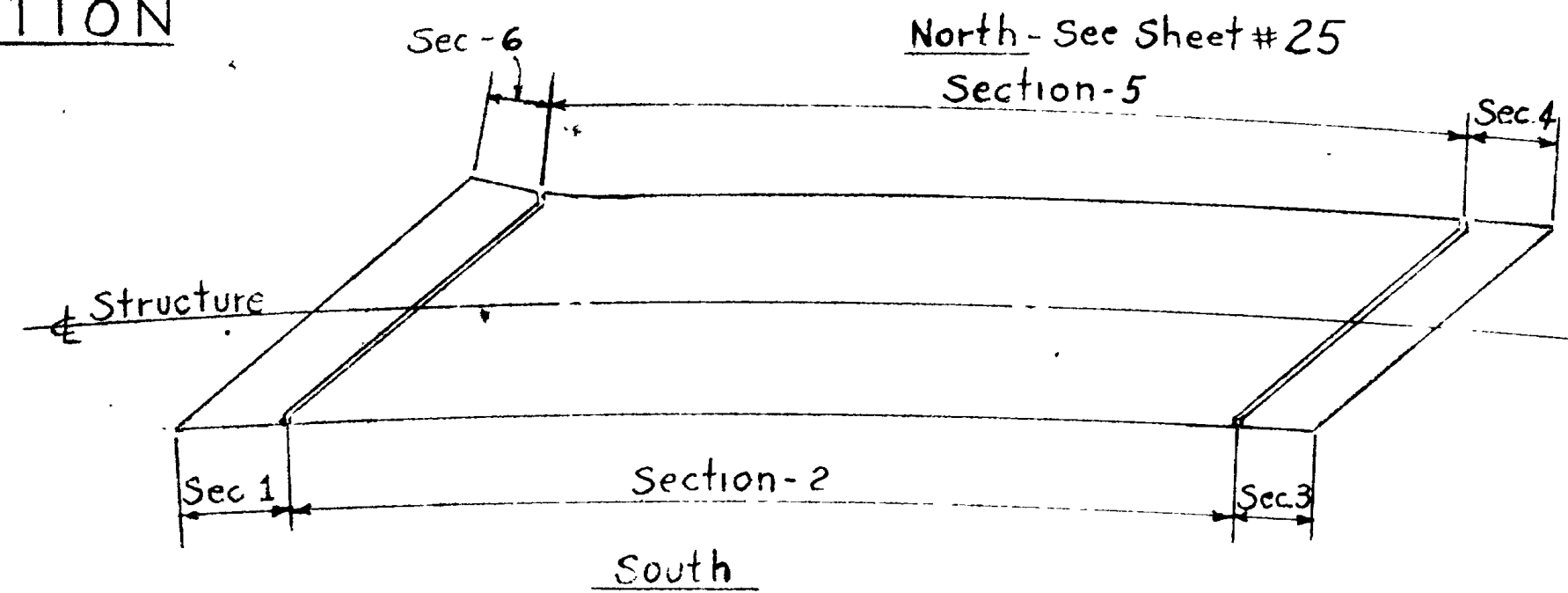


RAILING ELEVATION

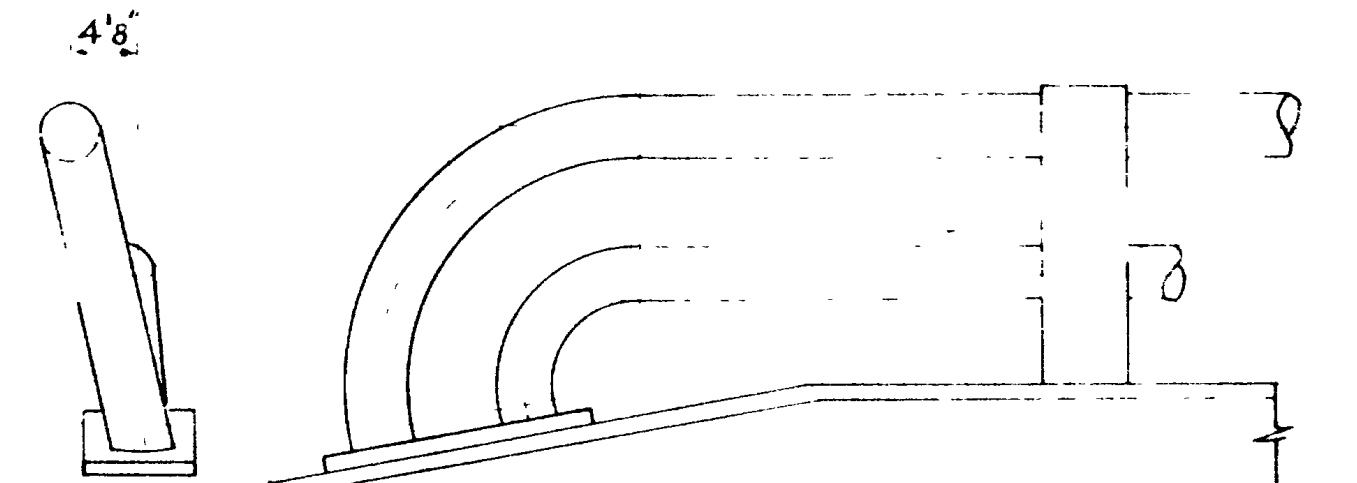
RAILING DETAILS



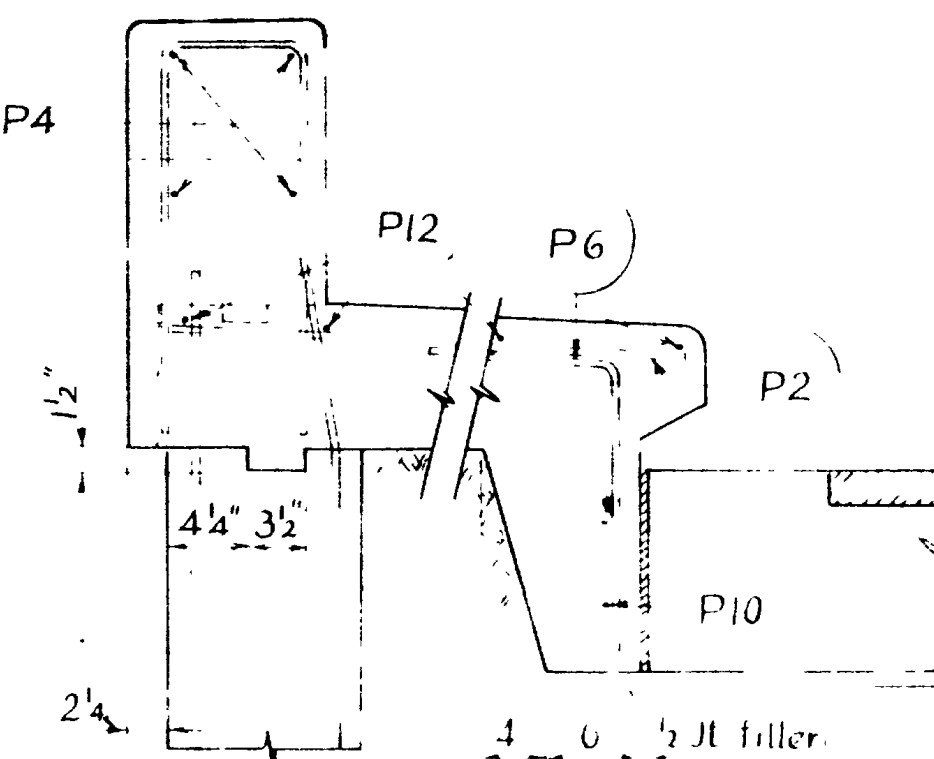
SECTION A-A



Schematic Plan



DETAIL AT RAIL END



SECTION B-B

UTAH STATE DEPARTMENT OF HIGHWAYS
 STRUCTURES DIVISION
 DAVIS-WEBER Co LINE TO 4400 SO
 RIVERDALE ROAD U'PASS
 SOUTH PARAPET DETAILS
 P. J. J. I-15-8(20)334
 H. M. C. 904+43.8
 J. J. J. WEBER

MARK	LOCATION	SIZE NO.	NO. BARS	L'GTH.	TOTAL LENGTH	SKETCH
Abutments						
A1		9	14	40	560.0	
A2		9	14	40.0	560.0	
A3		9	14	43.1	603.2	
A4		8	12	42.0	504.0	
A5		8	12	40.0	480.0	
A6		8	12	38.5	461.0	
A7		4	80	40.8	3264.0	
A8		4	16	34.0	544.0	
A9		4	6	6.6	39.0	
A10		4	225	9.4	2100.0	
A11		4	225	9.0	2025.0	
A12		4	6	6.4	38.0	
A13		118	4	4.0	472.0	
A14		200	16	16.0	3136.0	
A15		143	2	2.0	286.0	
A16		108	2	2.2	234.0	
A17		3	3	3.3	9.9	
A18	Abutment	4	3	3.9	11.3	
A51	Approach Sub	4	152	17.6	2660.0	
A52		7	201	14.8	2948.0	
A53		4	44	53.4	2346.8	
A54		7	6	53.8	322.0	
A55		4	44	58.2	2553.4	
A56	Approach Sub	7	6	58.6	351.0	
B1	Bent III-B	11	18	23.4	420.0	
B2	III-B	11	6	30.0	180.0	
B3	III-B	11	8	28.8	229.4	
B4	III-B	11	4	53.6	214.0	
B5	III-A & II-A	7	12	31.0	372.0	
B6	Bent III-B	4	8	27.6	220.0	

MARK	LOCATION	SIZE NO.	NO. BARS	L'GTH.	TOTAL LENGTH	SKETCH
Bent						
B7	IIA-III-B-III-A-III-B	4	82	16.3	1332.6	
Bent						
B8	III-A & II-A	11	12	54.8	656.0	
B9	III-A & II-A	4	16	22.8	362.6	
B10	III-A & II-A	10	24	20.8	496.0	
B11	III-A & II-A	9	4	43.9	175.0	
B12	IIA & II-B	4	35	14.5	498.5	
B13	III-A & II-A	7	2	43.9	175.0	
B14	II-B	4	4	56.3	225.0	
B15	II-B	11	12	22.2	266.0	
B16	II-B	11	2	56.3	112.4	
B17	II-B	11	6	24.8	148.0	
B18	Bent II-B	11	12	48.7	389.2	
B19	All Bents	5	44	7.9	341.0	
B20	Bent III-A	4	2	44.3	88.6	
B21	IIA	11	6	24.0	144.0	
B22		8	3	43.3	129.9	
B23		11	2	43.3	86.6	
B24	IIA	11	6	51.8	310.0	
B25	II-B	11	8	56.6	452.0	
B26	II-B	4	4	26.3	105.0	
B27	II-B	11	3	51.5	154.3	
B28	II-B	9	4	27.3	109.0	
B29	Bent III-B	11	12	25.8	308.0	

MARK	LOCATION	SIZE NO.	NO. BARS	L'GTH.	TOTAL LENGTH	SKETCH
C1	Bent III-A & III-B	11	H6	18.7	2155.8	
C2	All Bents	4	202	8-11	1801.2	
C3	Bent II-A II-B	11	80	21.0	1680.0	
C4	II-A II-B	11	80	5.4	426.8	
C5	Bent II-A II-B	11	48	19.0	912.0	
F1	Footings	6	494	7.8	3787.4	
F3		4	113	5.10	659.2	
F4		4	12	11.0	132.0	
F5		4	18	24.8	444.0	
F6		4	6	30.8	184.0	
F7	Footings	4	6	26.0	156.0	
P1	No. & So. Parapets	4	679	3.0	2037.0	
P2	No. & So. Parapets	4	715	2.8	1996.8	
P3N	No. Parapet	4	32	42.11	1373.4	
P3S	So. Parapet	4	32	43.8	1397.4	
P4	No. & So. Parapet	4	4	40.0	160.0	

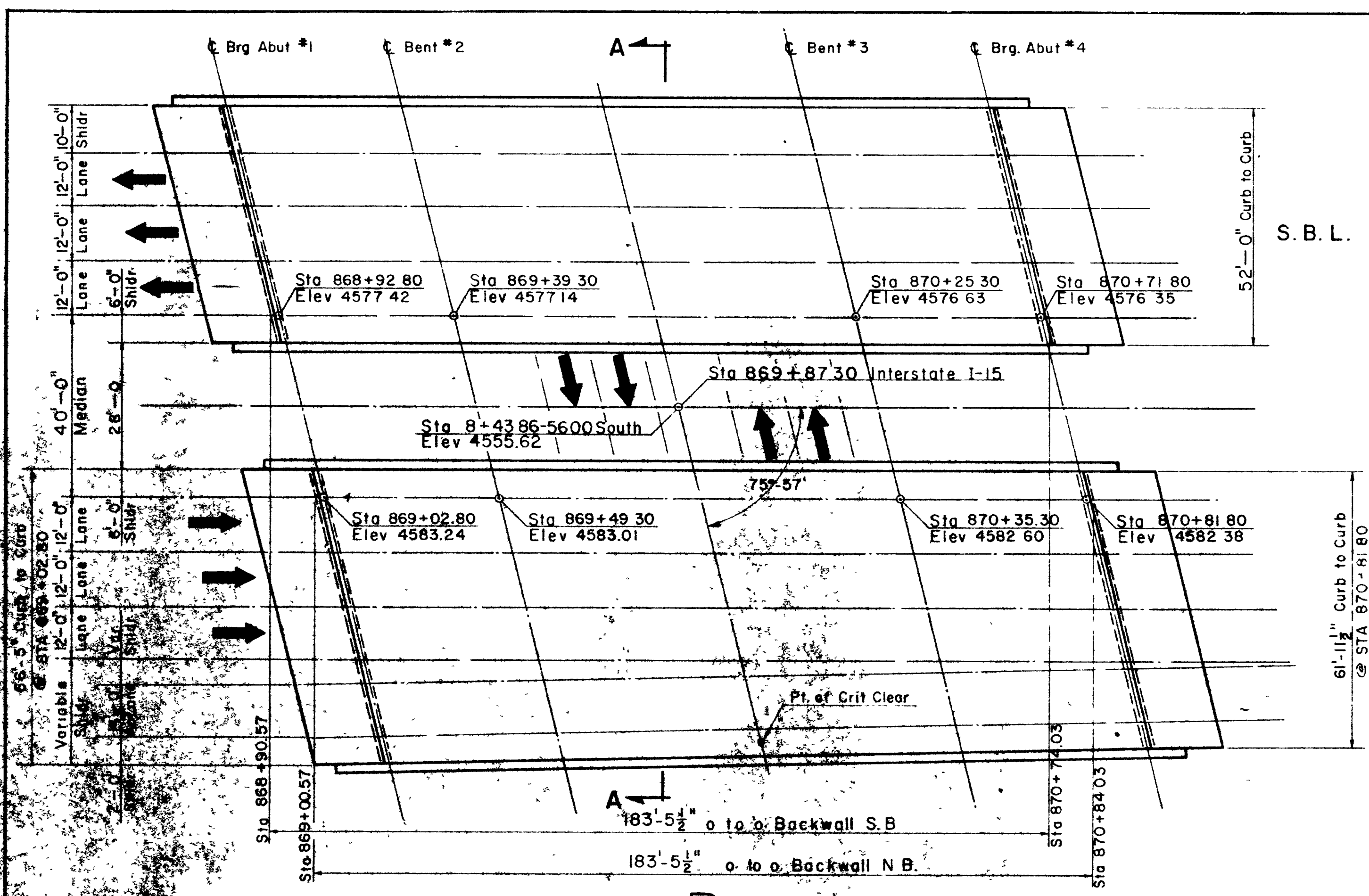
MARK	LOCATION	SIZE NO.	NO. BARS	L'GTH.	TOTAL LENGTH	SKETCH
P5	No. & So. Parapets	4	429	5.0	2145.0	
P5a		4	4	4.11	19.8	
P5b		4	4	4.8	18.8	
P5c		4	4	4.3	17.0	
P5d	No. & So. Parapets	4	4	3.10	15.4	
P6	No. & So. Parapets	4	36	2.7	93.0	
P7	No. & So. Parapets	4	158	4.0	632.0	
P8Na	No. Parapet	6	4	37.11	151.8	
P8N	No. Parapet	6	32	33.2	1064.4	
P8Nb	No. Parapet	6	4	29.6	118.0	
P8Sa	So. Parapet	6	8	37.7	300.8	
P8S	So. Parapet	6	32	34.10	1114.8	
P9	No. & So. Parapets	4	324	3.4	1080.0	
P10	No. & So. Parapets	4	1	40.0	40.0	
P11	No. & So. Parapets	4	8	2.0	16.0	
P12	No. & So. Parapet	4	3	40.0	120.0	
S1	Deck	6	164	20.0	3280.0	
S2		5	152	53.0	8056.0	
S3		5	242	40.0	9648.0	
S4		5	117	40.11	4787.3	
S5	Deck	5	106	37.0	3922.0	

UTAH STATE DEPARTMENT OF HIGHWAYS
STRUCTURES DIVISION
DAVIS-WEBER CO. LINE TO 4400 SOUTH RIVERDALE ROAD U'PASS
REINFORCING STEEL SCHEDULE

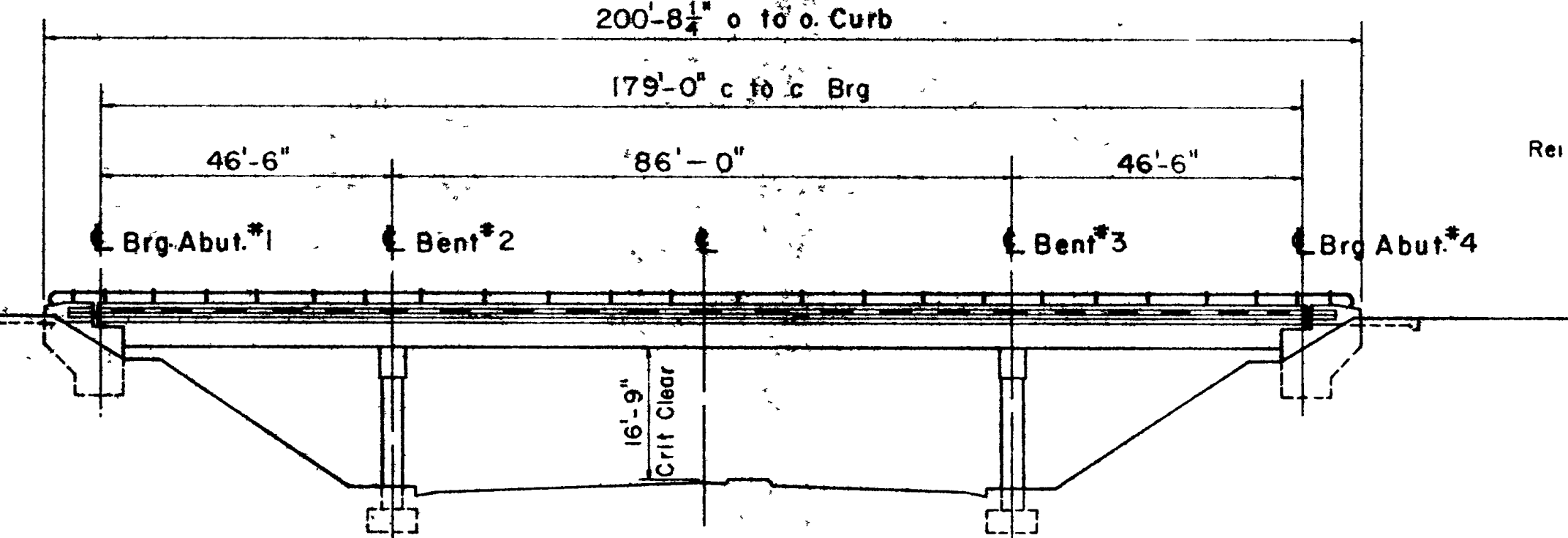
DATE: 1-15-80
DRAWN BY: G.W.J.
CHECKED BY: H.W.J.
DESIGNED BY: J. H. West

C.E.V. 1-15-80(20) 334
P.F.M. 904+43.8
P.F.M. WEBER

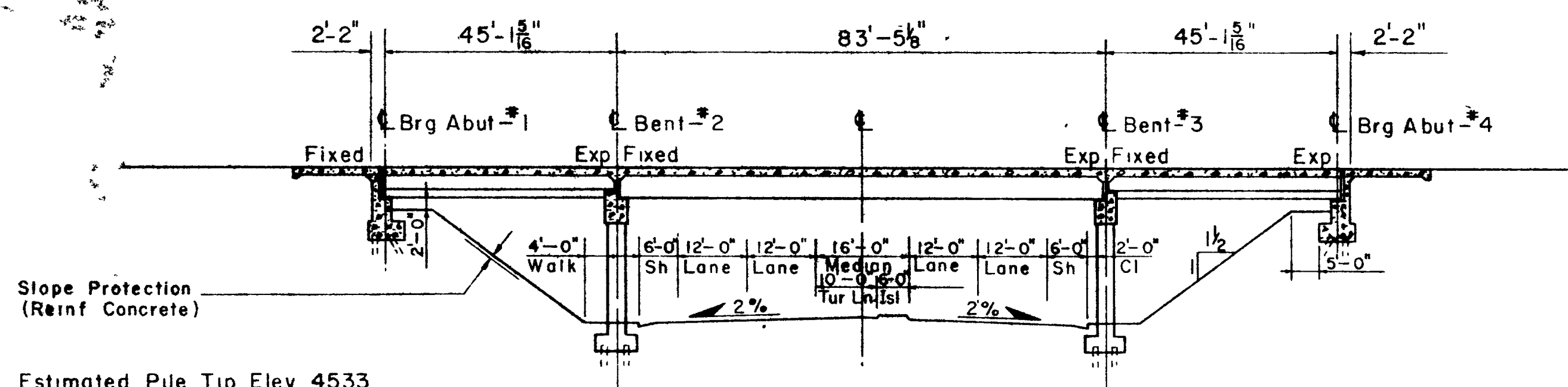
C-453 28 29



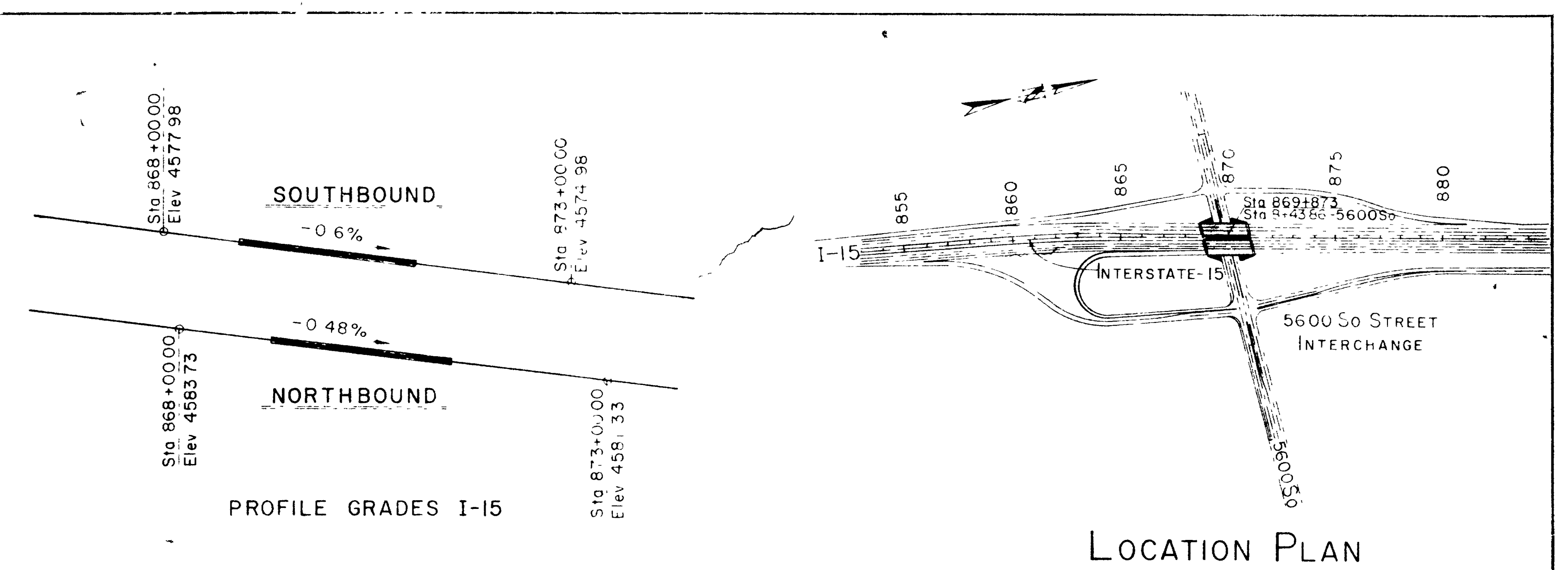
PLAN



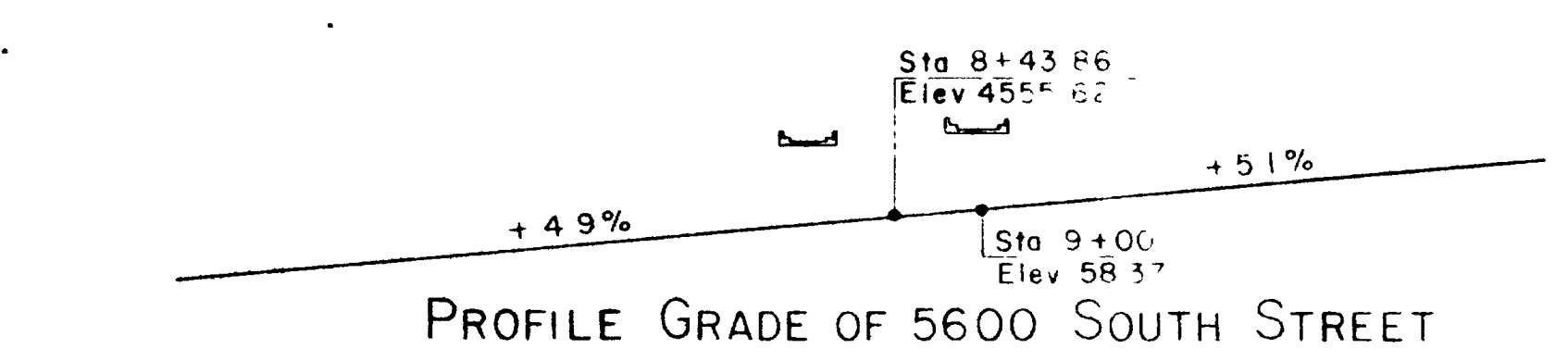
ELEVATION



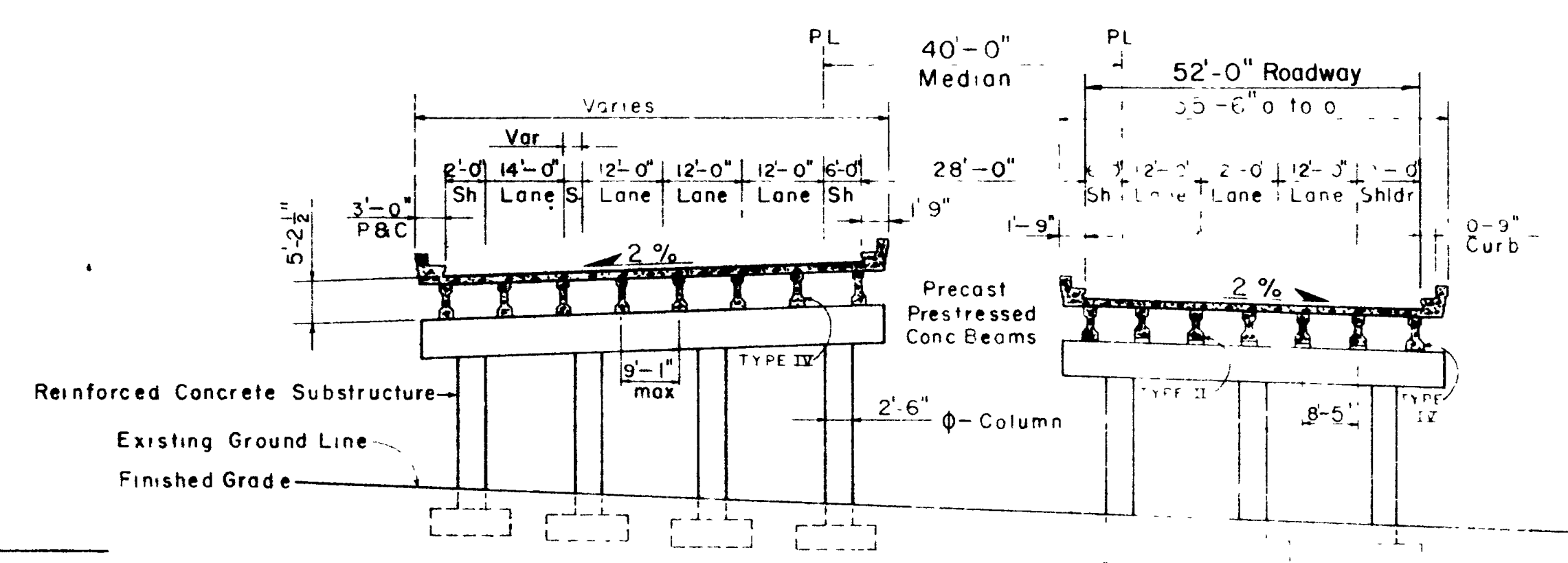
SECTION NORMAL TO 5600 SOUTH ST.



LOCATION PLAN



PROFILE GRADE OF 5600 SOUTH STREET



SECTION A - A

GENERAL NOTES

- Materials, construction, and workmanship shall be in accordance with Utah State Standard Specifications for Road and Bridge Construction, 1960 Edition and Supplement thereto which are in effect at the date of request for bids.
- All reinforcing bars shall be intermediate grade billet steel, conforming to AASHTO designation M-31. Deformations shall conform to AASHTO designation M-137.
- Type II cement required (low alkali), except for Prestressed Beams.
- Exposed corners at joint filler and open joints shall be beveled $\frac{3}{4}$ " or rounded.

DESIGN DATA

- H20-S16-44 or Interstate Alternate Loading in accordance with AASHTO Specifications of 1961
- Cast in place Concrete $f_c = 1,200$ psi, $n = 10$, f_s (Reinf) = 20,000 psi
 - Structural Steel $f_s = 20,000$ psi
 - Prestressed Concrete $f_c = 5,000$ psi, $n = 6$, f_s (Reinf) = 20,000 psi
 - f_s (Prestressing Strands) = 175,000 psi
 - Wearing Surface $\frac{1}{2}$ " Concrete, Future Wearing Surface 15 Lbs/Sq Ft

QUANTITIES

ITEM	ESTIMATED	UNIT	AS CONSTR.
Excavation for Structures - (unclass)	538	Cu Yd	351.9
Class A Concrete - (AE)	1,343	Cu Yd	1335.3
Reinforcing Steel	238,783	Lb	231,619.0
Structural Steel	12,270	Lb	12,270.0
Pile - (other than timber)	2,217	Lin Ft	2,313.2
Furnishing Pile Driving Equipment	1	Lump	100%
Metal Railing - (Single Rail)	802	Lin Ft	803.0
Mechanical Tamping	51	Hr	19.0
Prestressed Members Type II, 47'-0" Long	24	Each	24.0
Prestressed Members Type IV, 46'-8 3/4" Long	2	Each	2.0
Prestressed Members Type IV, 47'-0" Long	6	Each	6.0
Prestressed Members Type IV, 85'-2" Long	1	Each	1.0
Prestressed Members Type IV, 85'-8" Long	14	Each	14.0
Elastomeric Bearing Pad, 3/4" Thick	42	Sq Ft	42.0
Elastomeric Bearing Pad, 1" Thick	20	Sq Ft	20.0
Reinforced Concrete Slope Protection	1,663	Sq Yd	1702.7
ELECTRICAL WORK BRIDGES	1	Lump	100%
Welding Studs on prot. &	(SA # 29)	Each	108.0

INDEX OF SHEETS

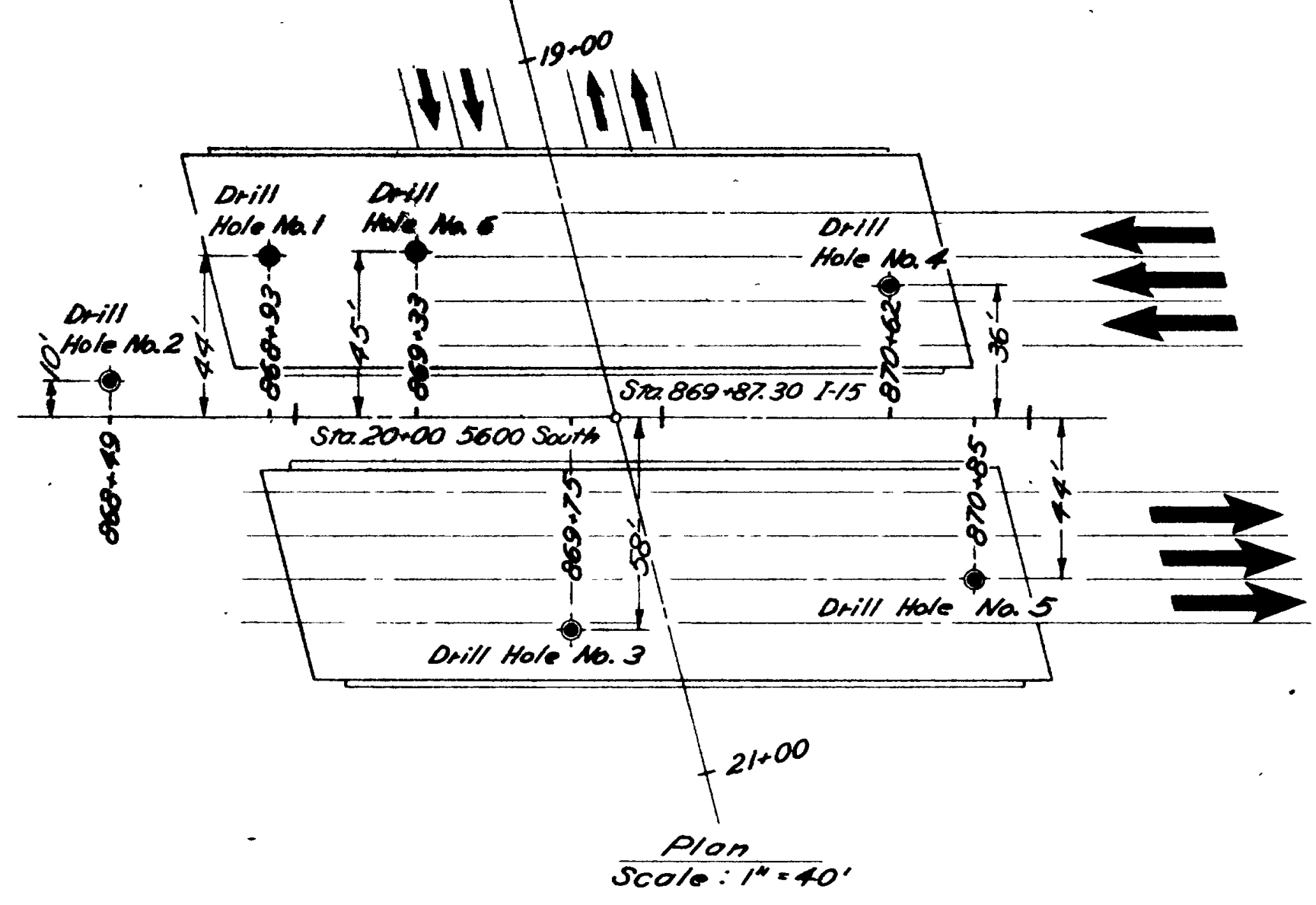
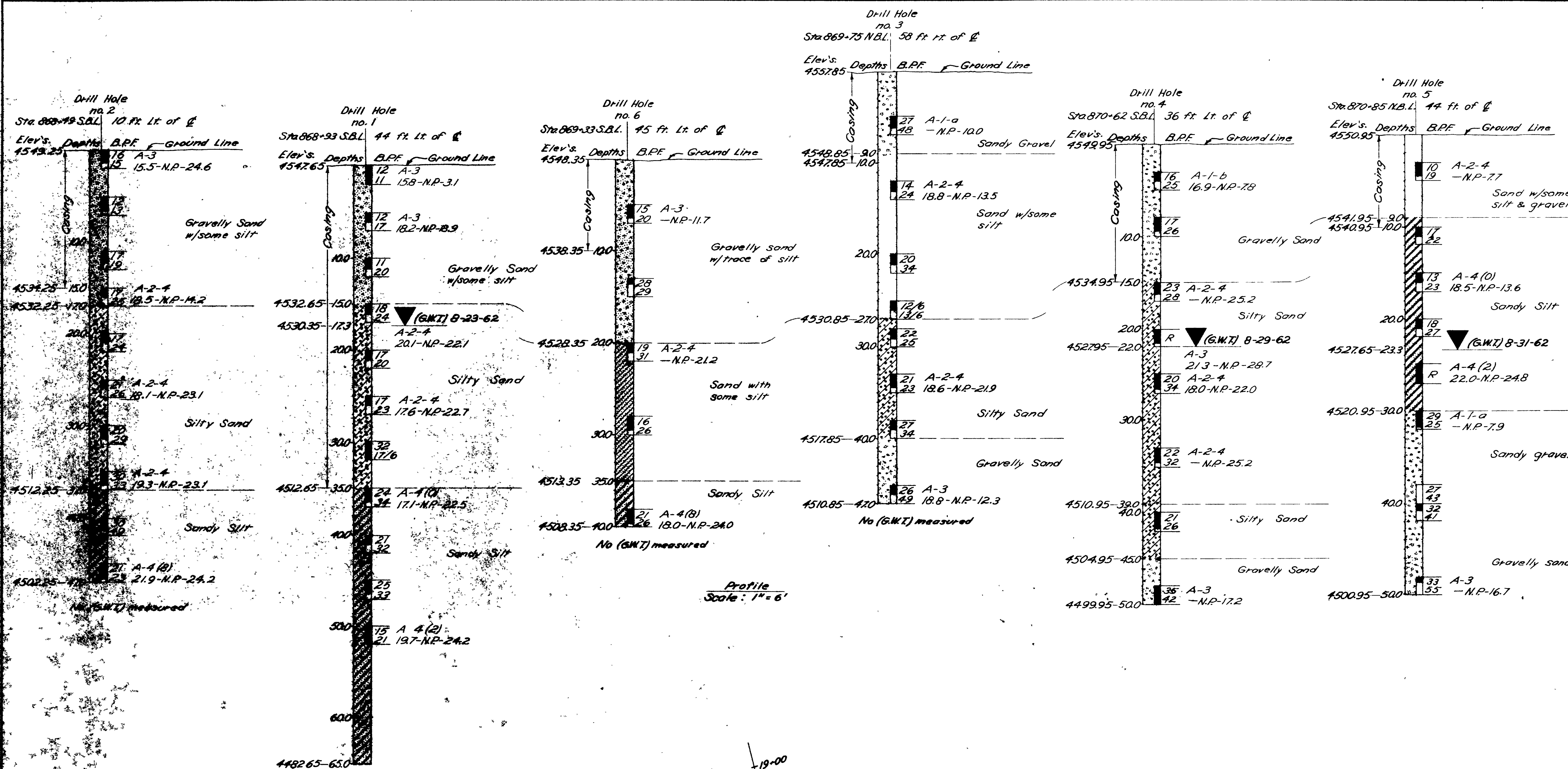
- | | |
|--|--|
| 1 - LAYOUT & SITUATION | 16 - DECK DETAILS SBL |
| 2 - SOIL DATA | 17 - DECK DETAILS NBL |
| 3 - FOUNDATION PLAN | 18 - DECK SECTIONS NBL |
| 4 - ABUTMENT #1 & #4 SBL | 19 - DIAPHRAGM & JOINT DETAILS |
| 5 - ABUTMENT #1 NBL | 20 - SCREED ELEVATIONS |
| 6 - ABUTMENT #4 NBL | 21 - PARAPET & RAIL DETAILS |
| 7 - BENTS #2 & #3 SBL | 22 - PARAPET & RAIL DETAILS |
| 8 - BENT #2 NBL | 23 - REINFORCED CONCRETE SLOPE PROTECTION |
| 9 - BENT #3 NBL | 24 - REINFORCING STEEL SCHEDULE |
| 10 - FRAMING PLAN | 25 - REINFORCING STEEL SCHEDULE |
| 11 - PRESTRESSED CONC BEAM DETAILS TYPE I | 26 - LIGHTING DETAILS FOR STRUCTURE STANDARD DRAWINGS MR-1,SS-3,SP-1 |
| 12 - PRESTRESSED CONC BEAM DETAILS TYPE II | |
| 13 - PRESTRESSED CONC BEAM DETAILS TYPE IV | |
| 14 - PRESTRESSED CONC BEAM DETAILS TYPE IV | |
| 15 - PRESTRESSED CONC BEAM DETAILS TYPE IV | |

UTAH STATE DEPARTMENT OF HIGHWAYS
STRUCTURES DIVISION
DAVIS-WEBER CO LINE TO 4400 So.
5600 SOUTH INTERCHANGE
LAYOUT AND SITUATION

MEL WEST
L.B.L.
H.J.S.
M. Christensen

H-15-870334
H.G.E. 869+8730
Weber

F-66 I-26



KEY TO DRILLING LOGS
RELATIVE DENSITY (SAND & SILT)
 VERY LOOSE - LESS THAN 4 BLOWS PER FOOT
 LOOSE - 4 TO 10 BLOWS PER FOOT
 MEDIUM - 10 TO 30 BLOWS PER FOOT
 DENSE - 30 TO 50 BLOWS PER FOOT
 VERY DENSE - MORE THAN 50 BLOWS PER FOOT

CONSISTENCY (CLAY)
 VERY SOFT - LESS THAN 2 BLOWS PER FOOT
 SOFT - 2 TO 4 BLOWS PER FOOT
 MEDIUM - 4 TO 8 BLOWS PER FOOT
 STIFF - 8 TO 15 BLOWS PER FOOT
 VERY STIFF - 15 TO 30 BLOWS PER FOOT
 HARD - MORE THAN 30 BLOWS PER FOOT

TOP SOIL OR FILL	IGNEOUS	SANDY CLAY
GRAVEL	LIMESTONE	CLAYEY SAND
SAND	CONGLOMERATE	SILTY CLAY
SILT	DOLOMITE	CLAYEY SILT
CLAY		SILTY SILT
SHALE		SANDY SILT

DRILL HOLE NO. STATION 0+00 E OR LT OR RT IN FT. OFFSET.

GROUND ELEVATION	DEPTHS	GR EL. 4562 FT.
4555	2	EXAMPLE TYPICAL STIFF MEDIUM PLASTIC BRN CLAY, SOME SILT
4552	5	DATE
4546	10	THIN WALL SHELBY TUBE, UNDISTURBED SAMPLER USED.
	14	SPLIT BARREL UNDISTURBED SAMPLER WITH LINER RINGS OR CALIFORNIA TYPE SAMPLER
	25	
	30	SAMPLE NOT RECOVERED
4531	30	REASON NOT RECOVERED

NO. OF BLOWS OF A 140 LB. HAMMER FALLING 30 INCHES REQUIRED TO DRIVE A STD. 1 1/2" I.D. & 2" O.D. SAMPLE TUBE 1 FT.

CLASSIFICATION OF EACH SAMPLE AND RESULTS OF TESTS.

ABBREVIATIONS
 L.L. - LIQUID LIMIT IN %
 P.I. - PLASTIC INDEX %
 W. - NATURAL MOISTURE CONTENT IN %
 W.G. - WELL GRADED
 PEN - PENETRATION
 G.W.T. - GROUND WATER TABLE
 B.P.F. - BLOWS PER FOOT.
 N.P. - NON-PLASTIC

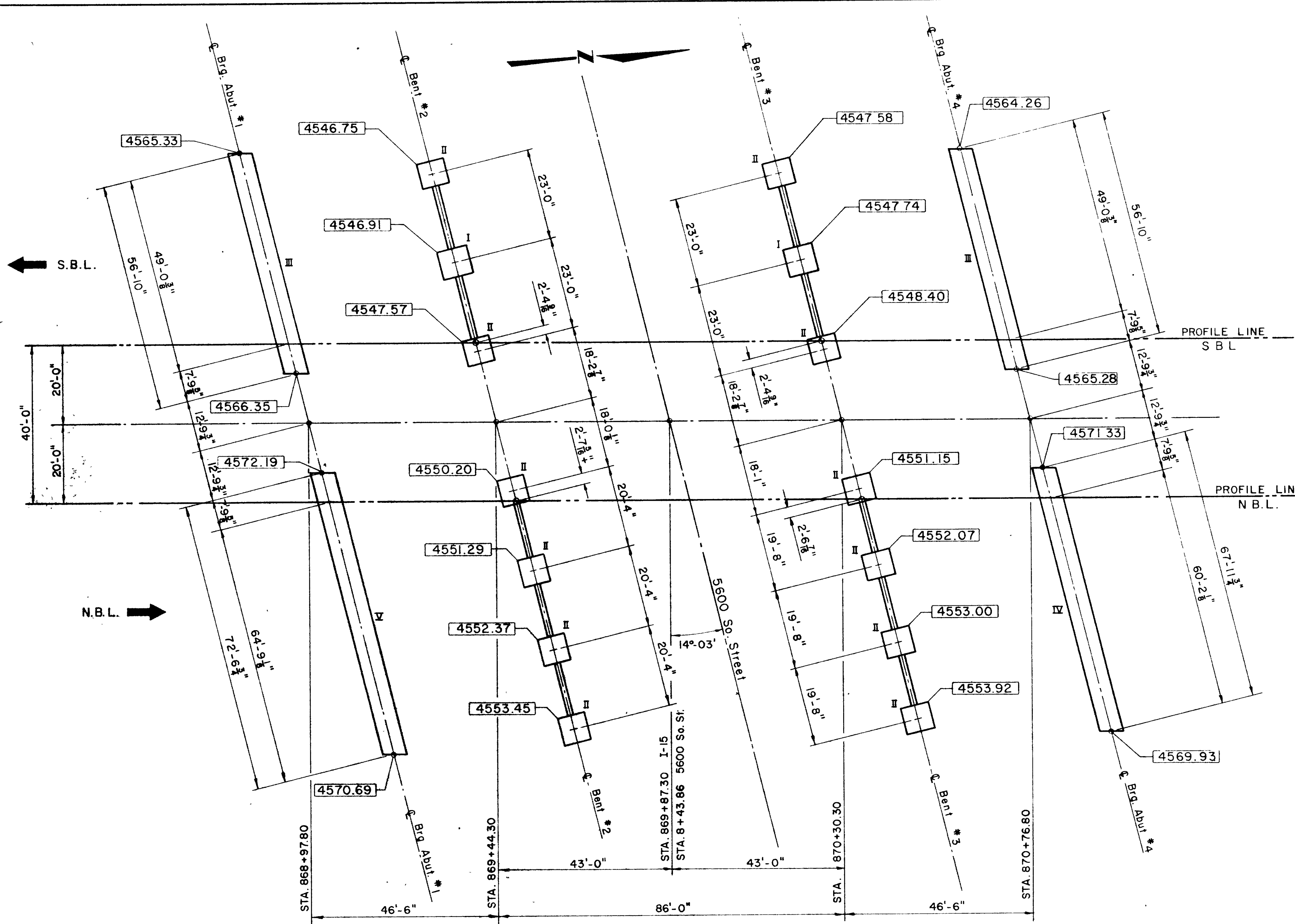
UTAH STATE DEPARTMENT OF HIGHWAYS
 SALT LAKE CITY, UTAH
 STRUCTURES DIVISION
 DAVIS-WEBER Co. LINE TO 4400 So.
 5600 SOUTH INTERCHANGE
 SOIL DATA

DESIGNED BY: MW	CHECKED BY: MJC	PROJECT NUMBER: 1-15-8(20)334
DRAWN BY: JCP	CHECKED BY:	PROJECT NUMBER: 869+87.30
QUANTIFIED BY:	CHECKED BY:	COUNTY: WEBER
APPROVAL: My Christian	DATE:	COUNTY:

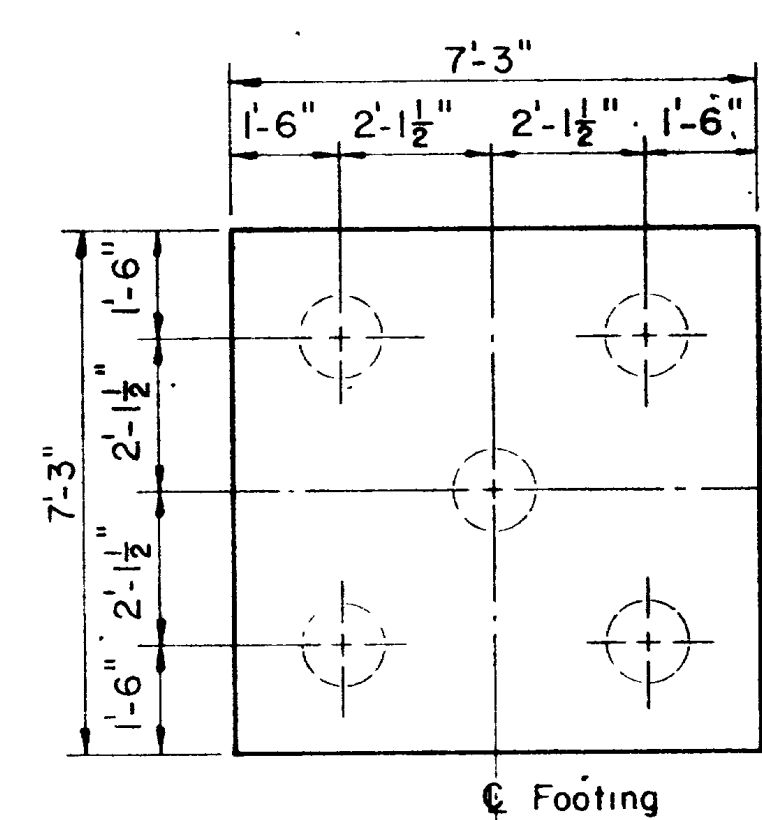
NO.	BY	DATE	REVISIONS	REMARKS
BR			DRG. F-66	2 26

REVISIONS

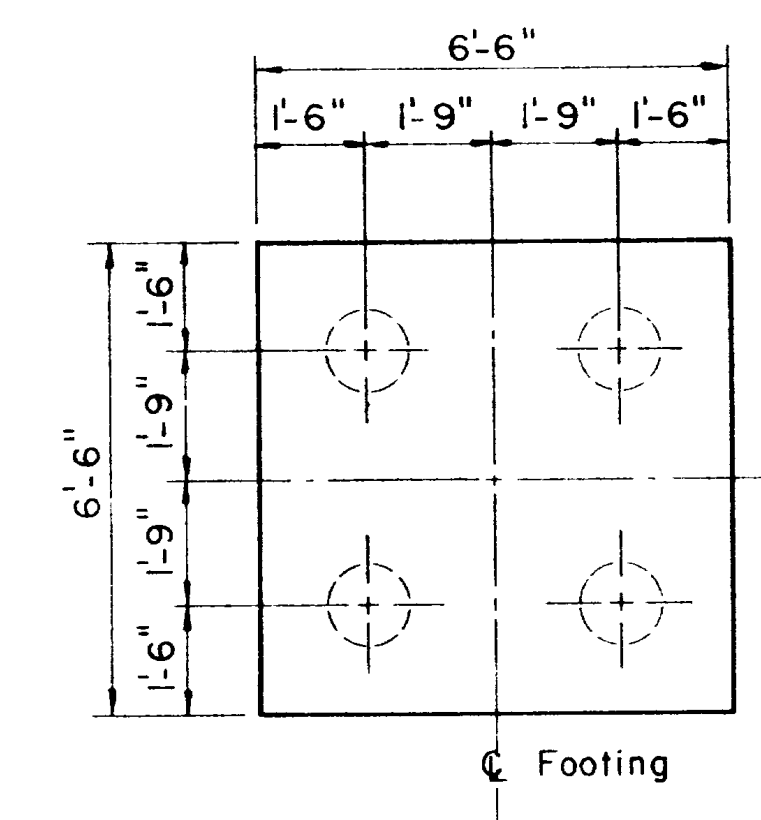
NO.	DATE	REVISIONS



PLAN



PILE LAYOUT-I

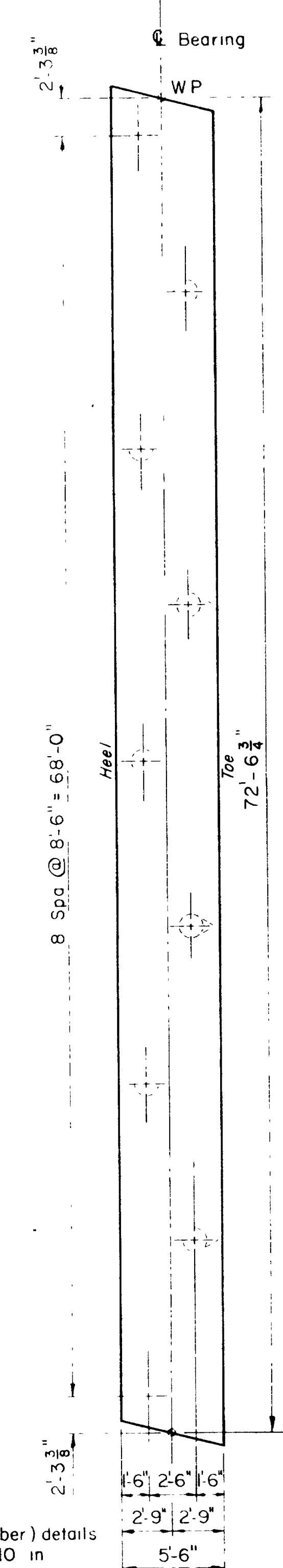


PILE LAYOUT-II

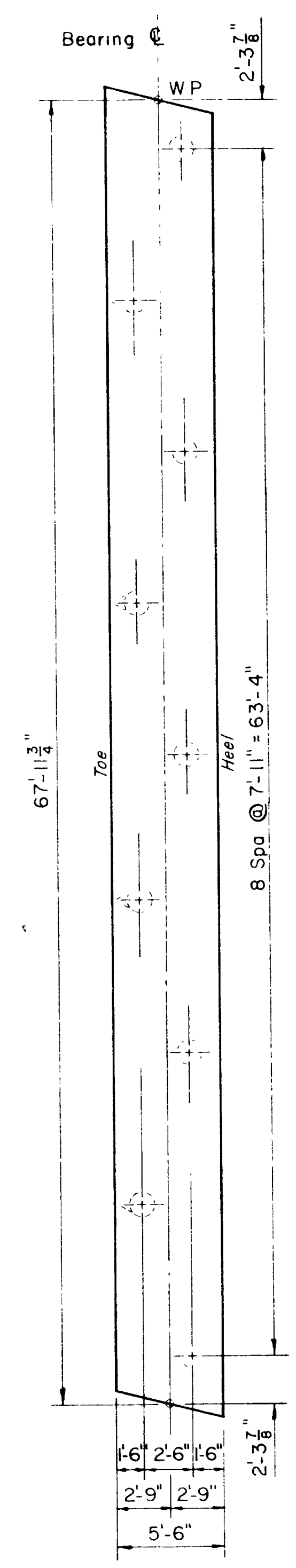
NOTES

1. Pile design load is 60 tons - D.L + L.L
2. See drawing S-P-1 for piles (other than timber) details
3. Piles shown thus $\leftarrow \rightarrow$ are to be battered 1:10 in the direction indicated unless otherwise shown
4. Tip elevation given is approximate only & will be verified at time of construction in accordance with the specifications
5. Elevations at bottom of footing are shown enclosed in rectangles e.g. 4564.26
6. All abutments and bents are parallel to 5600 So St.

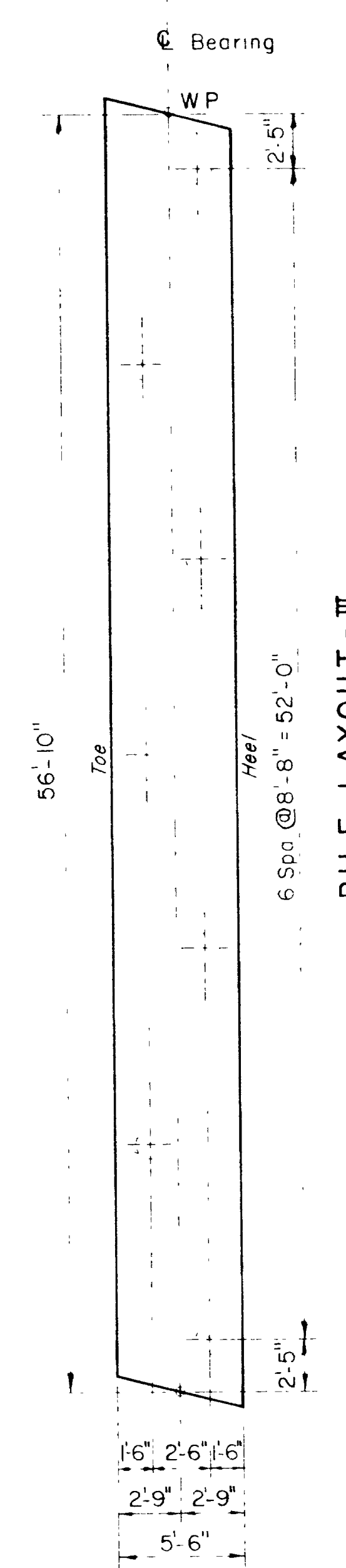
COMPUTED LOAD (TONS)					
ABUTMENTS			BENTS		
LOADING	REAR	FRONT	LOADING	Plumb	V
	V	H			
D L+E	47	45	5	D L	42
Group I	60	55	6	Group I	53



PILE LAYOUT-V



PILE LAYOUT-IV



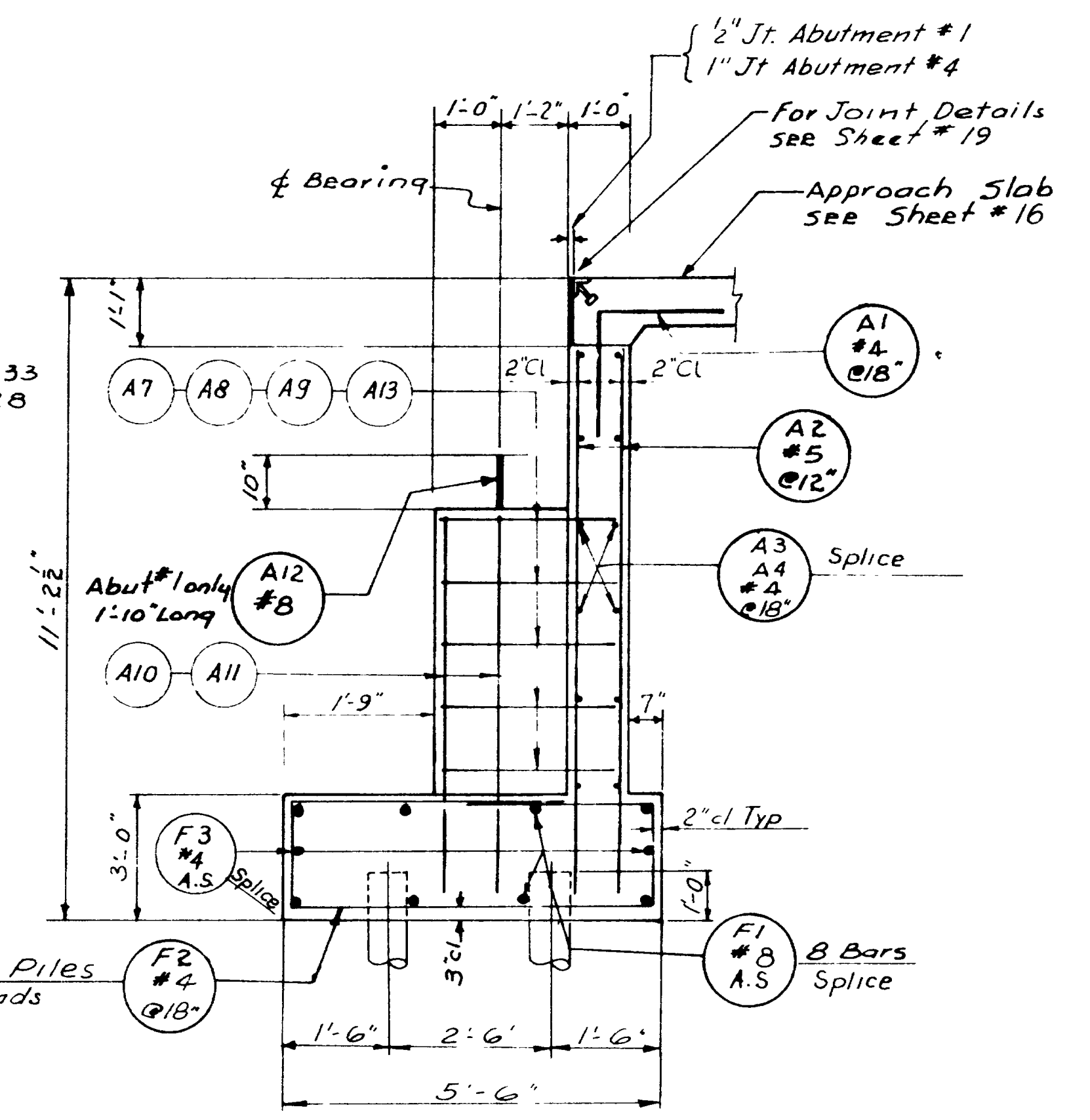
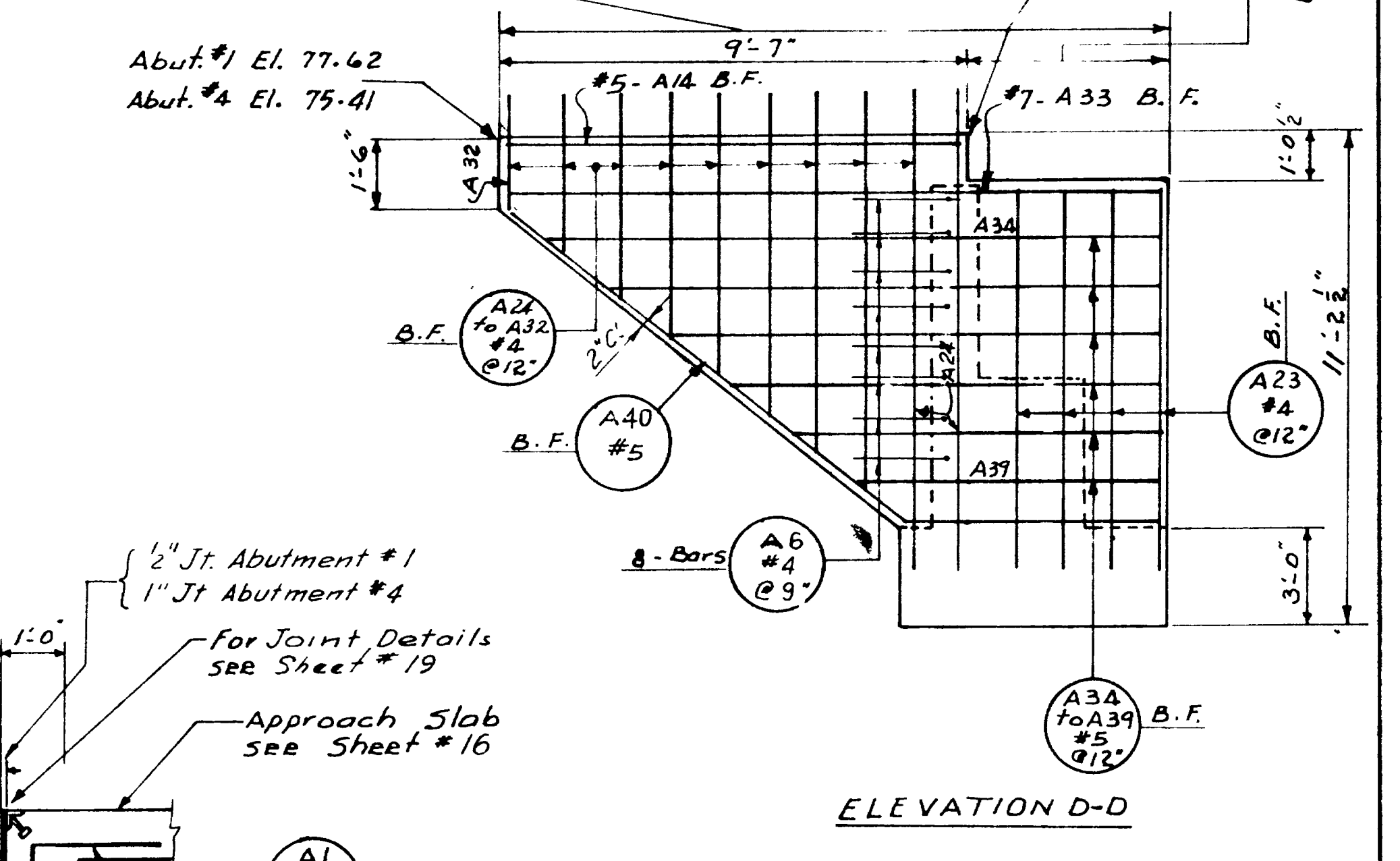
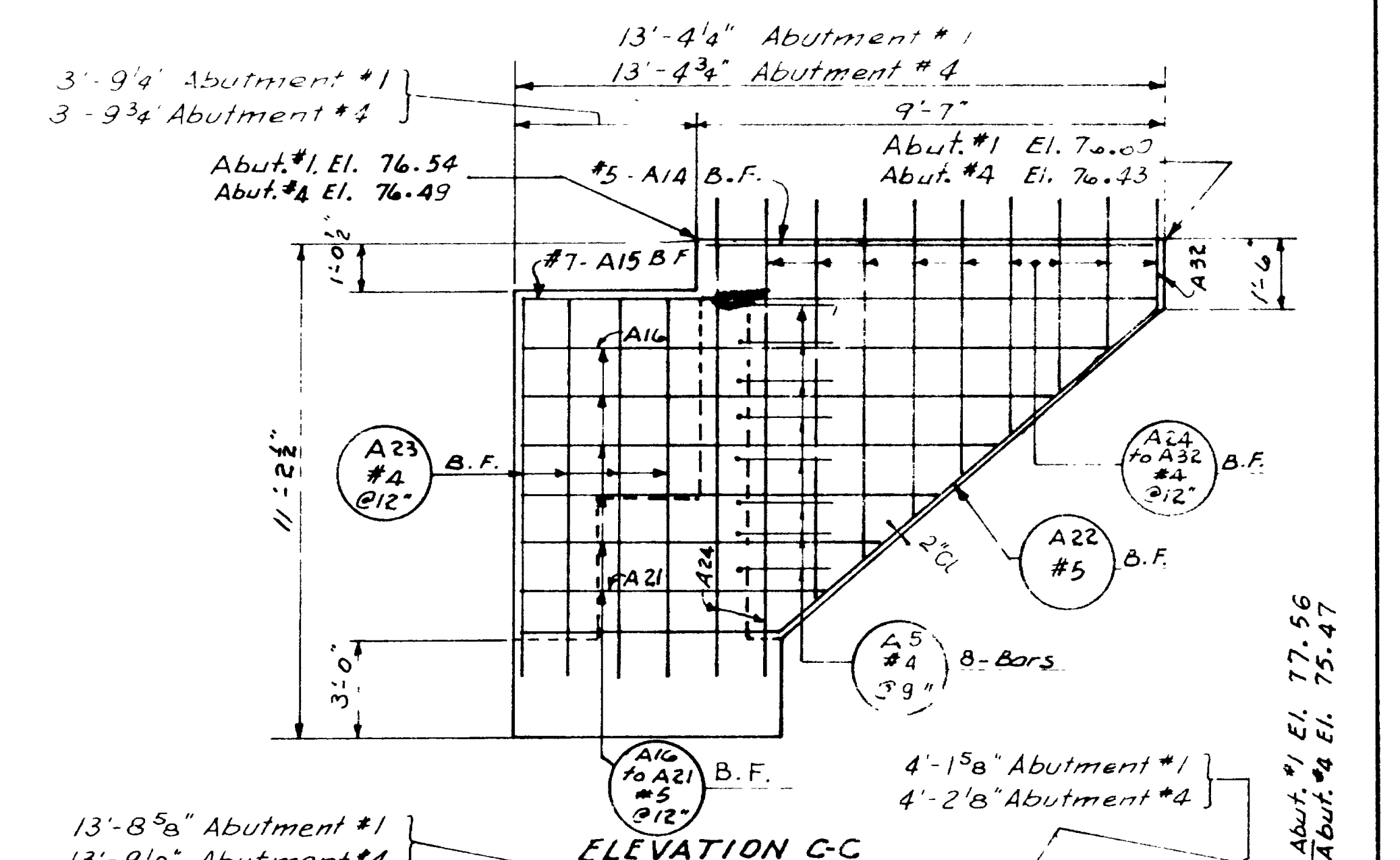
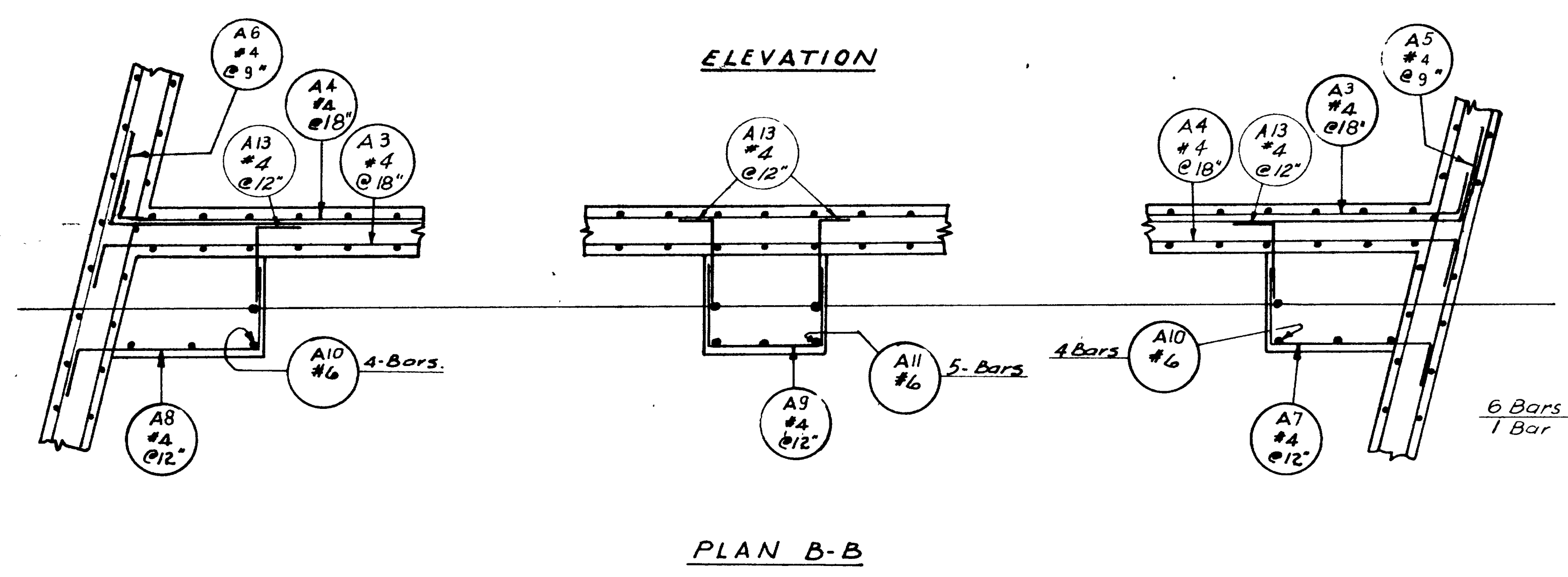
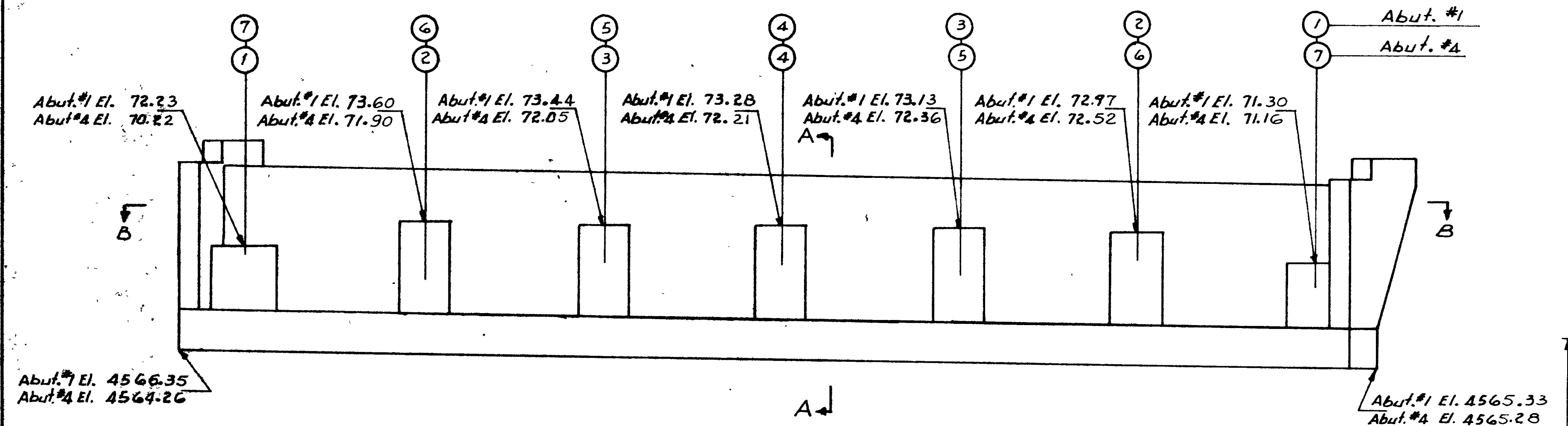
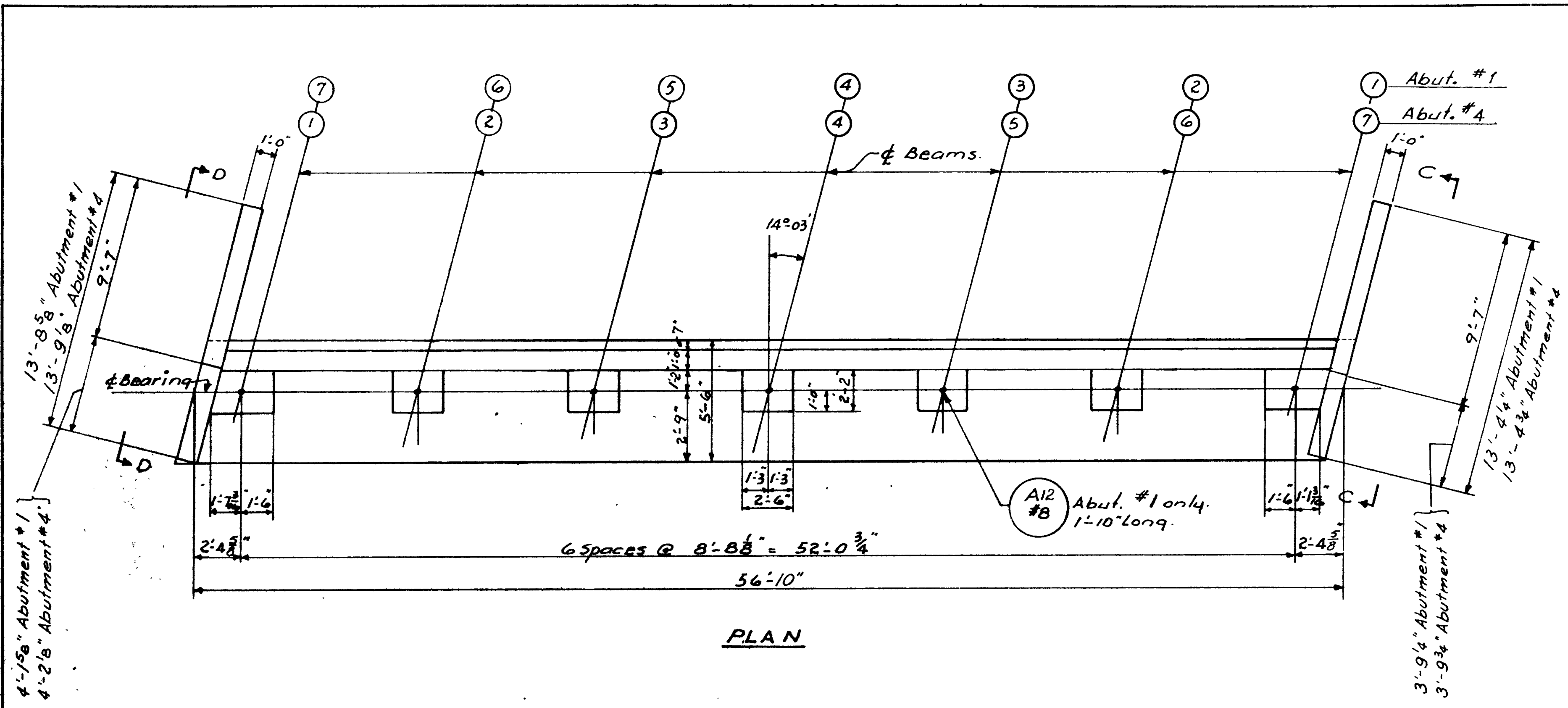
PILE LAYOUT-III

Est. Pile tip elev = 4533
See note 4 below

UTAH STATE DEPARTMENT OF HIGHWAYS
SALT LAKE CITY, UTAH
STRUCTURES DIVISION
DAVIS - WEBER Co. LINE TO 4400 Sp.
5600 SOUTH INTERCHANGE
FOUNDATION PLAN

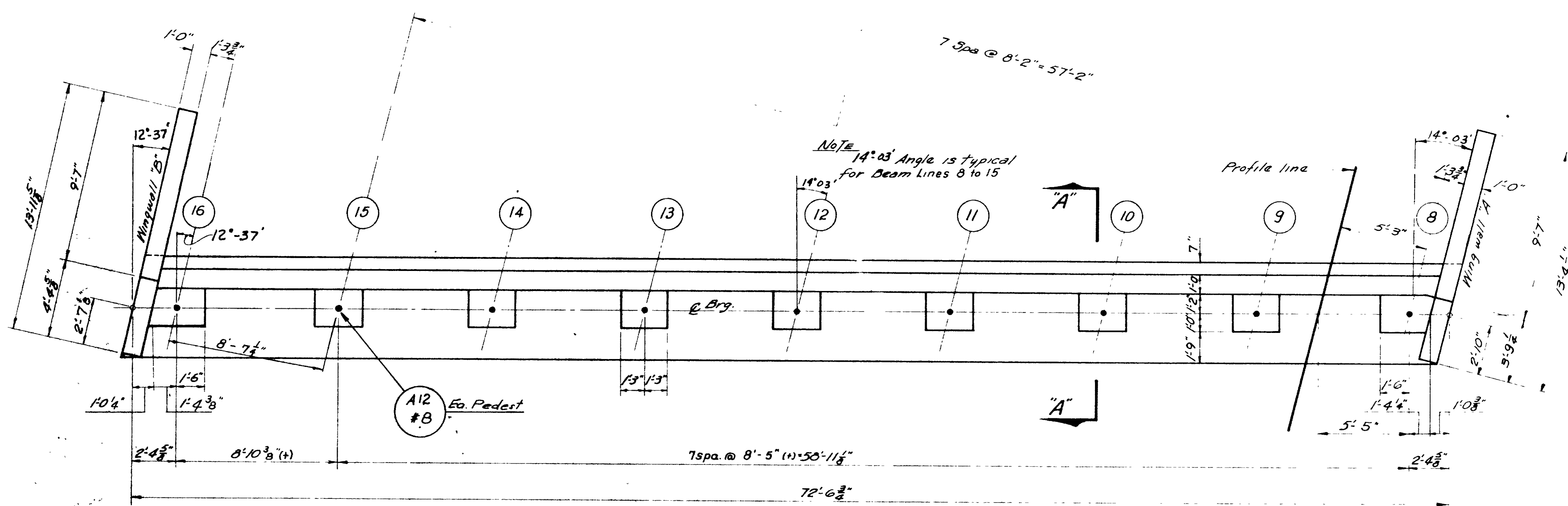
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DRAWN BY AEF	DATE 11/2/58	PROJECT NUMBER 869+87.30
QUANTIFIED BY HJS	CHECKED BY HGE	STATION WEBER
APPROVED BY M.J. CHRISTENSEN	ENGINEER	COUNTY

F-66 3 OF 26

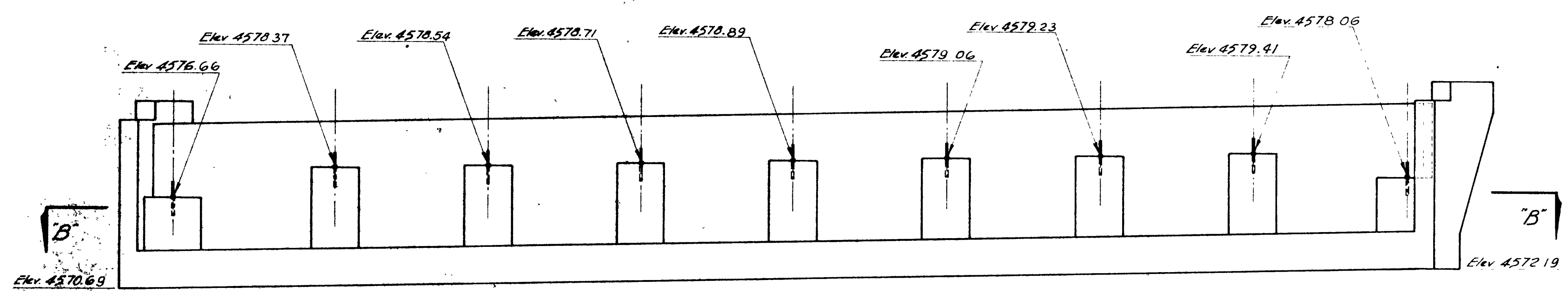


NOTE
Bridge seat bearing area (2" larger than bearing pad) shall be finished high and rubbed or ground to grade within ± 1/16". No Grouting.
B.F. = Both Faces.

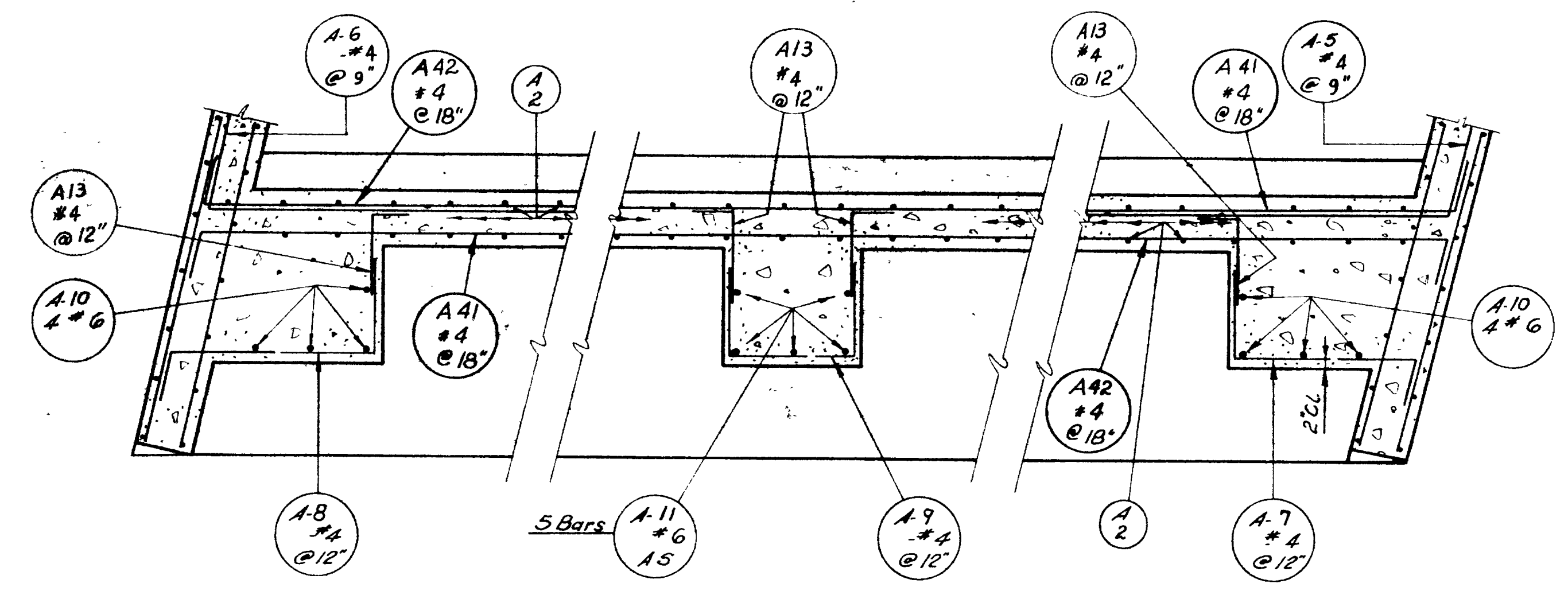
UTAH STATE DEPARTMENT OF HIGHWAYS			
STRUCTURES DIVISION			
DAVIS-WEBER Co. LINE TO 4400 So. 5600 SOUTH INTERCHANGE			
ABUTMENTS 1 & 4 S. B. L.			
DESIGNED BY: MW	CHECKED BY: mjc	DATE: I-15-8(20)334	
DRAWN BY: B.M.W.	IN CHARGE: HJS	PROJECT NUMBER:	
QUANTITY BY: HJS	DATE: HGE	849+87.30	
APPROVAL:	APPROVAL:	DATE:	WEBER
BR NO:	REV:	DATE:	REMARKS:
	F-66		4 OF 26



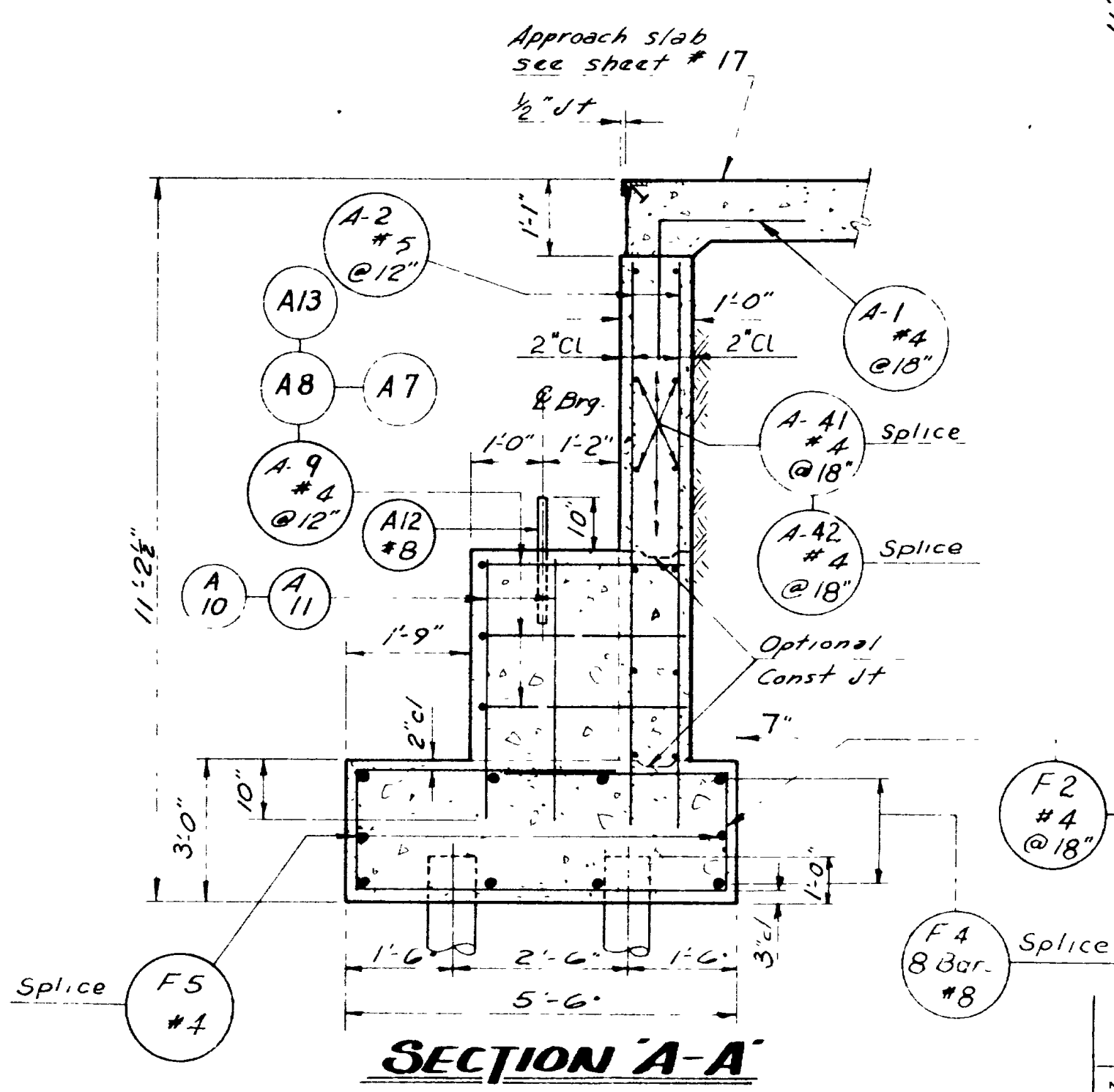
ABUTMENT PLAN



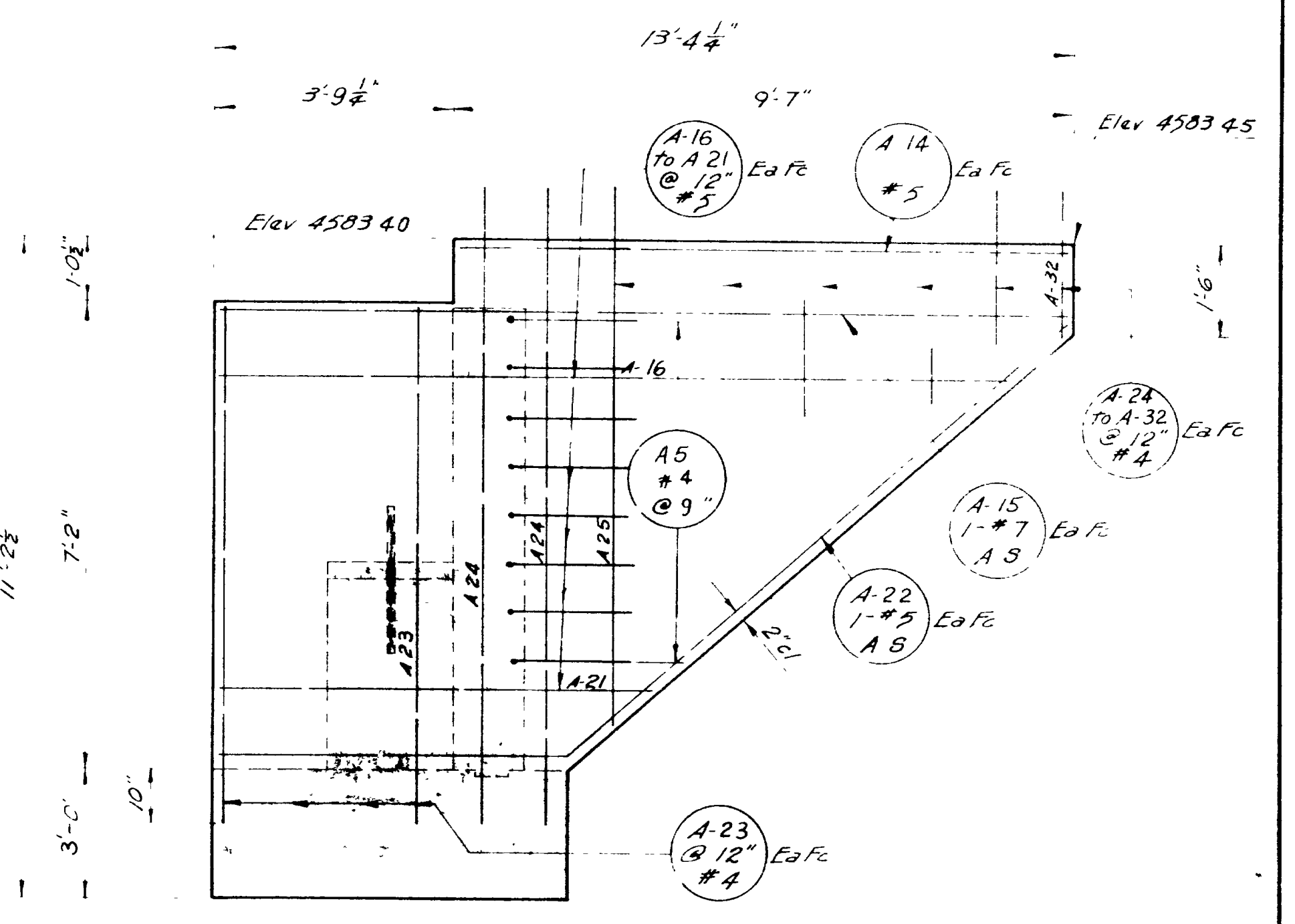
ABUTMENT ELEVATION



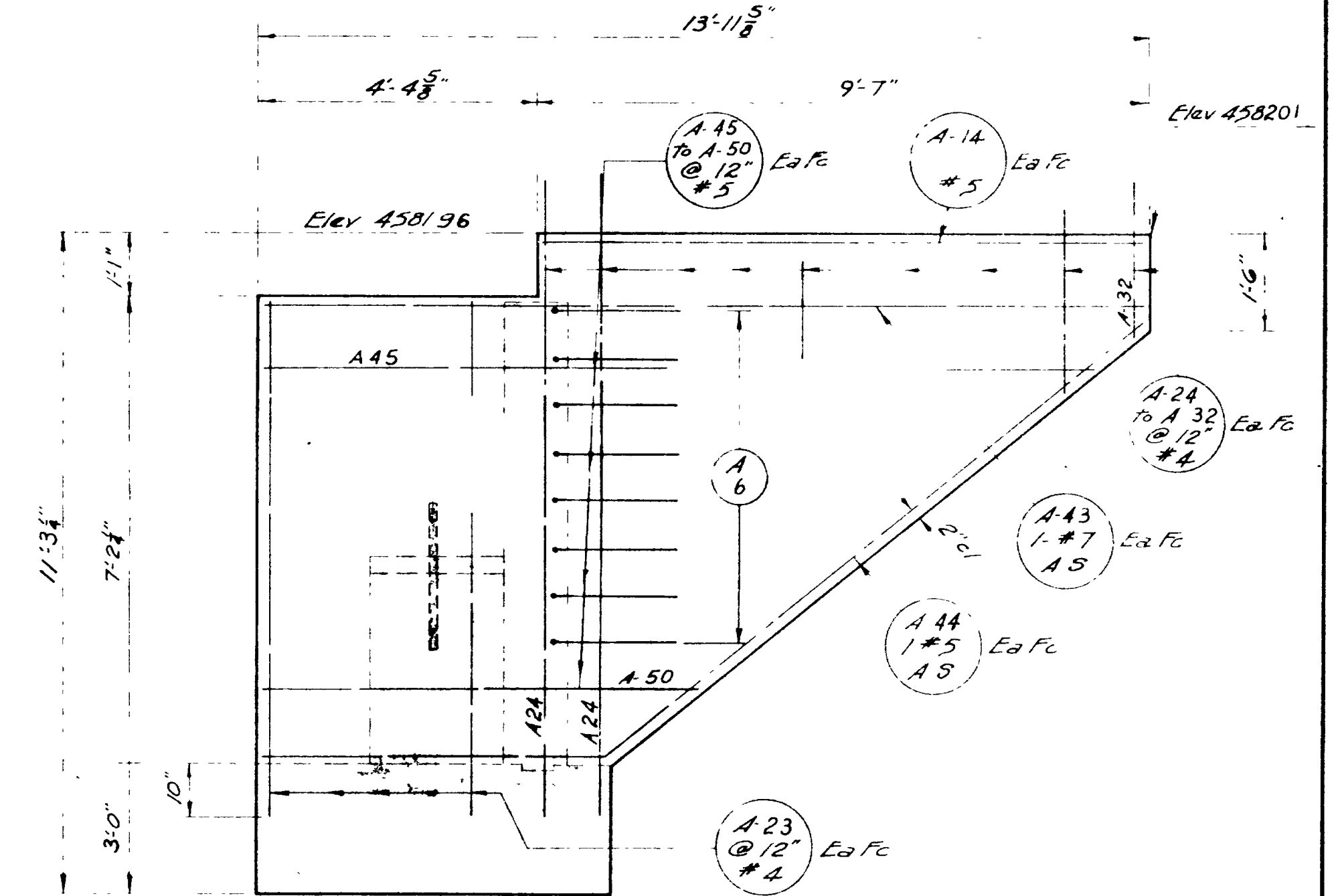
PLAN 'B-B'



SECTION 'A-A'



ELEVATION-WINGWALL 'A'



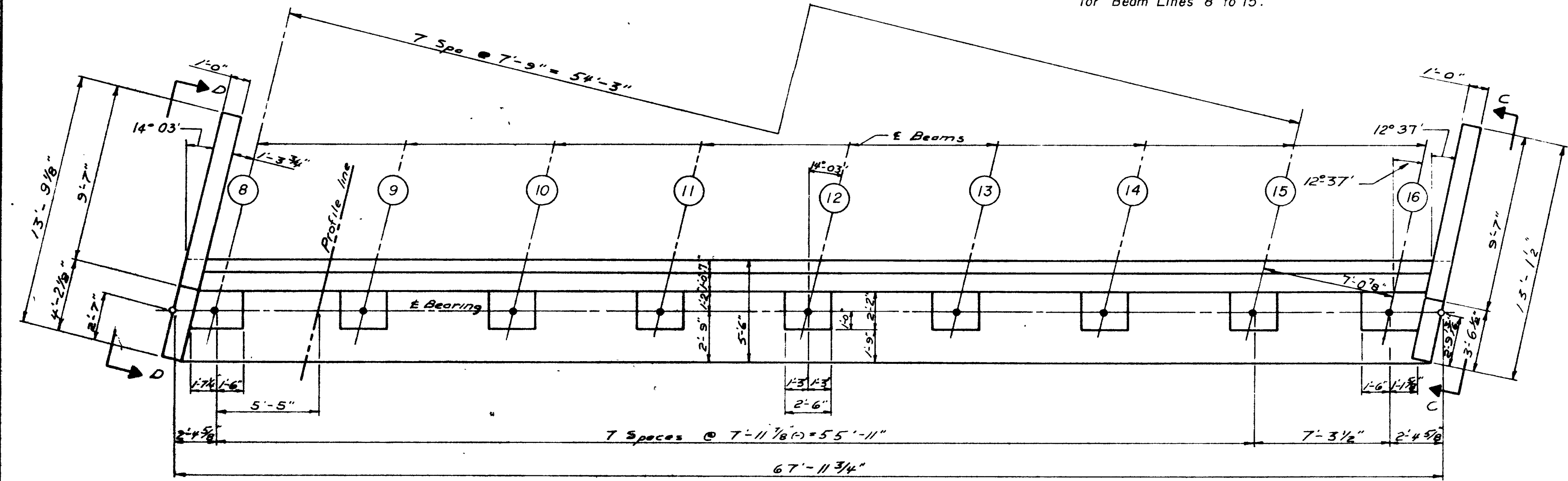
ELEVATION-WINGWALL 'B'

NOTE
 Bridge seat bearing area (2" larger than bearing plate) shall be finished high and rubbed or ground to grade within 1/16". NO grouting.

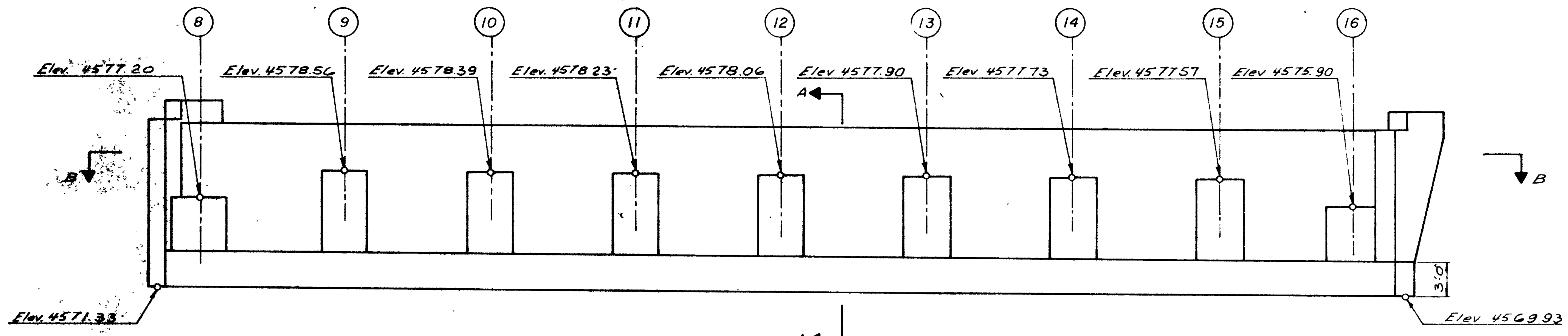
UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION		
DAVIS-WEBER Co. LINE TO 4400 So. 5600 SOUTH INTERCHANGE ABUTMENT #1 DETAILS N.B.L.		
DESIGNED BY: MW	CHECKED BY: MJC	DATE: 1/15/82(20)334
DRAWN BY: HJS	APPROVED BY: HJS	SCALE: AS SHOWN
QUANTITIES BY: HJS	REVISIONS BY: HJS	804+87.30
APPROVAL: [Signature]	RECOMMENDED BY: [Signature]	DATE: 1/15/82
APPROVED: [Signature]	DATE: 1/15/82	WILBER COUNTY

NO.	BY	DATE	REVISIONS	REMARKS

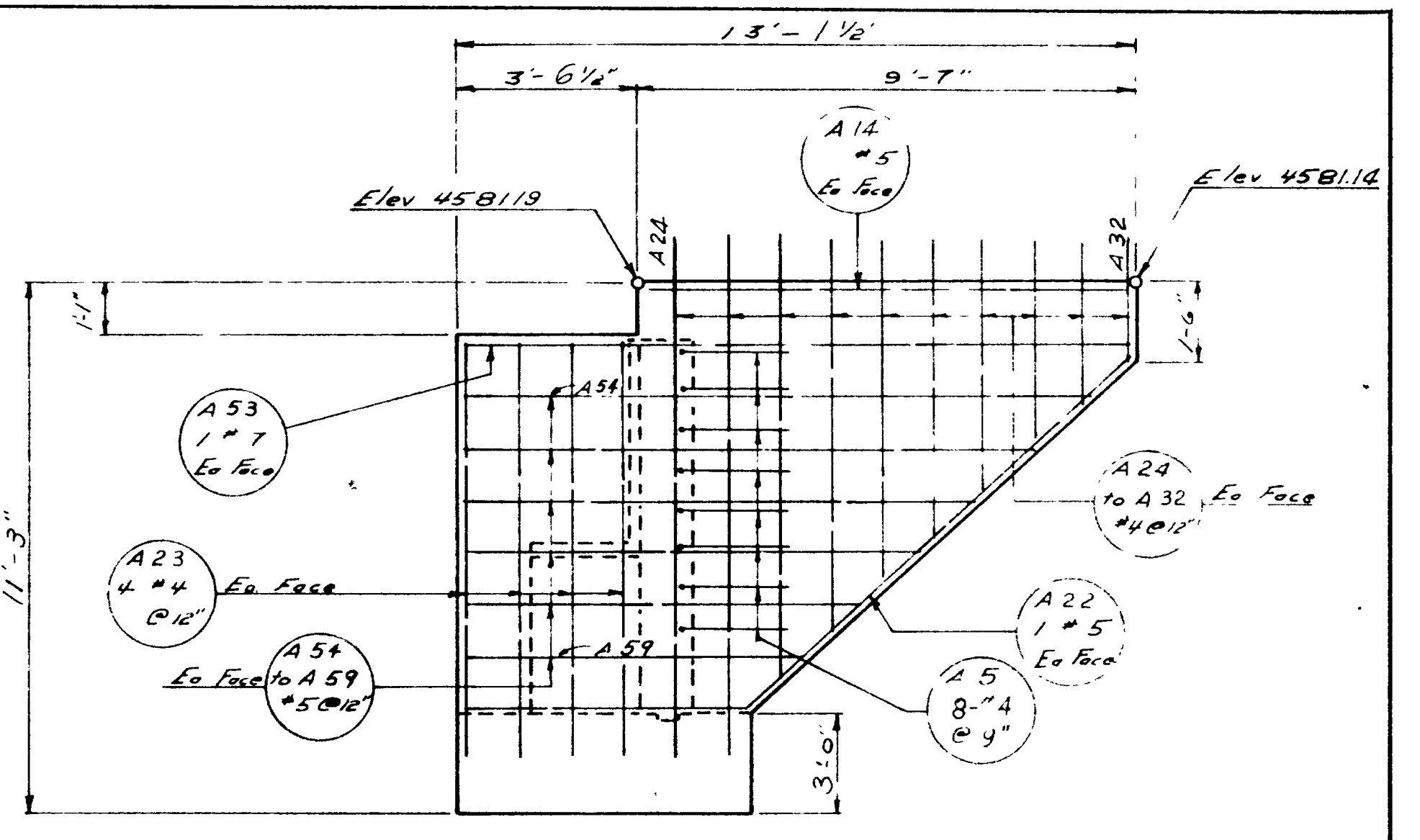
NOTE: 14° 03' Angle is typical for Beam Lines 8 to 15.



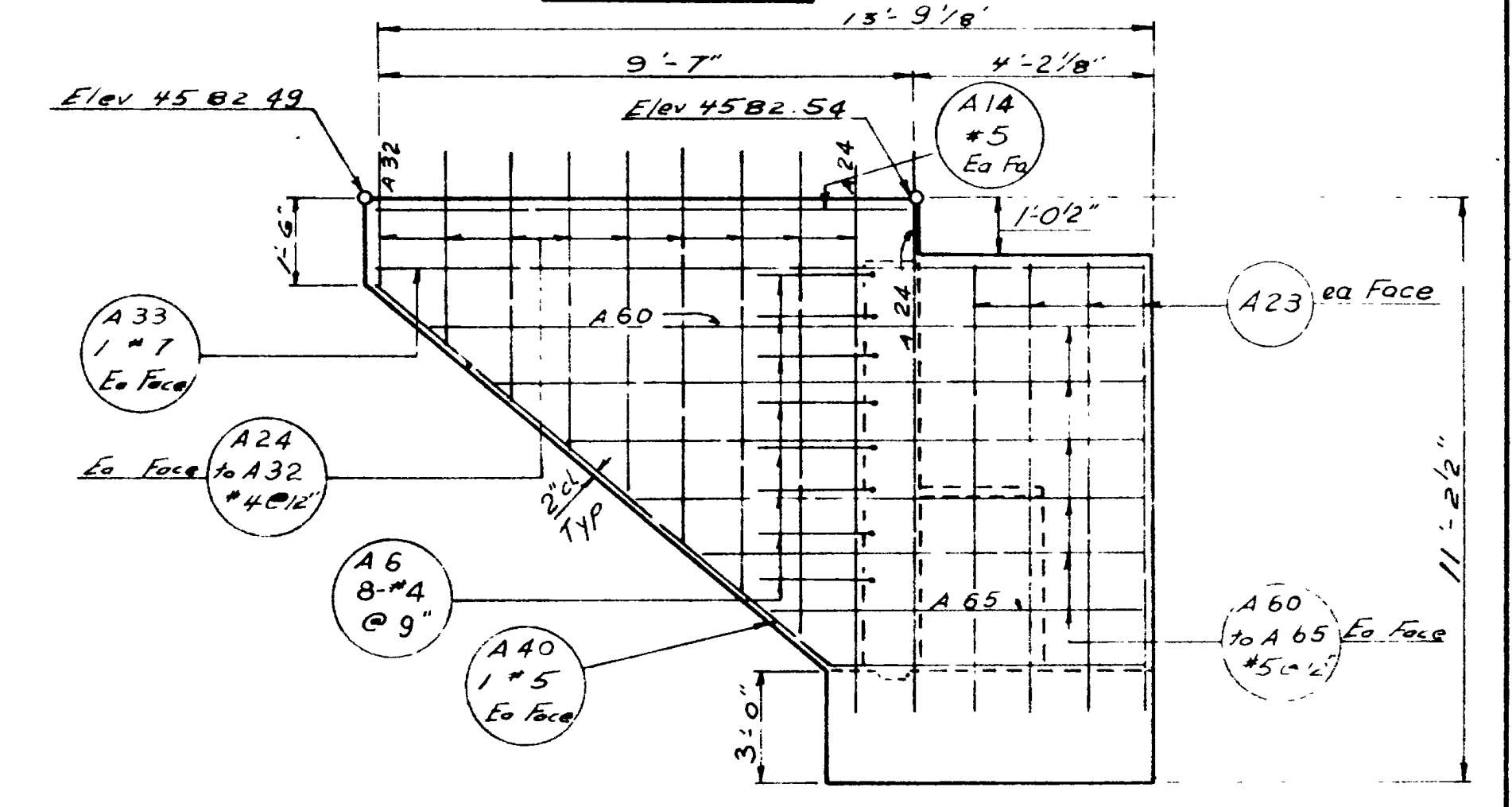
PLAN



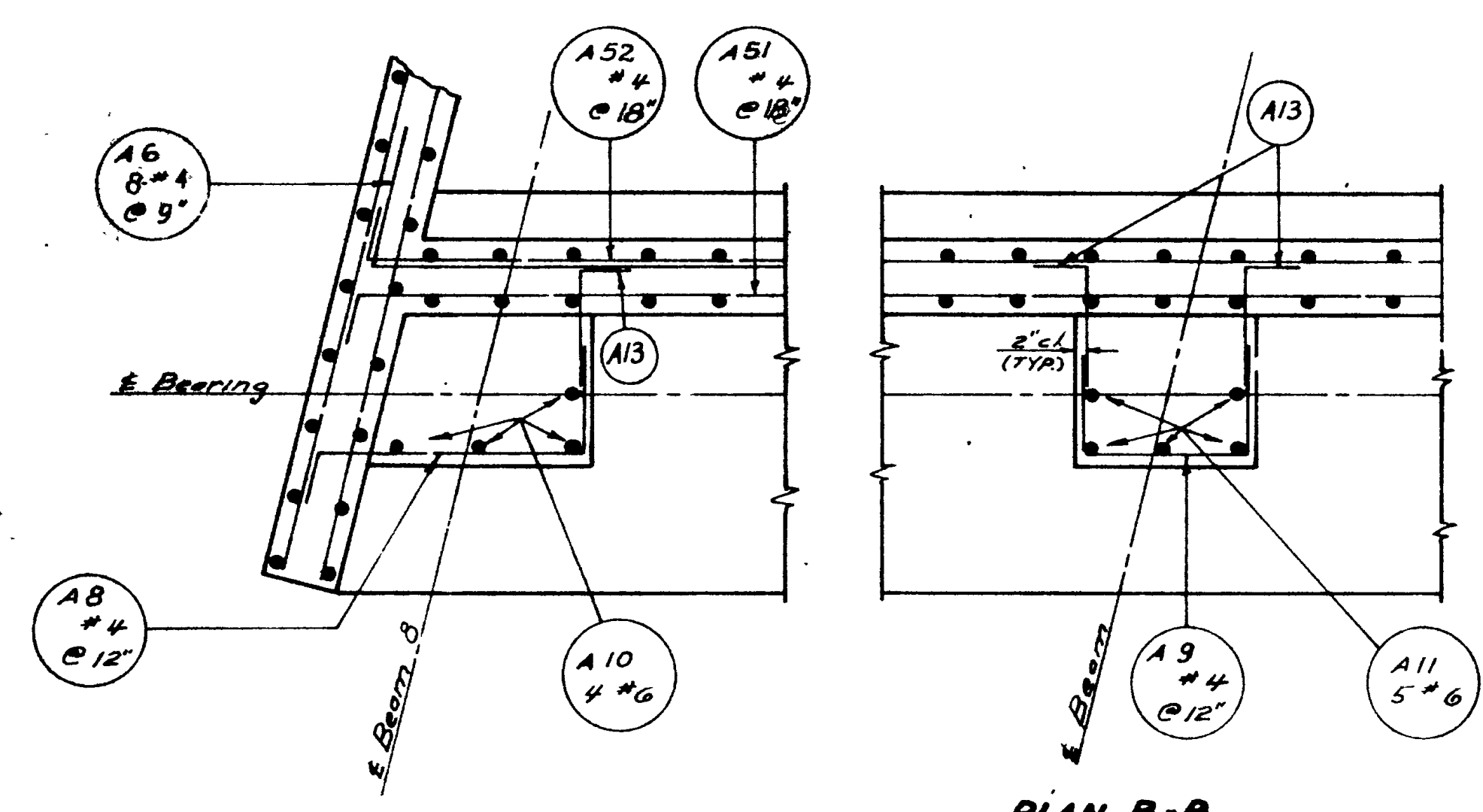
ELEVATION



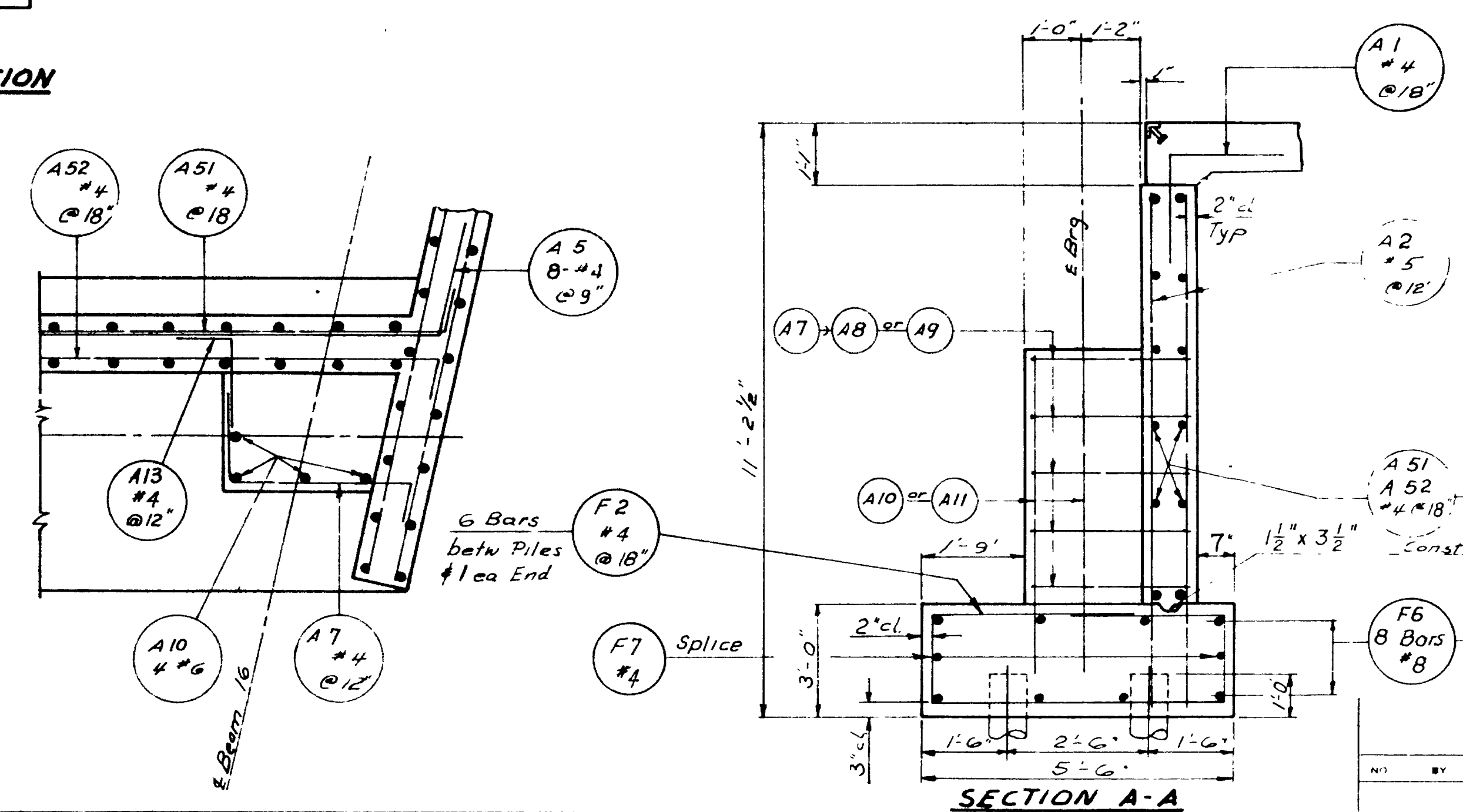
SECTION C-C



SECTION D-D



PLAN B-B

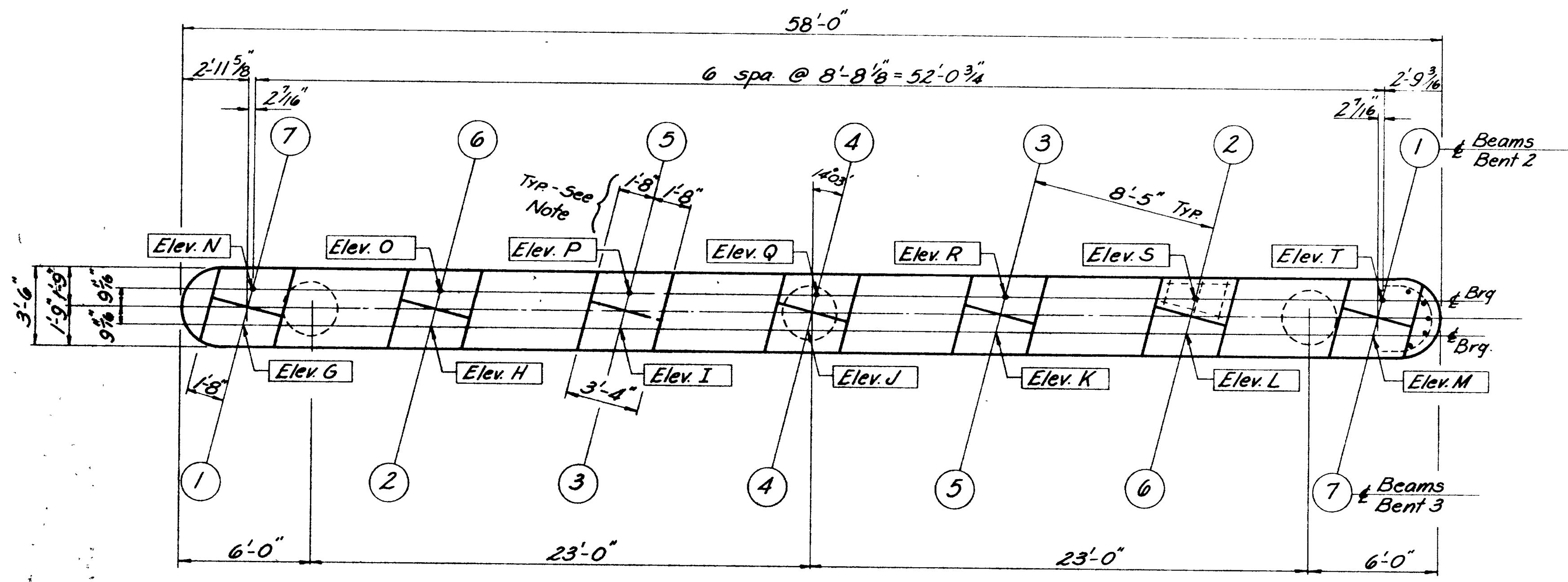


SECTION A-A

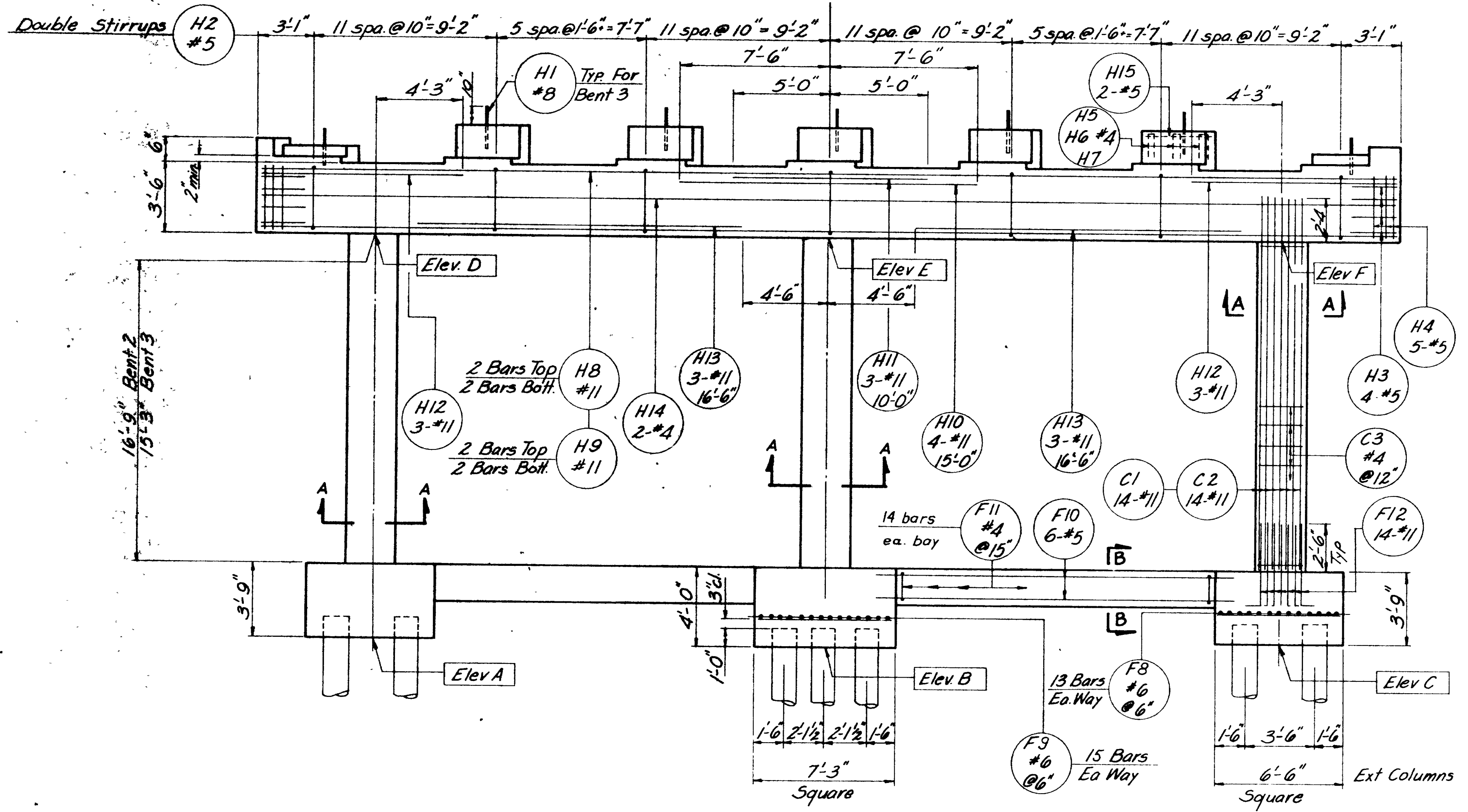
NOTES
 1. Bridge seat bearing area (2" larger than bearing pad) shall be finished high and rubbed or ground to grade within ± 1/16".
 No grouting

UTAH STATE DEPARTMENT OF HIGHWAYS			
STRUCTURES DIVISION			
DAVIS-WEBER Co. LINE TO 4400 So. 5600 SOUTH INTERCHANGE ABUTMENT 4 NBL			
DESIGNED BY: MW	NO. REVISIONS: mxc	DATE: 1-15-8(20)334	PROJECT NUMBER
DRAWN BY: HDS	CHECKED BY: HJS	DATE: 8-9-87	PROJECT NUMBER
QUANTITIES BY: HJS	DATE: 8-9-87	PROJECT NUMBER: 80918730	STATION
APPROVED BY: m jay christensen	DATE: 8-9-87	PROJECT NUMBER: 80918730	STATION
APPROVED BY: [Signature]	DATE: 8-9-87	PROJECT NUMBER: 80918730	STATION
BY: [Signature]		DATE: 8-9-87	STATION
REVISIONS		REMARKS	STATION
BR: [Signature]		DATE: 8-9-87	STATION
ERF: F-66		DATE: 8-9-87	STATION
6 OF 26		DATE: 8-9-87	STATION

REVISIONS: DATE, REVISION BY, DATE, REVISION BY, DATE, REVISION BY

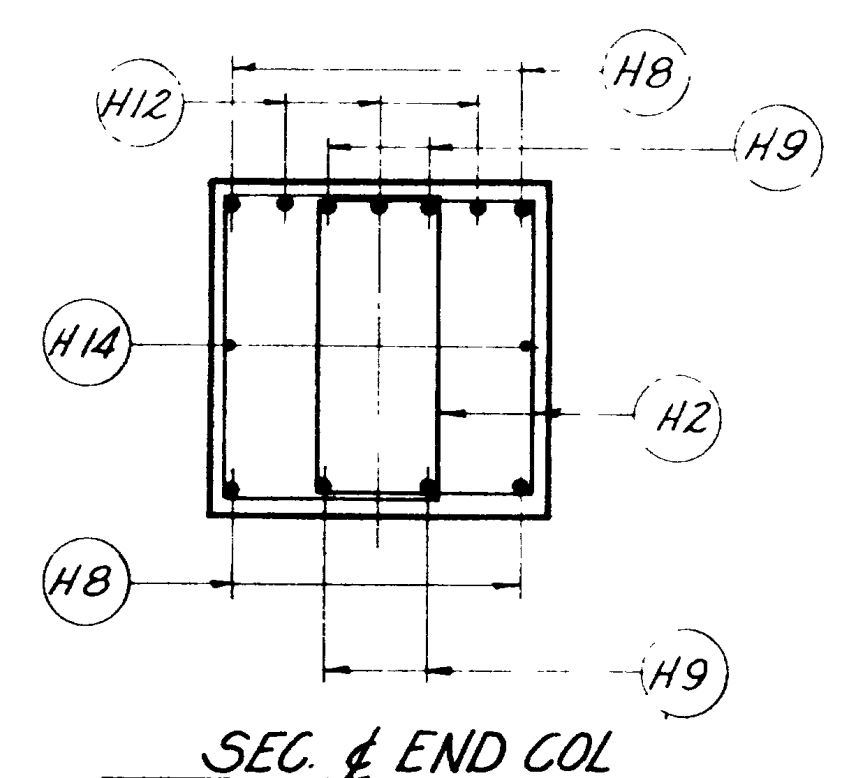


PLAN

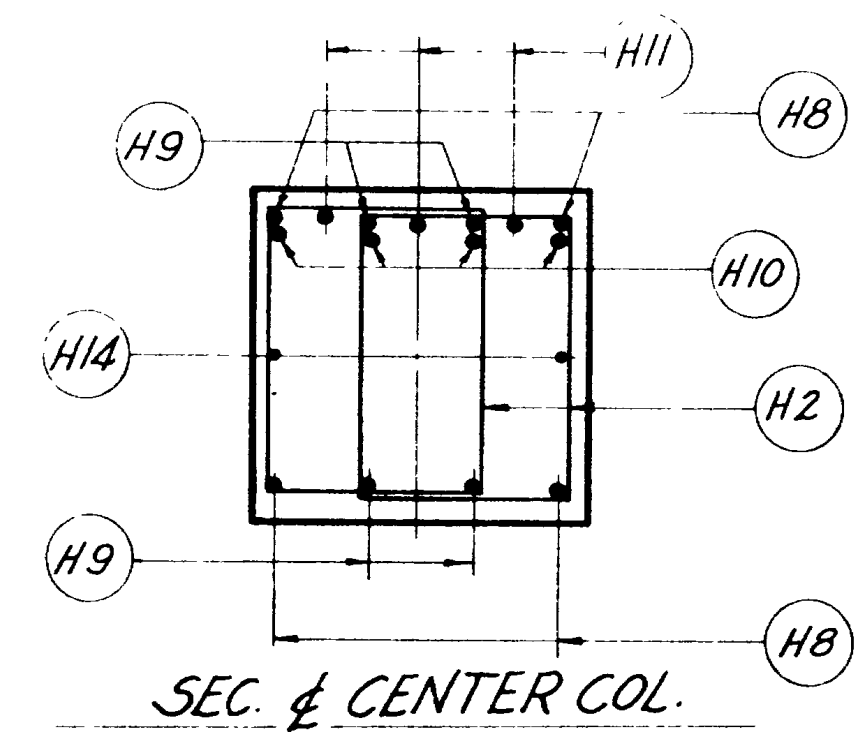


ELEVATION

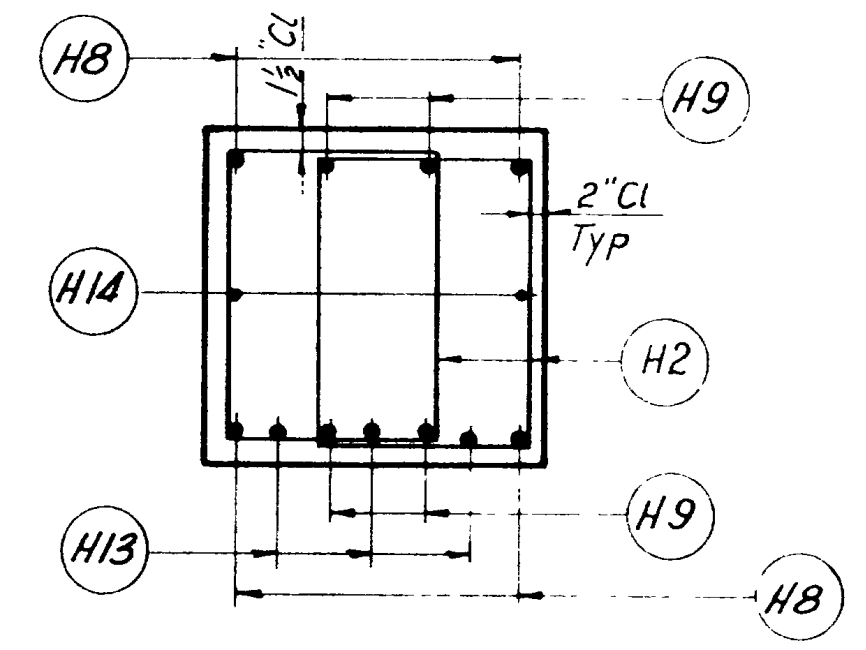
Bent 3 Looking North
Bent 2 Looking South



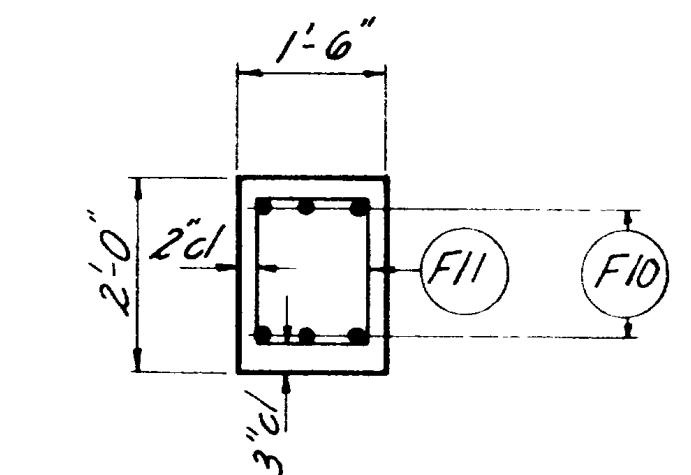
SEC. & END COL



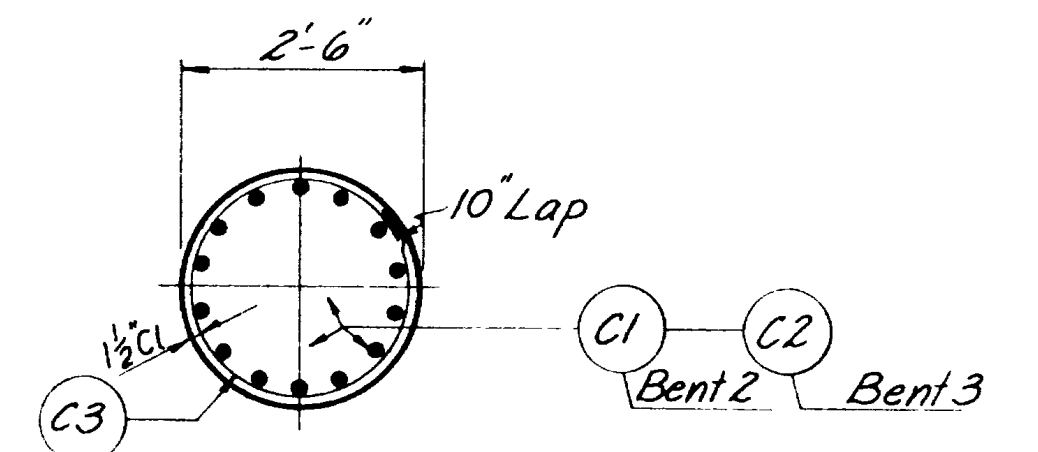
SEC. & CENTER COL.



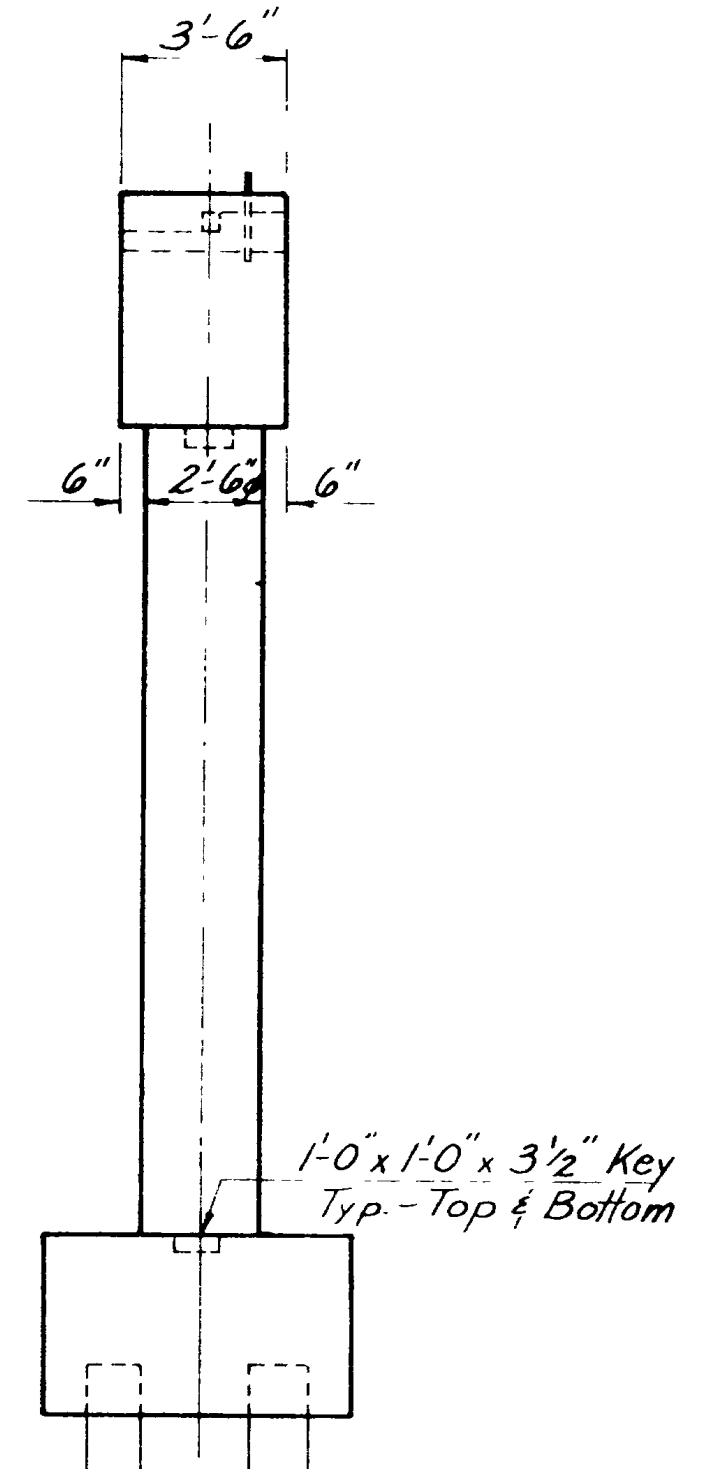
SEC. BETWEEN COLS.



SECTION B-B



SECTION A-A



END ELEVATION

ELEVATIONS		
POINT	BENT 2	BENT 3
A	45.4757	45.4758
B	46.91	47.74
C	46.75	48.40
D	68.07	66.58
E	67.66	66.99
F	67.25	67.40
G	71.83	70.23
H	71.67	70.38
I	71.51	70.54
J	71.36	70.69
K	71.20	70.85
L	71.05	71.01
M	70.89	71.16
N	71.96	70.50
O	73.32	72.17
P	73.16	72.33
Q	73.00	72.48
R	72.85	72.64
S	72.69	72.80
T	71.02	71.43

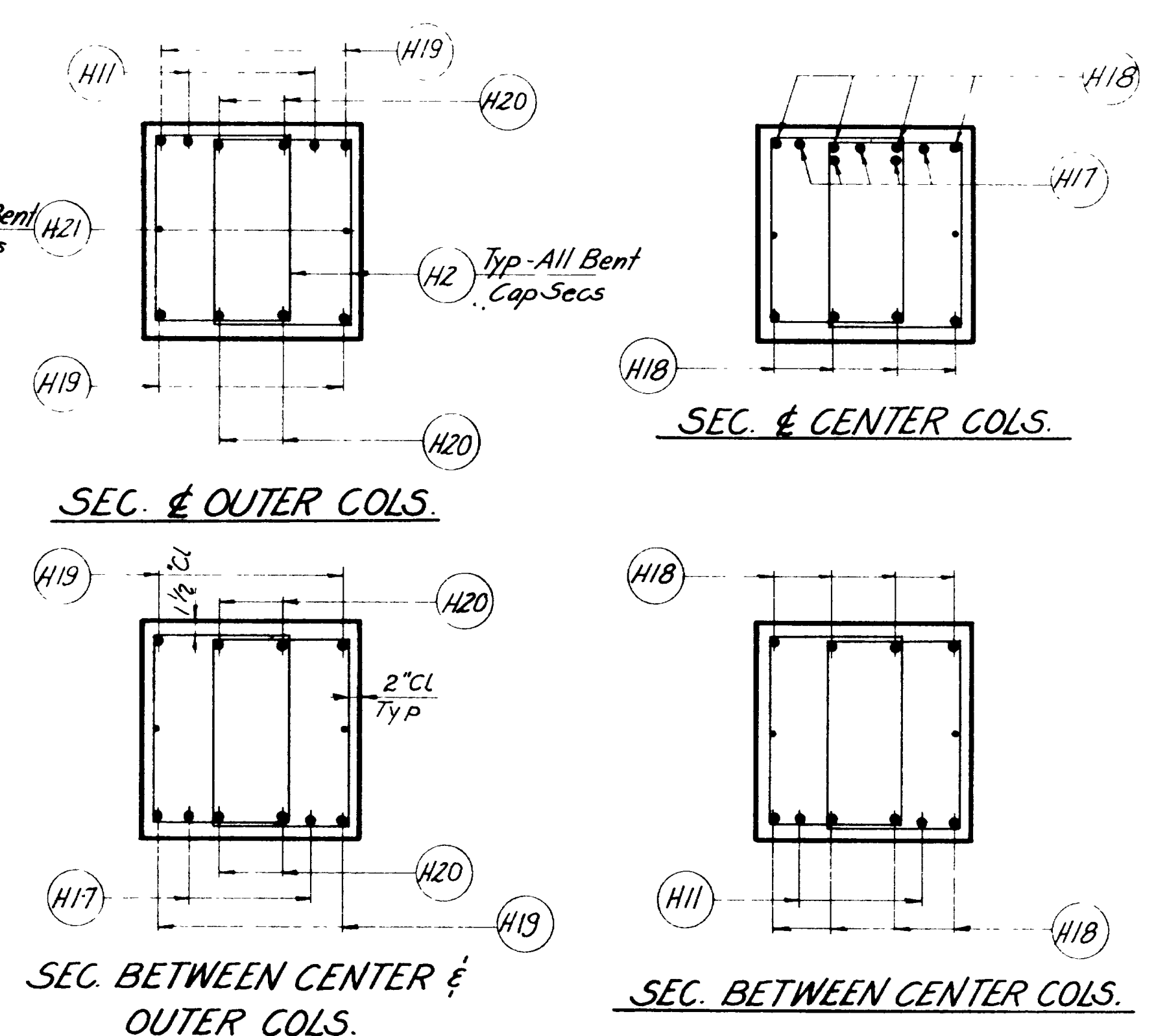
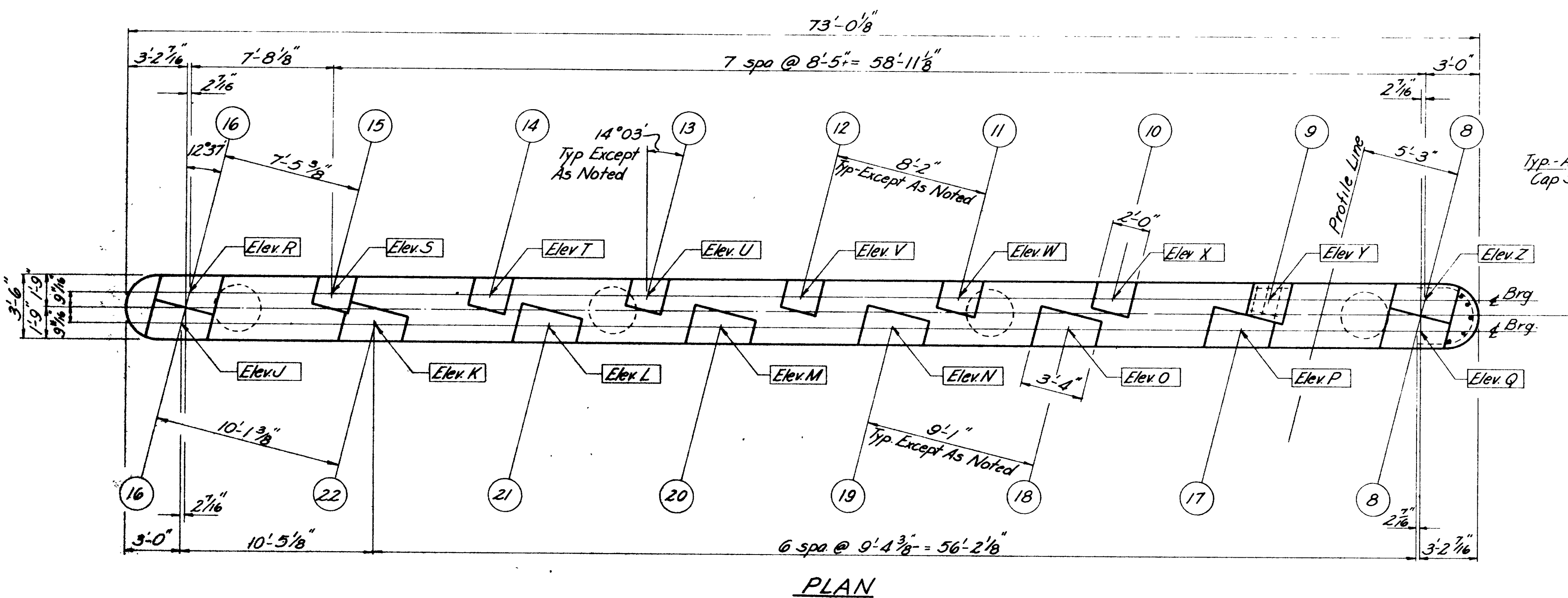
NOTE:
Bridge Seat Bearing Areas (2" larger than masonry plate) shall be finished high and rubbed or ground to grade within ± 1/16". No grouting.
For Bearing and Anchor Bolt details Span 2 see sheet SS-3.

UTAH STATE DEPARTMENT OF HIGHWAYS
SALT LAKE CITY, UTAH
STRUCTURES DIVISION
DAVIS-WEBER Co. LINE TO 4400 So.
5600 SOUTH INTERCHANGE
BENTS 2 & 3 SBL

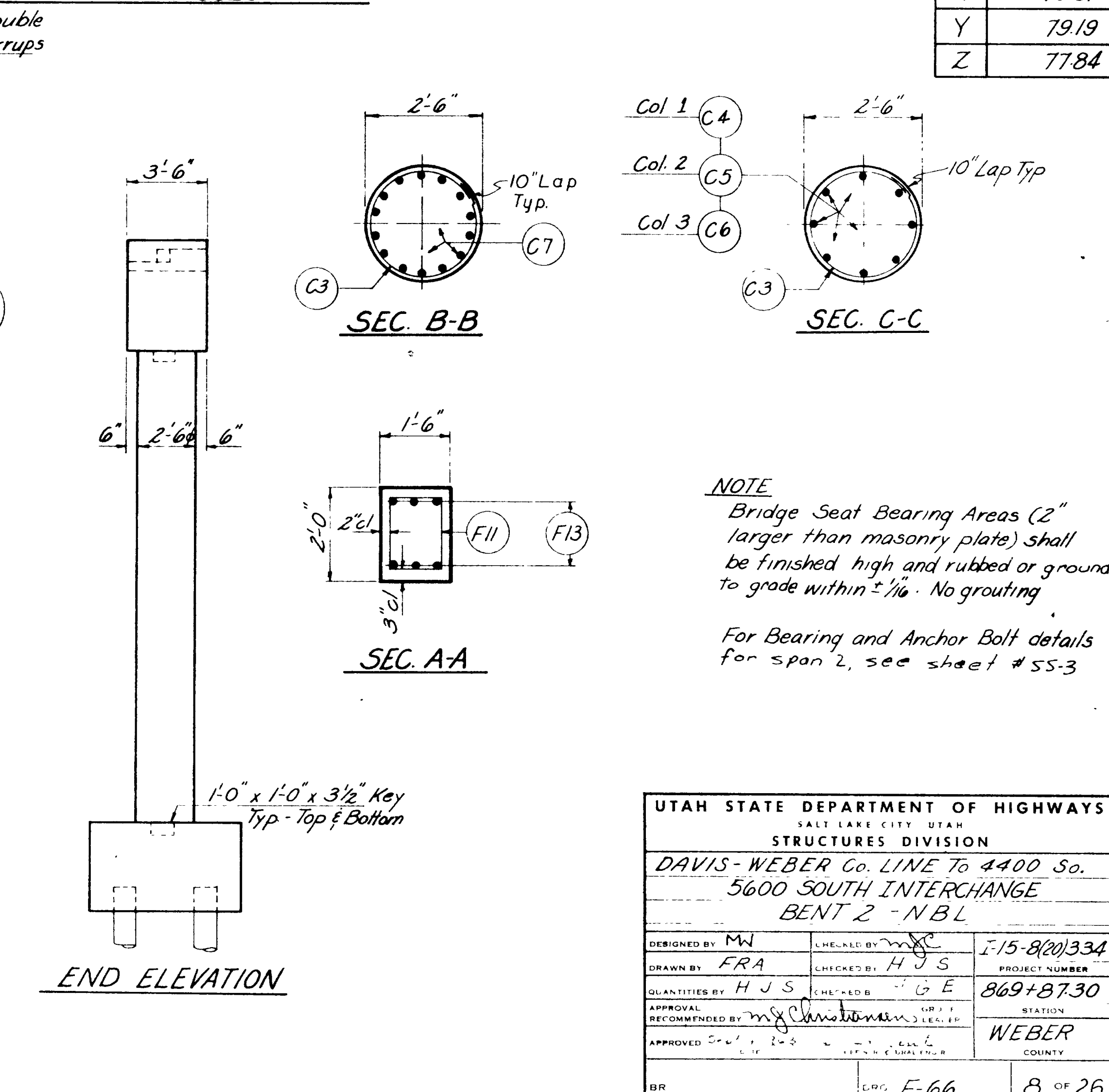
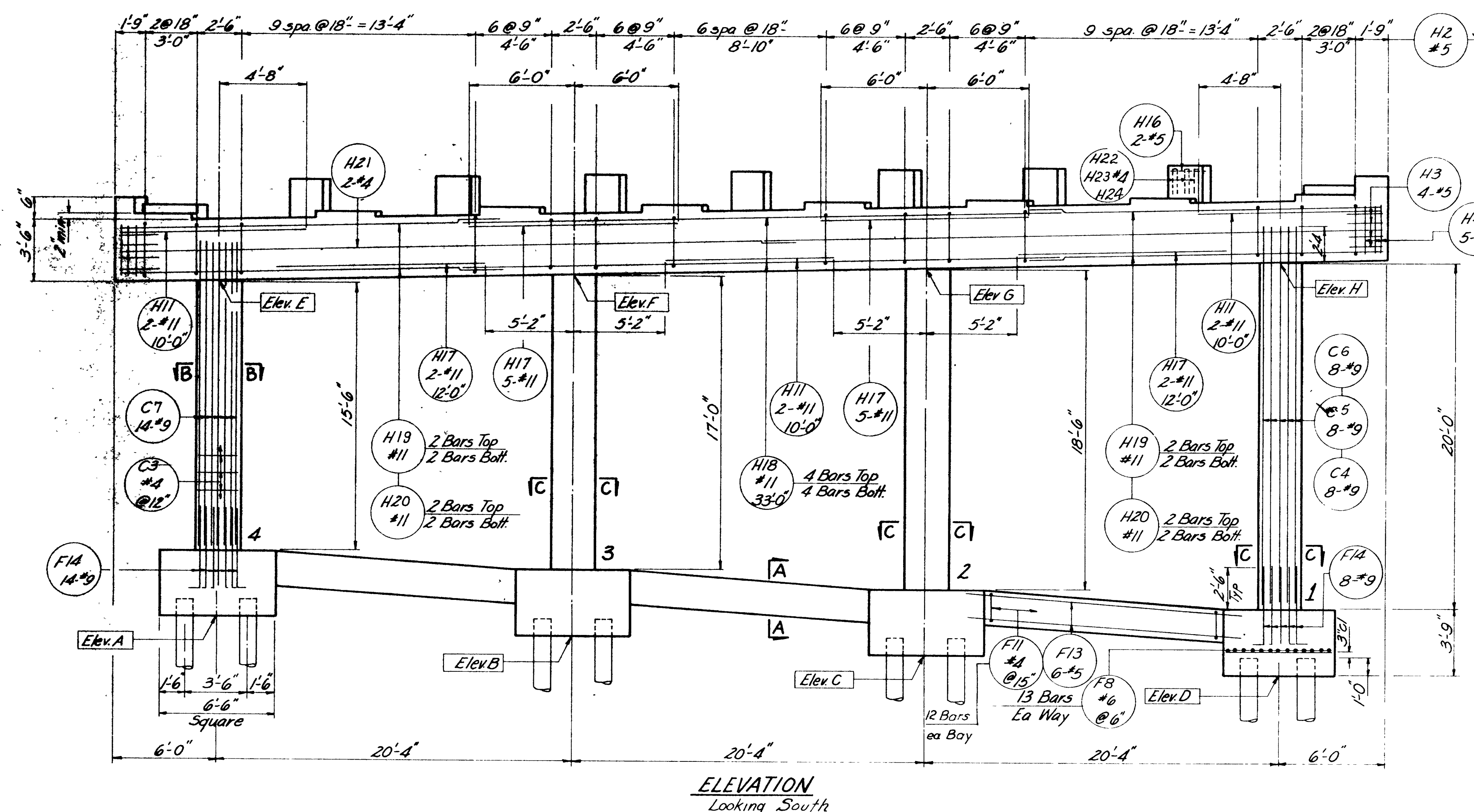
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DRAWN BY: FRA	CHECKED BY: HJS	QUANTITIES BY: HJS
APPROVAL: MJC	DATE: 1-15-80	STATION: 869+87.30
APPROVED: MJC		COUNTY: WEBER

BR NO: ORG F-66 7 OF 26

REVISIONS: DATE, REVISION BY, DATE, REVISION BY

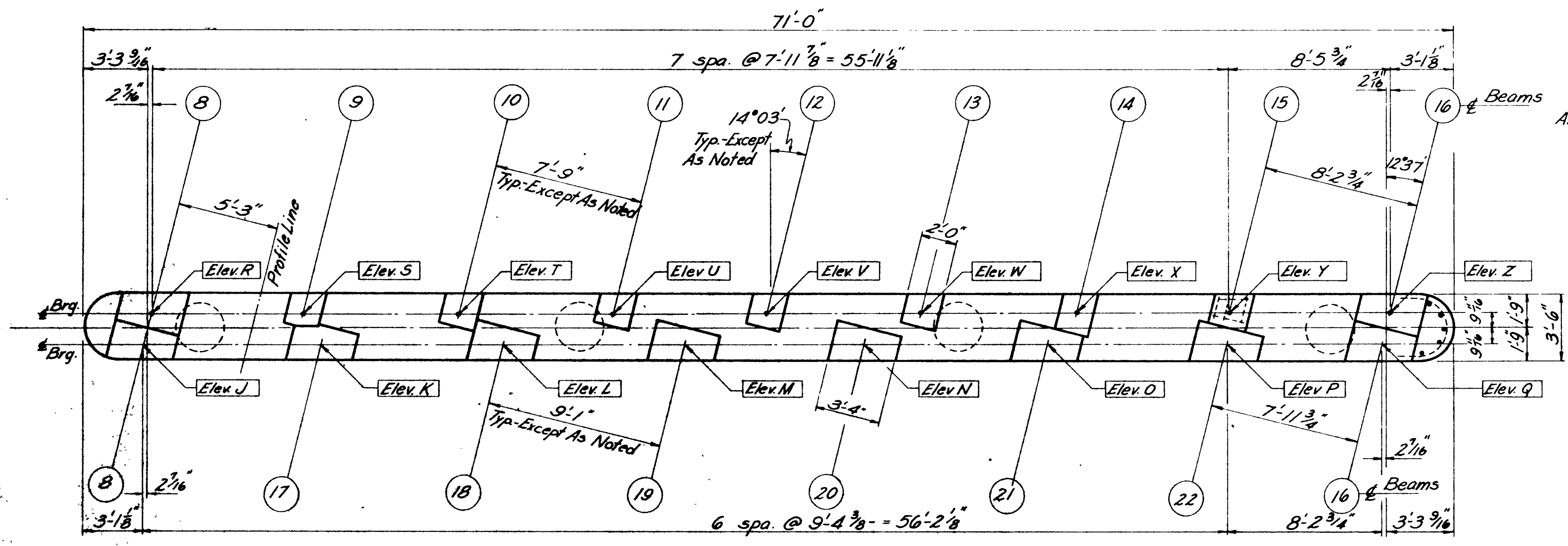


POINT	ELEVATION
A	45.5345
B	52.37
C	51.29
D	50.20
E	72.70
F	73.12
G	73.54
H	73.95
J	76.34
K	76.56
L	76.75
M	76.94
N	77.13
O	77.33
P	77.52
Q	77.71
R	76.47
S	78.15
T	78.32
U	78.49
V	78.67
W	78.84
X	79.01
Y	79.19
Z	77.84

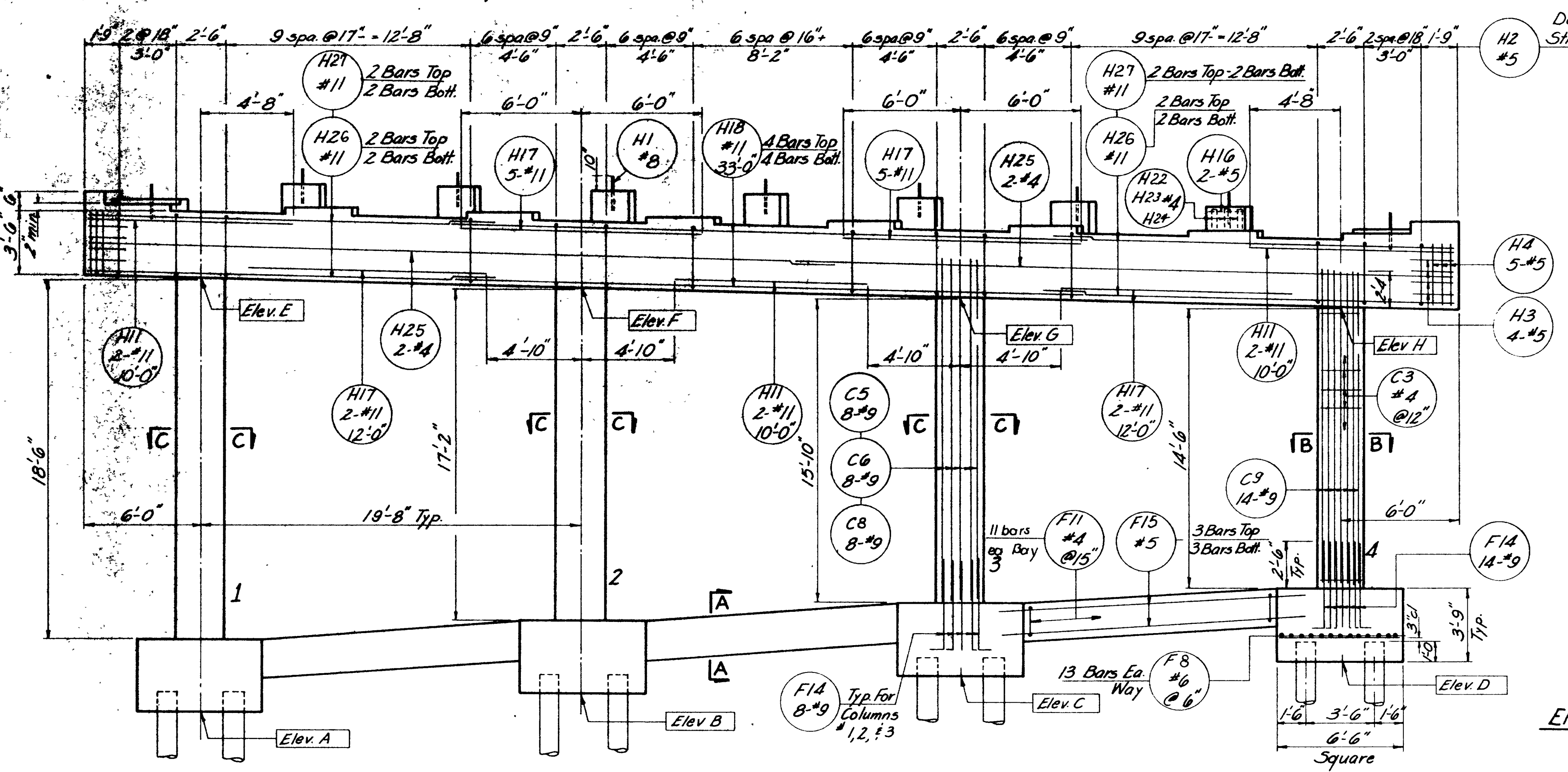


UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS-WEBER Co. LINE To 4400 So. 5600 SOUTH INTERCHANGE BENT 2 - NBL			
DESIGNED BY: MW	CHECKED BY: HJS	PROJECT NUMBER: I-15-8(20)334	STATION: WEBER COUNTY
DRAWN BY: FRA	CHECKED BY: G.E.	PROJECT NUMBER: 869+87.30	STATION: WEBER COUNTY
QUANTITIES BY: HJS	CHECKED BY: G.E.	PROJECT NUMBER: 869+87.30	STATION: WEBER COUNTY
APPROVAL: [Signature]	DATE: [Blank]	PROJECT NUMBER: 869+87.30	STATION: WEBER COUNTY
APPROVED: [Signature]	DATE: [Blank]	PROJECT NUMBER: 869+87.30	STATION: WEBER COUNTY
BR NO:	EDG. F-66	8 OF 26	

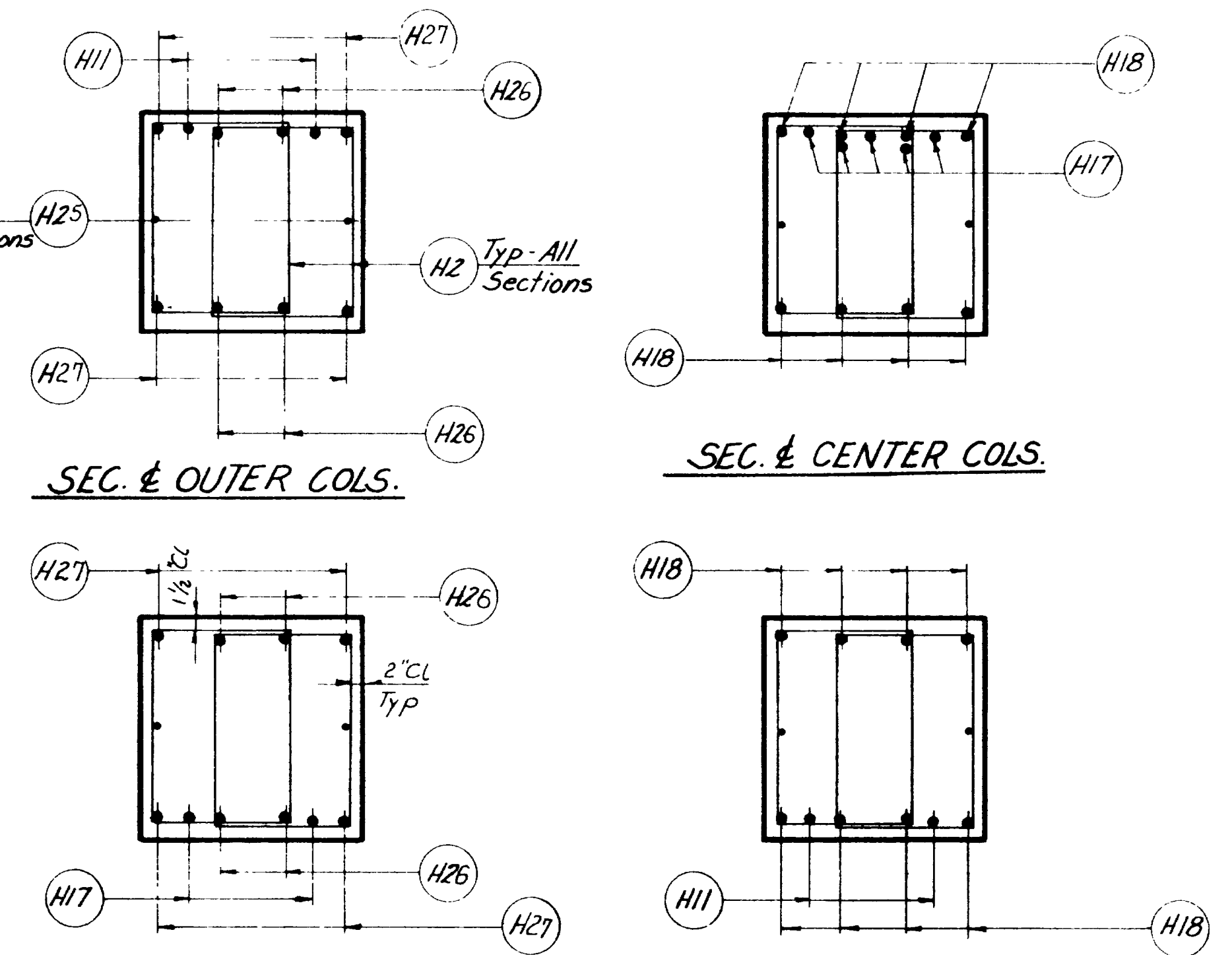
REVISIONS: 1. DATE: 2. DATE: 3. DATE: 4. DATE: 5. DATE:



PLAN

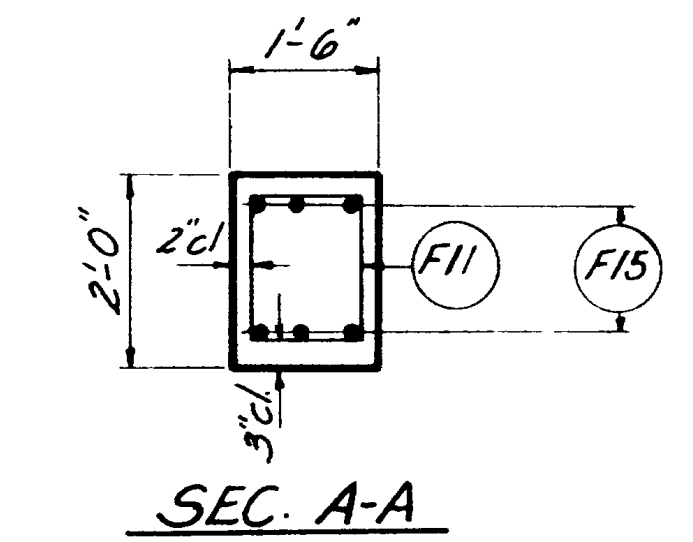


ELEVATION
Looking North

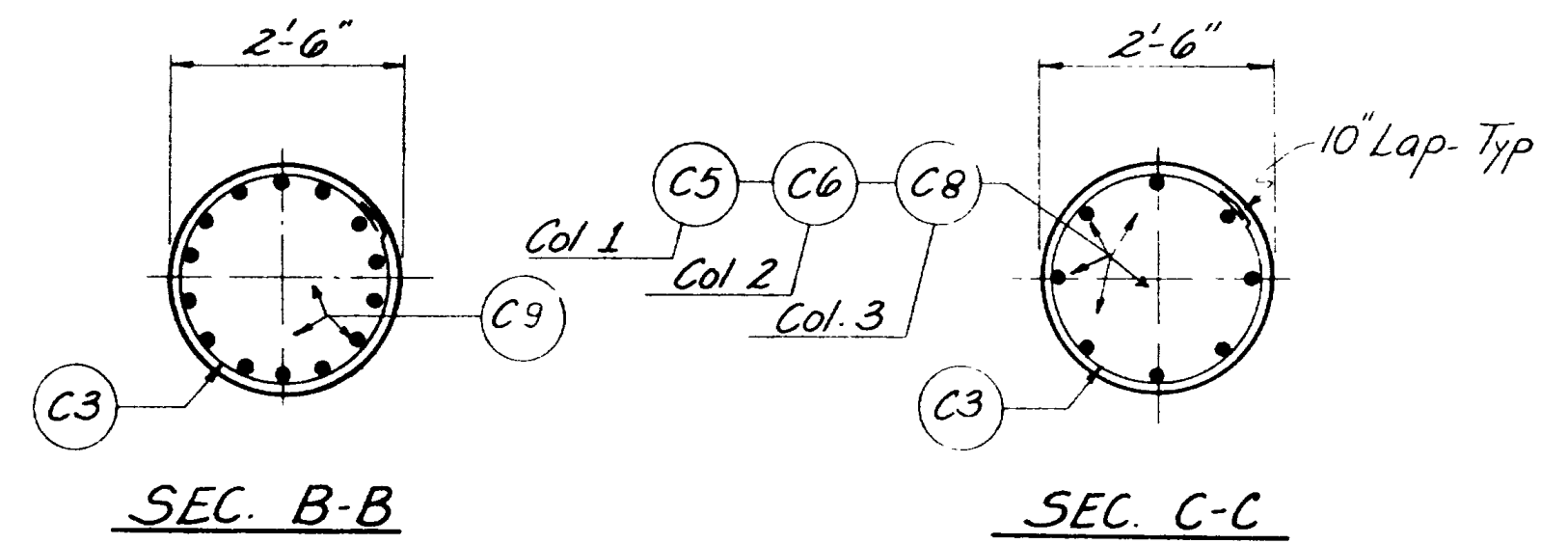


SEC. BETWEEN CENTER & OUTER COLS.

SEC. BETWEEN CENTER COLS.

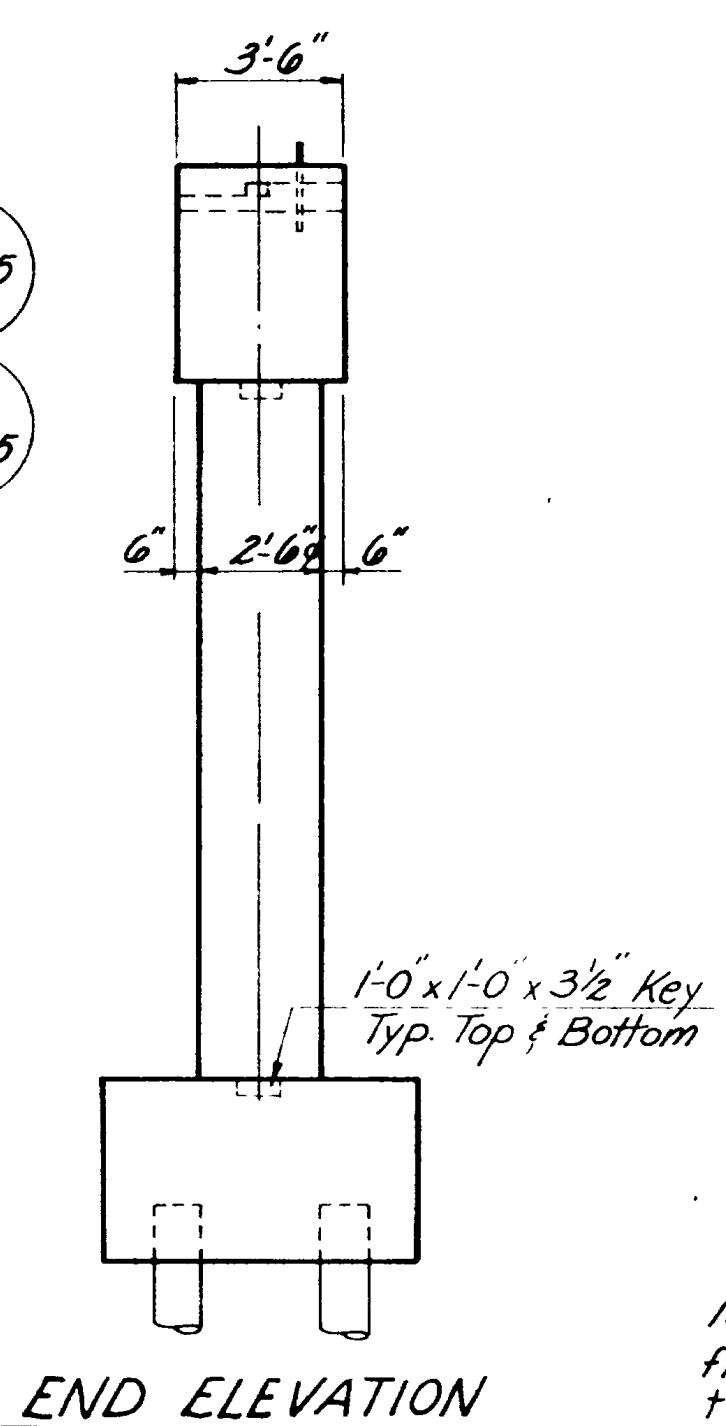


SEC. A-A



SEC. B-B

SEC. C-C



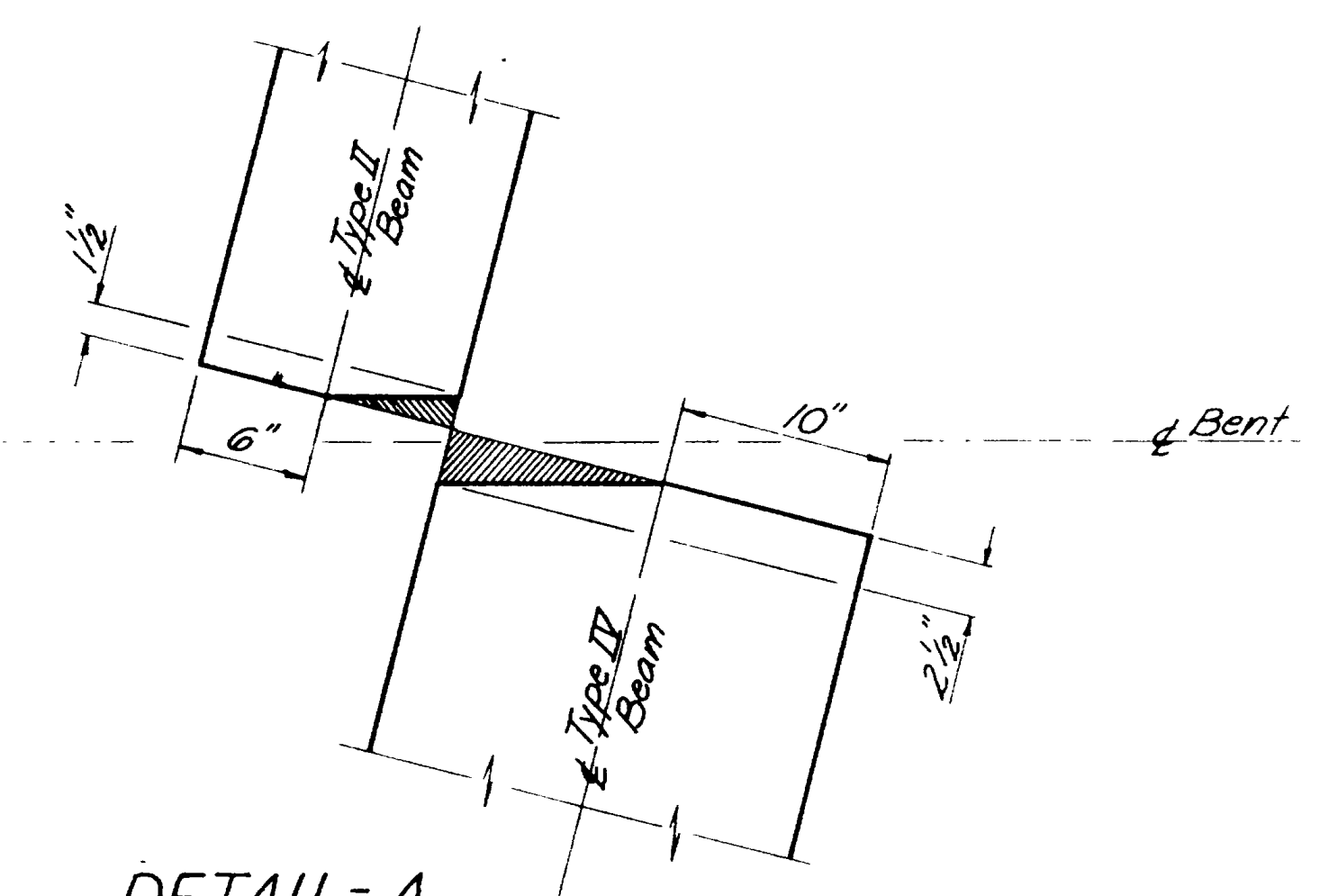
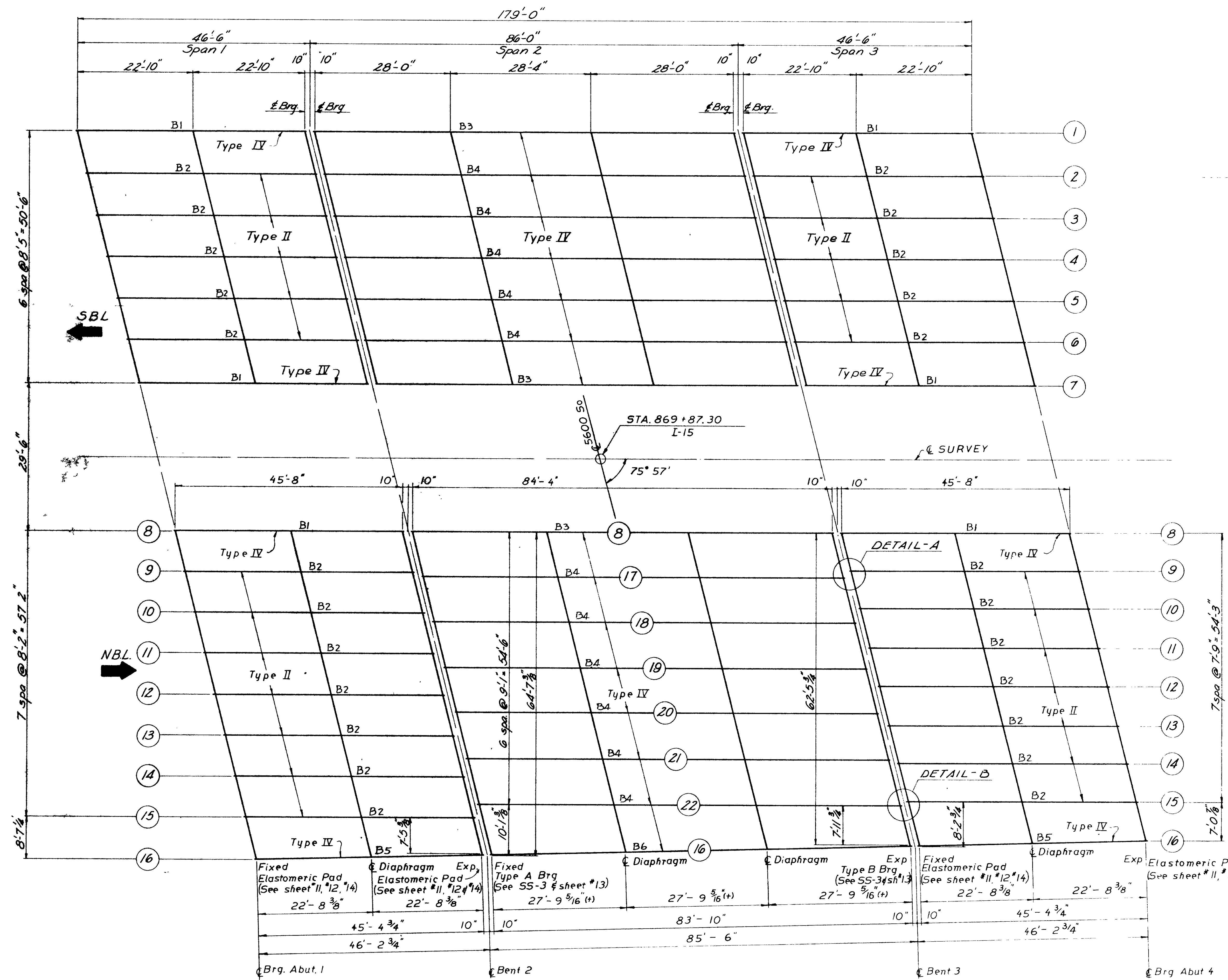
END ELEVATION

NOTE
 Bridge Seat Bearing Areas (2" larger than masonry plate) shall be finished high and rubbed or ground to grade within 1/4". No grouting.
 For Bearing and Anchor Bolt details for span 2 see sheet #SS-3

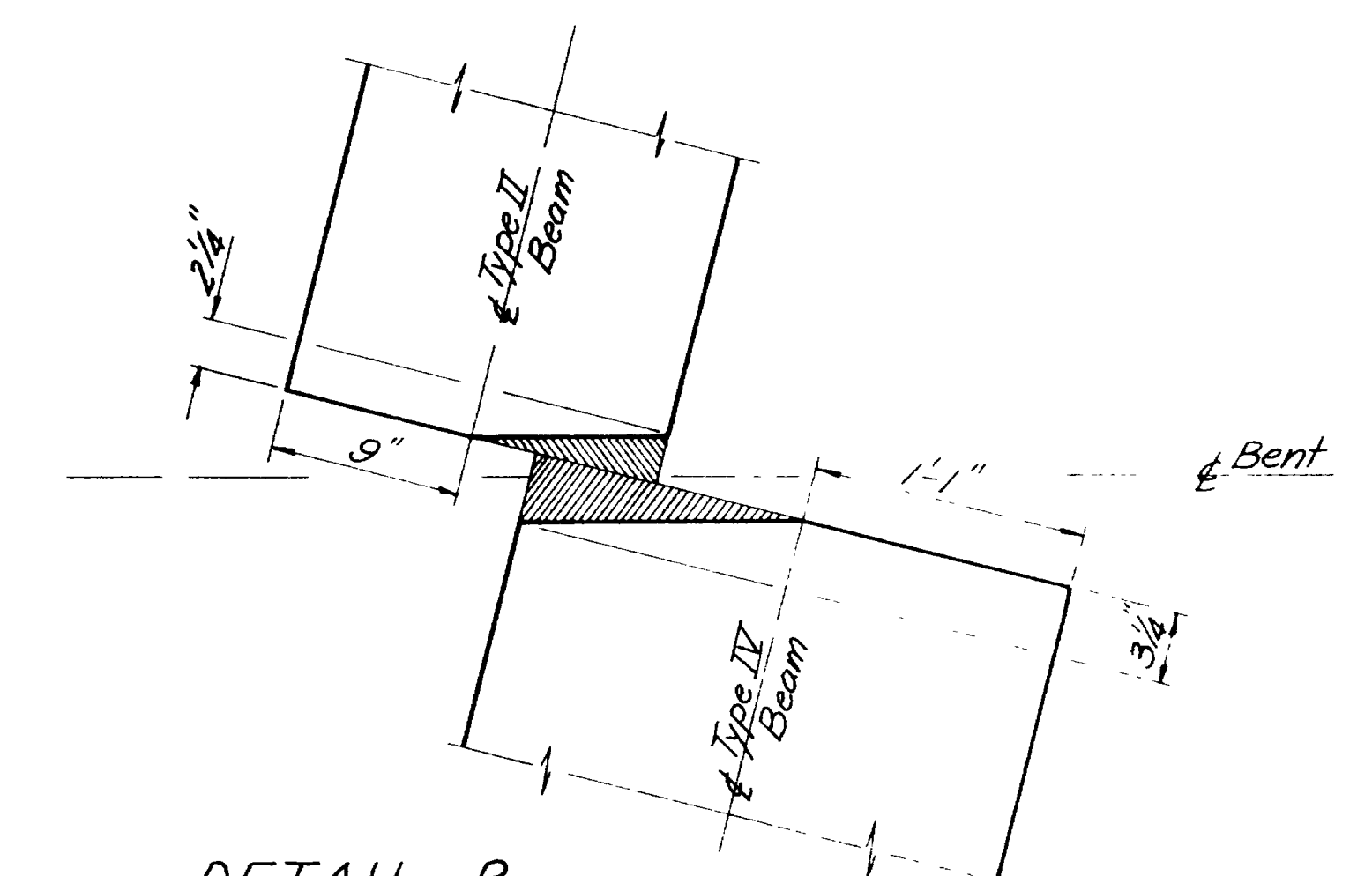
POINT	ELEVATION
A	455115
B	5207
C	5300
D	5392
E	7340
F	7299
G	7258
H	7217
J	7715
K	7696
L	7676
M	7657
N	7638
O	7619
P	7599
Q	7582
R	7742
S	7877
T	7861
U	7845
V	7828
W	7812
X	7795
Y	7779
Z	7609

UTAH STATE DEPARTMENT OF HIGHWAYS		
SALT LAKE CITY, UTAH		
STRUCTURES DIVISION		
DAVIS - WEBER Co. LINE To 4400 So.		
5600 SOUTH INTERCHANGE		
BENT 3 N.B.L		
DESIGNED BY: MW	CHECKED BY: MYC	I-15-8(20)334
DRAWN BY: FRA	CHECKED BY: HJS	PROJECT NUMBER
QUANTITIES BY: HJS	CHECKED BY: HGE	869+87.30
APPROVAL	RECOMMENDED BY: My Christian	STATION
APPROVED - HJS	LEADER	WEBER
BR NO	DRG NO: F-66	COUNTY
		9 OF 26

REVISIONS BY DATE

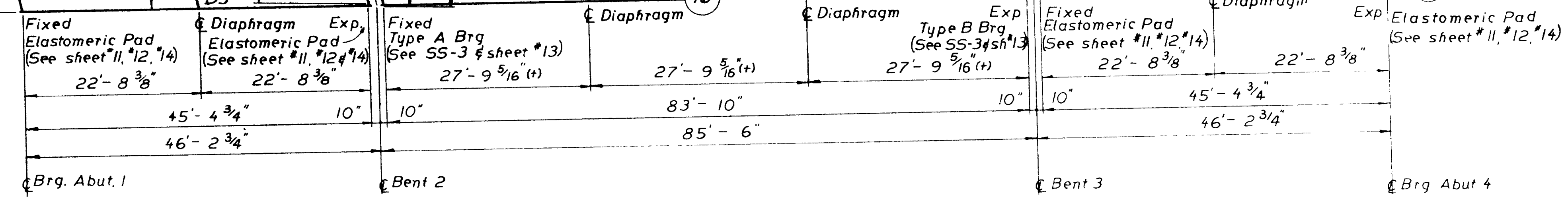


DETAIL-A
TOP FLANGE BLOCKOUT DETAILS
 Full Depth for Positions Indicated
 2" Deep - All Other Beams



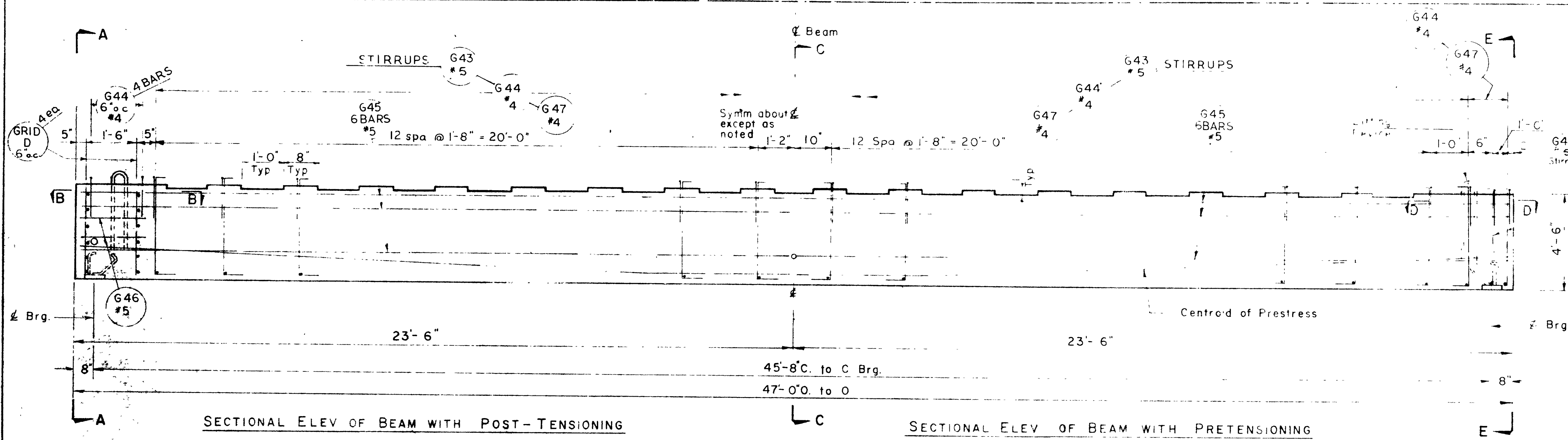
DETAIL-B
BOTTOM FLANGE BLOCKOUT DETAILS
 Full Depth for Positions Indicated

- NOTES**
- 1 A - For Spans 1 & 3
 1 Ext Beams - Type IV
 2 Interior Beams - Type II
 - B - For Span 2
 All Beams - Type IV
 2. For Diaphragm Details - See Sheet #19
 3. Numbers shown in Circle e.g. (3) indicate Beam Line



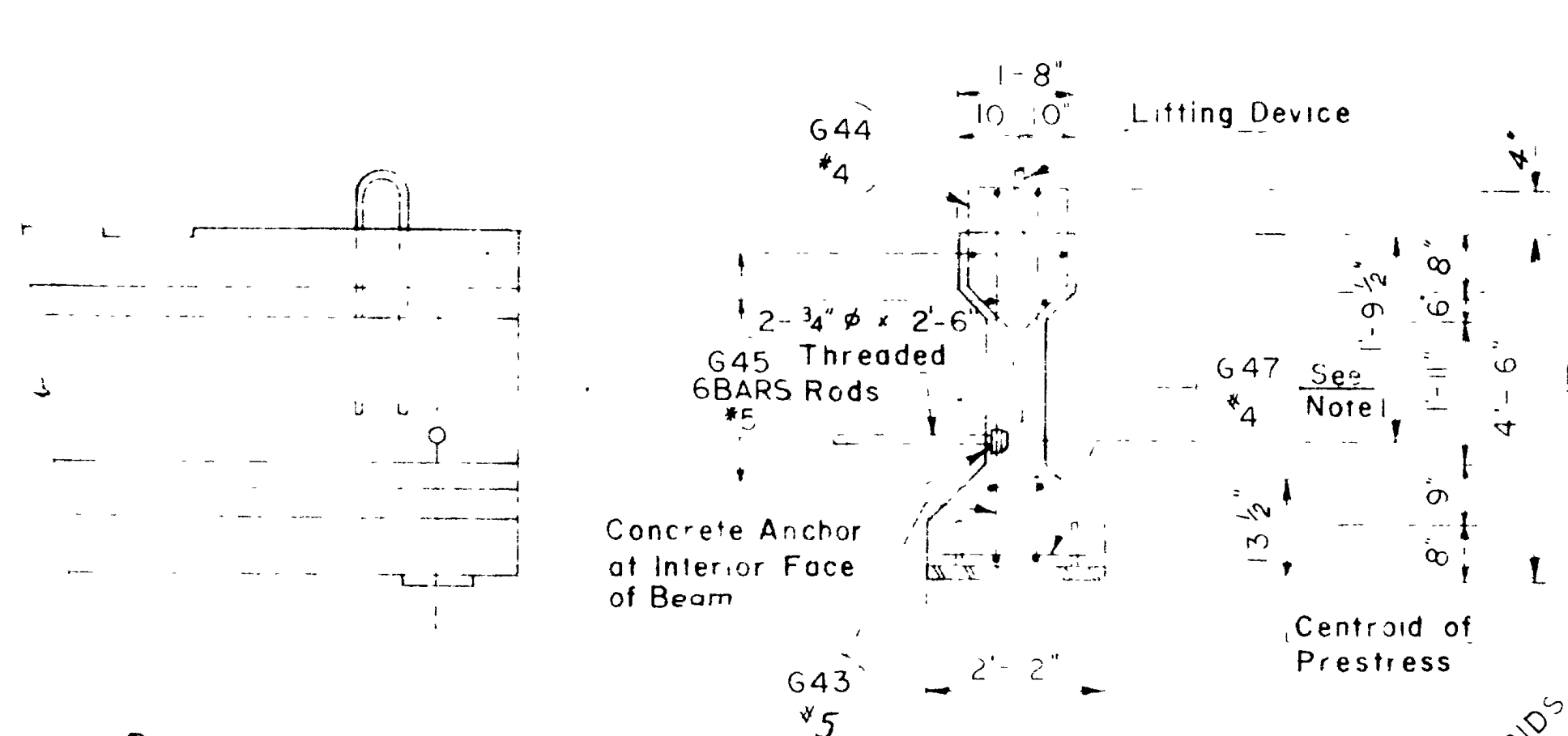
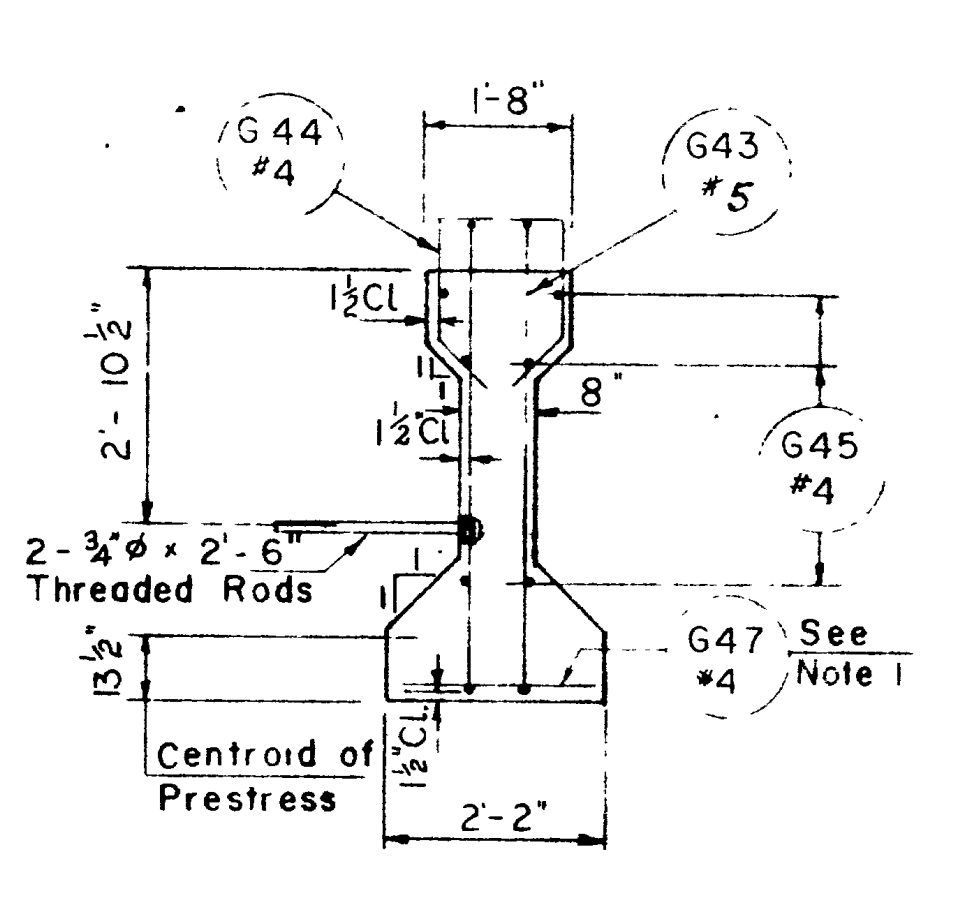
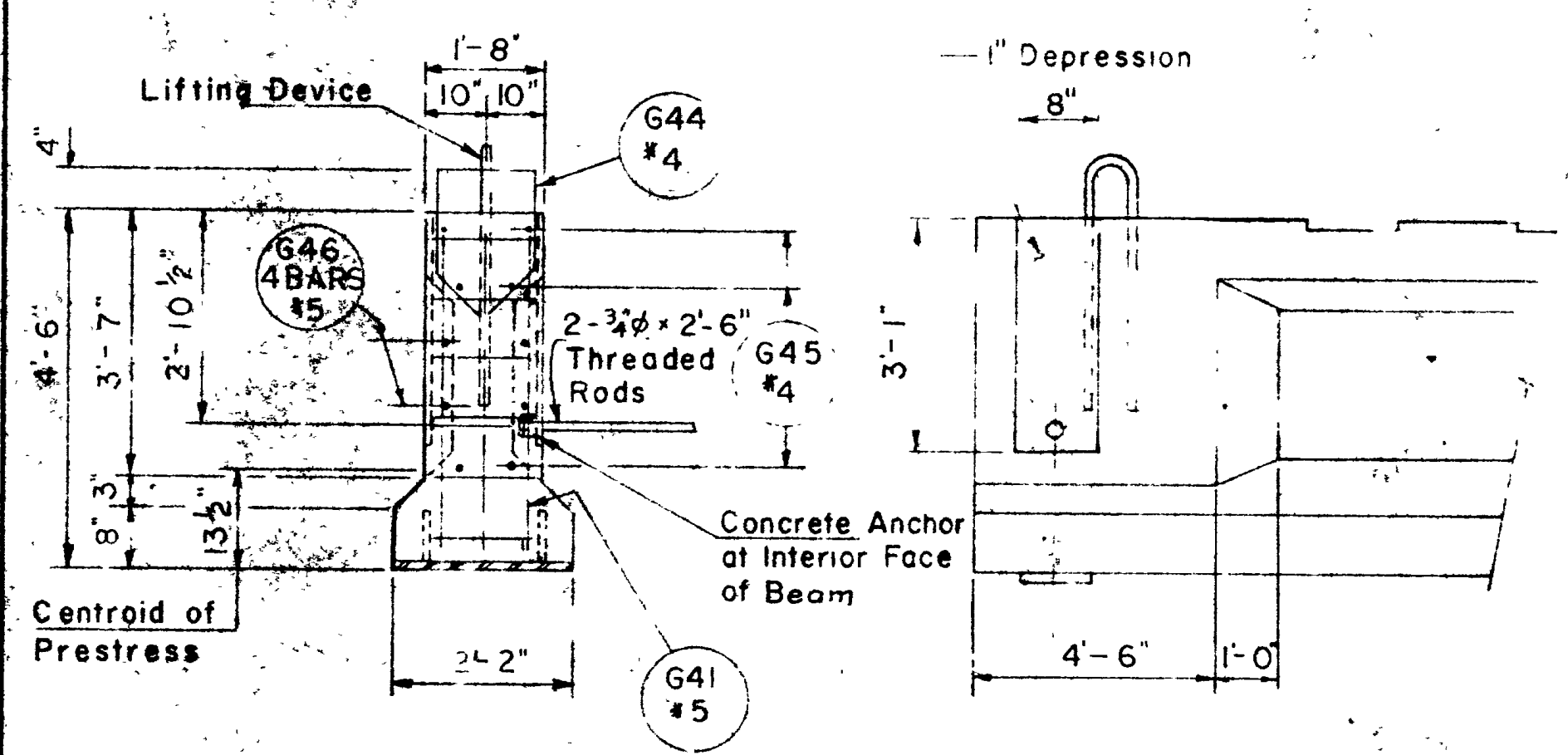
FRAMING PLAN

UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION		
DAVIS - WEBER Co. LINE TO 4400 So. 5600 SOUTH INTERCHANGE FRAMING PLAN		
DESIGNED BY: MW	CHECKED BY: [Signature]	I-15-820334
DRAWN BY: FRA	CHECKED BY: H J S	PROJECT NUMBER
QUANTITIES BY: H J S	CHECKED BY: [Signature]	869+8730
APPROVAL RECOMMENDED BY: [Signature]	GROUP LEADER	STATION
APPROVED: Sept 4, 1982	[Signature]	WEBER COUNTY
DRG NO	F-66	10 OF 26



MARK	SIZE	LGTH	NO. OF BEARS
G41	5/8"	4'-2"	24
G42	5/8"	1'-6"	72
G43	5/8"	6'-0"	56
G44	4/8"	5'-3"	38
G45	5/8"	46'-9"	6
G46	5/8"	1'-10"	8
G47	4/8"	2'-0"	38
G48	5/8"	4'-1"	20

* With Post-Tensioning
NOTE Unless otherwise shown, all dimensions are out to out.



NOTE: Bar G47 shall be placed immediately above lower row of prestressing strands.

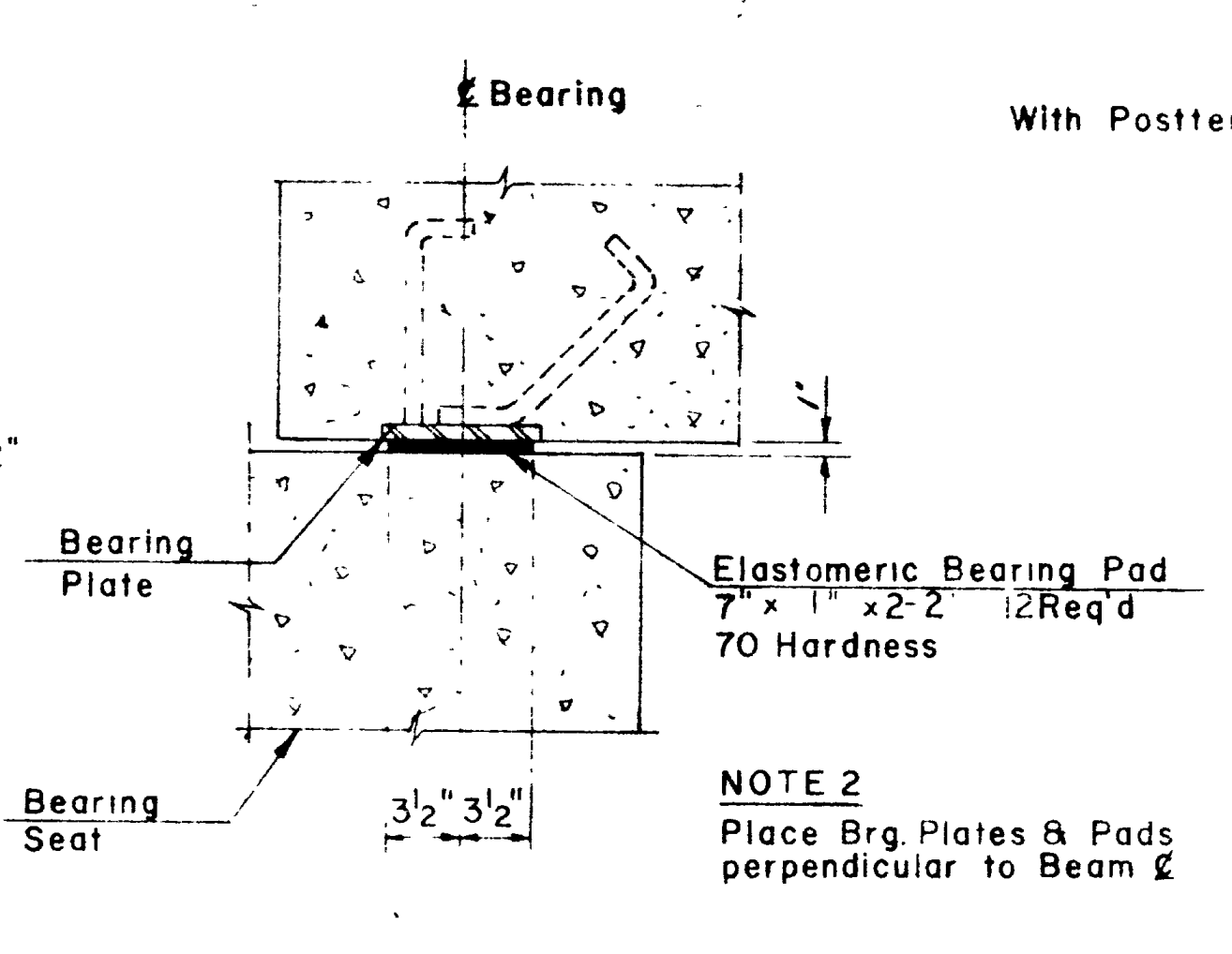
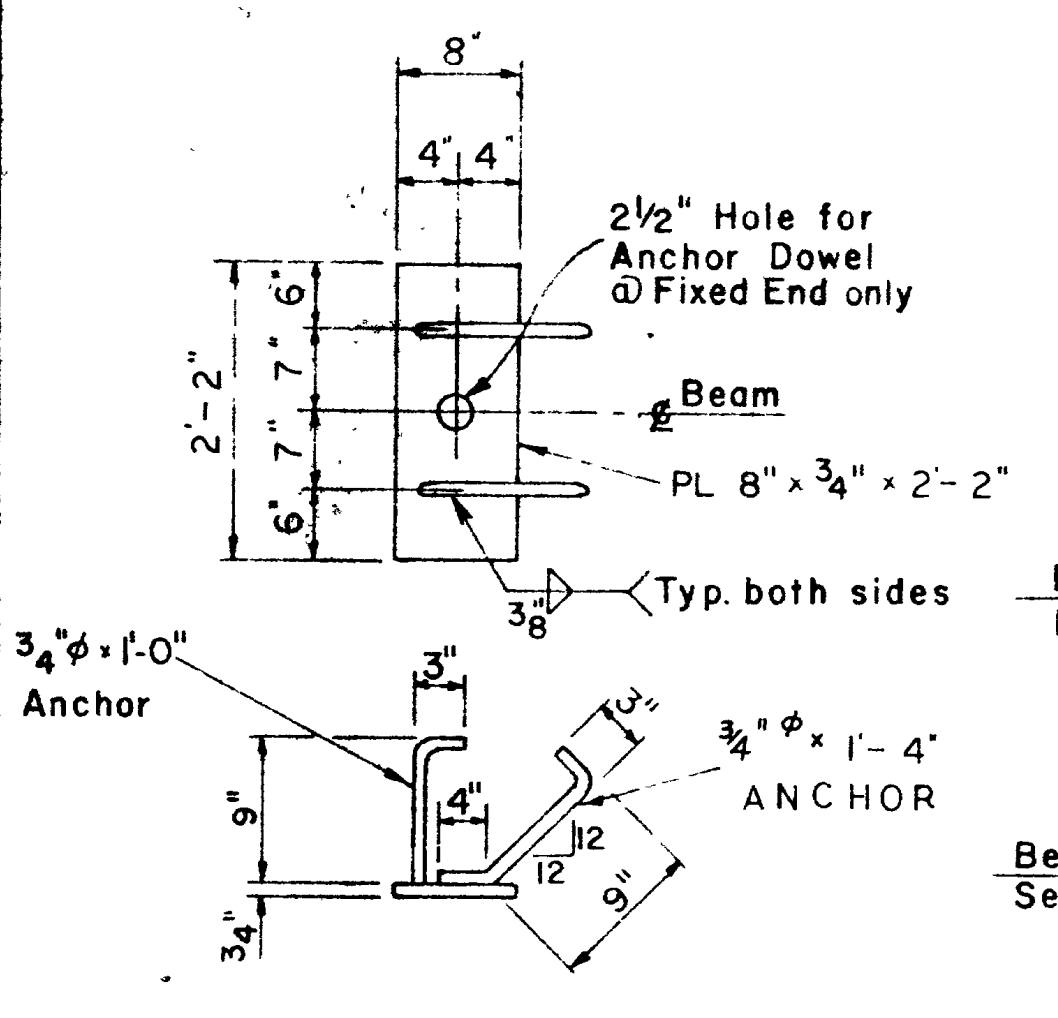
SECTION A-A WITH POST-TENSIONING

POST-TENSIONED END-ELEVATION

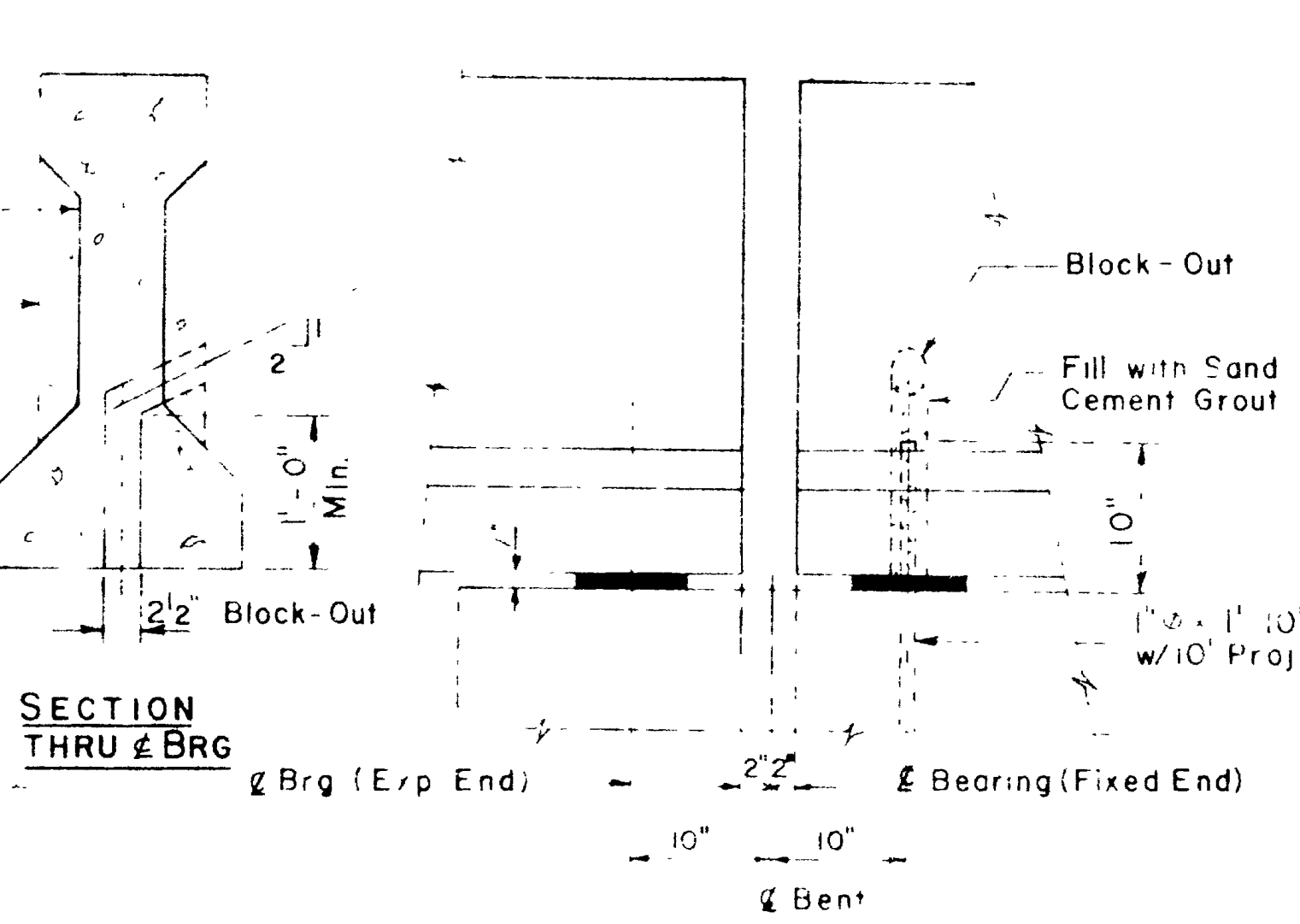
SECTION C-C TYPICAL ENTIRE LENGTH WITH PRETENSIONING

PRETENSIONED END-ELEVATION

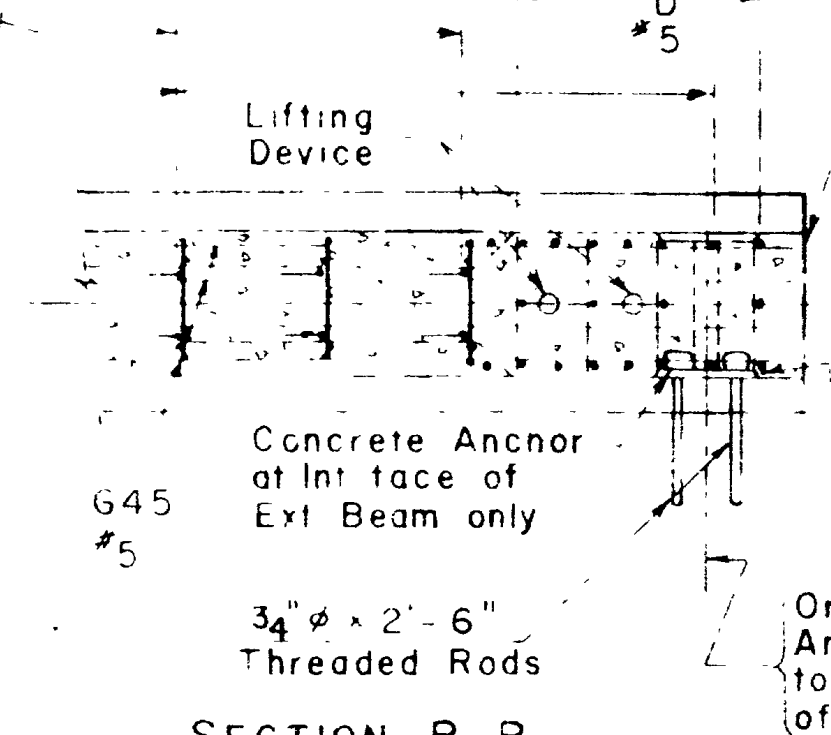
SECTION E-E WITH PRETENSIONING



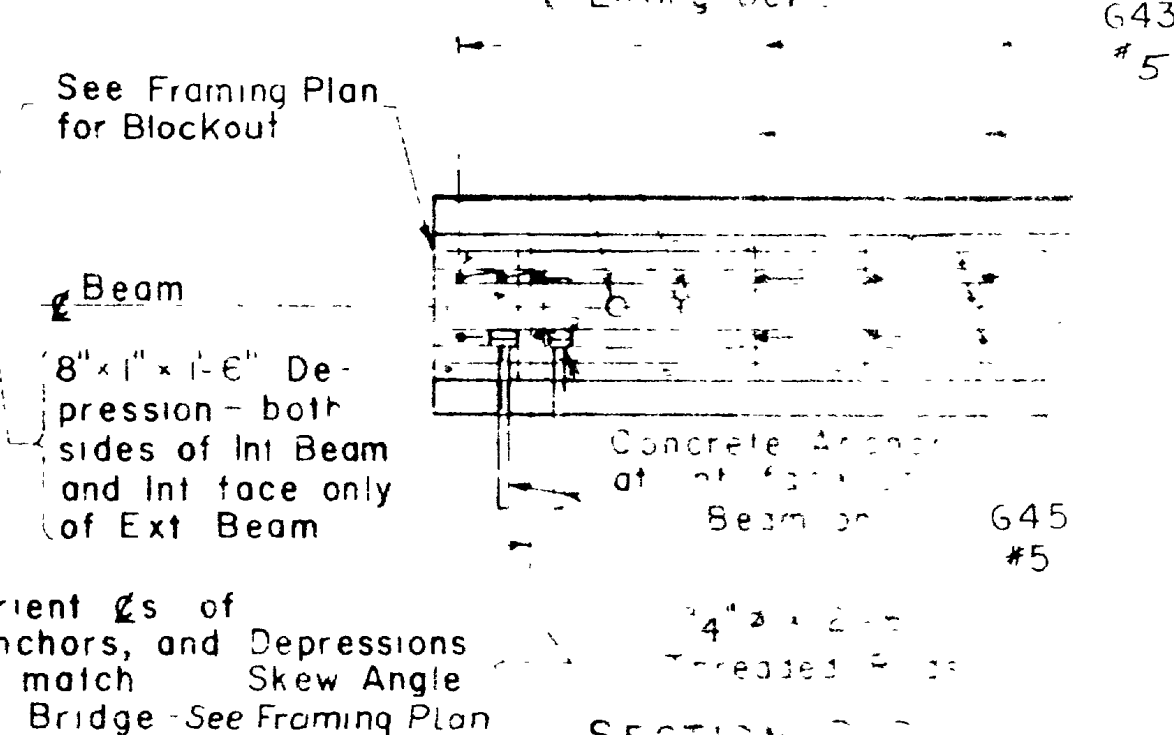
NOTE 2: Place Brg. Plates & Pads perpendicular to Beam ϕ .



SECTION THRU BRG



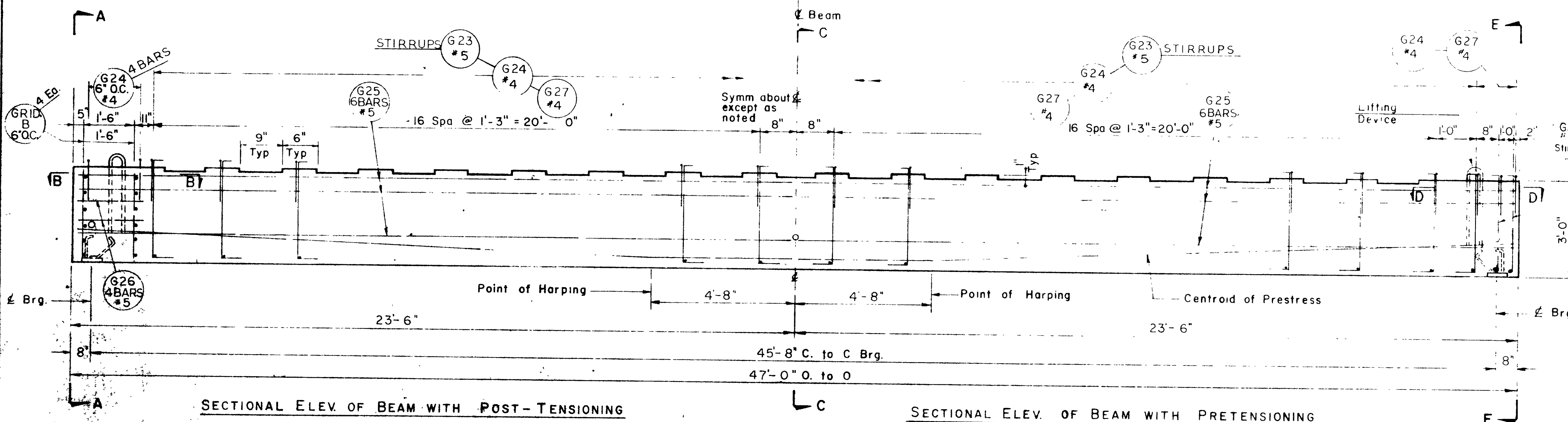
SECTION B-B END BLOCK DETAILS WITH POST-TENSIONING



SECTION D-D END DETAILS WITH PRETENSIONING

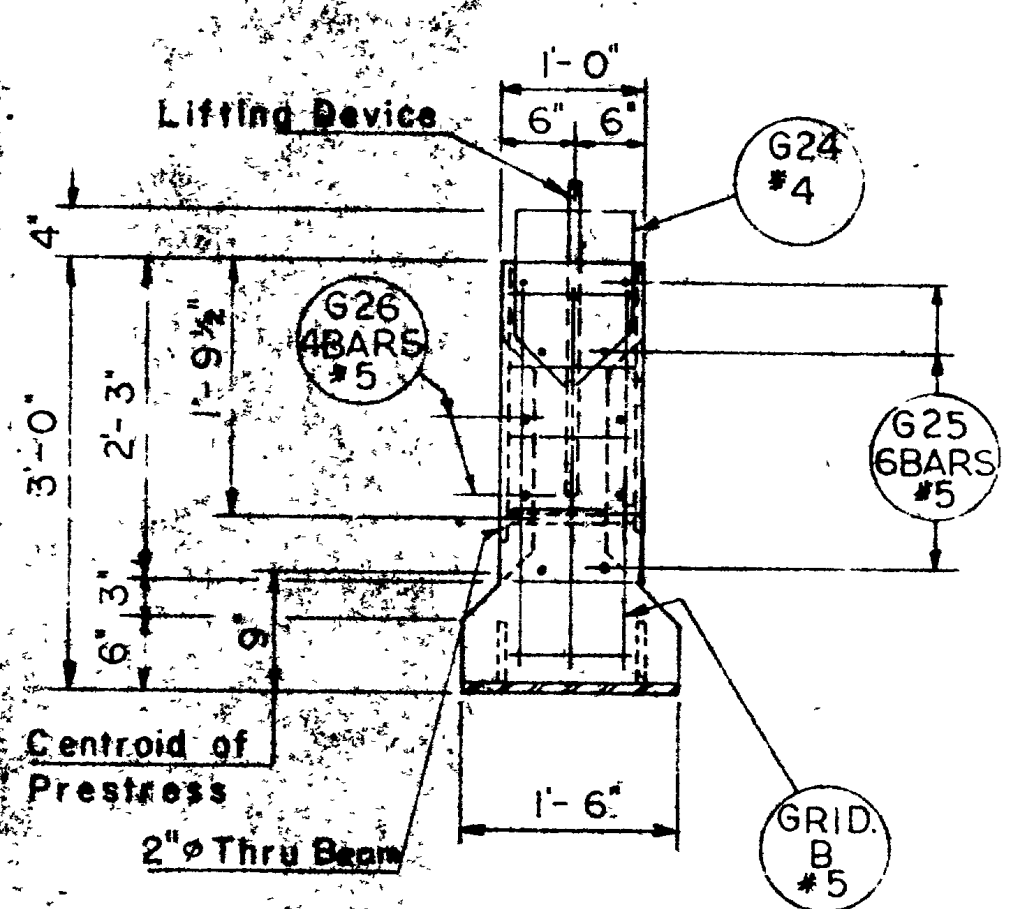
AASHTO Type IV Beam
Final prestressing forces after losses = 497,000 LBS.
 $f_{ci} = 4,000$ psi
6 Exterior Beams required, Mark B1
0 Interior Beams required

UTAH STATE DEPARTMENT OF HIGHWAYS
STRUCTURAL DIVISION
DAVIS-WEBER Co. LINE TO 4400 So.
5600 SOUTH INTERCHANGE
PRESTRESSED CONCRETE BEAMS
MW
U.S. H.S.E. 1-15-8/20/334
H.S. 31 869 + 87.30
WEBER

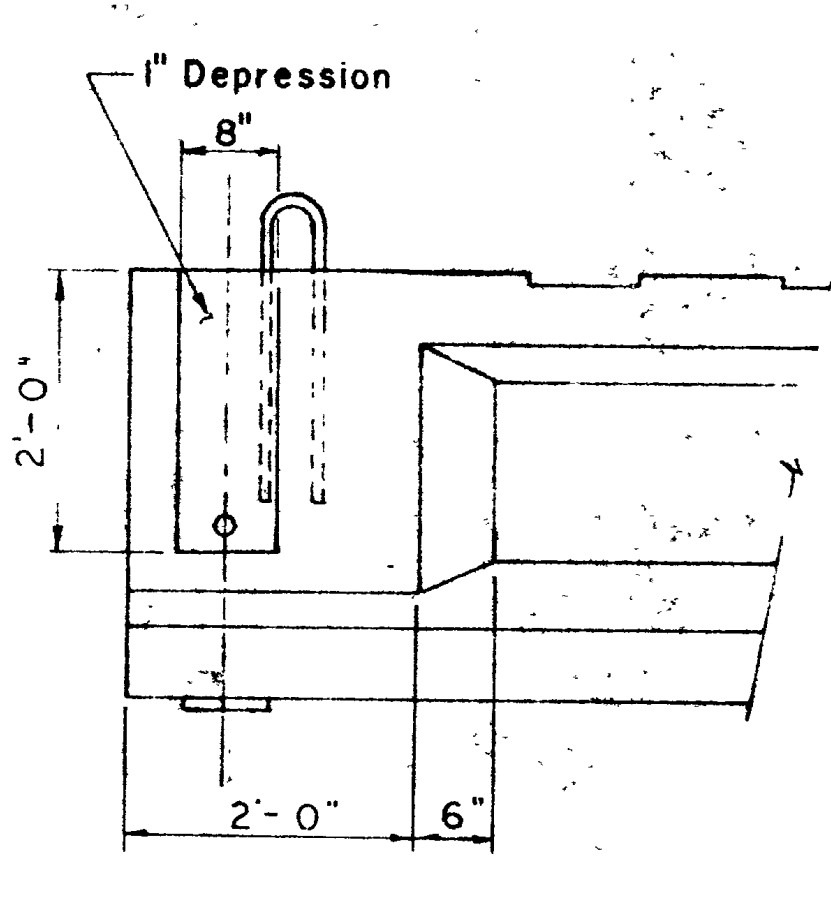


REINFORCING BAR SCHEDULE (PER BEAM)					
MARK	SIZE	L'GTH	NO	NO*	SKETCH
			BAR	BAR	
G21	5	2'-8"	2	4	
G22	5	10"	4	8	
G23	5	4'-2"	72	68	
G24	4	3'-2"	46	42	
G25	5	46'-9"	6	6	
G26	5	1'-10"	-	8	
G27	4	1'-3"	46	34	
G28	5	3'-5"	20	-	

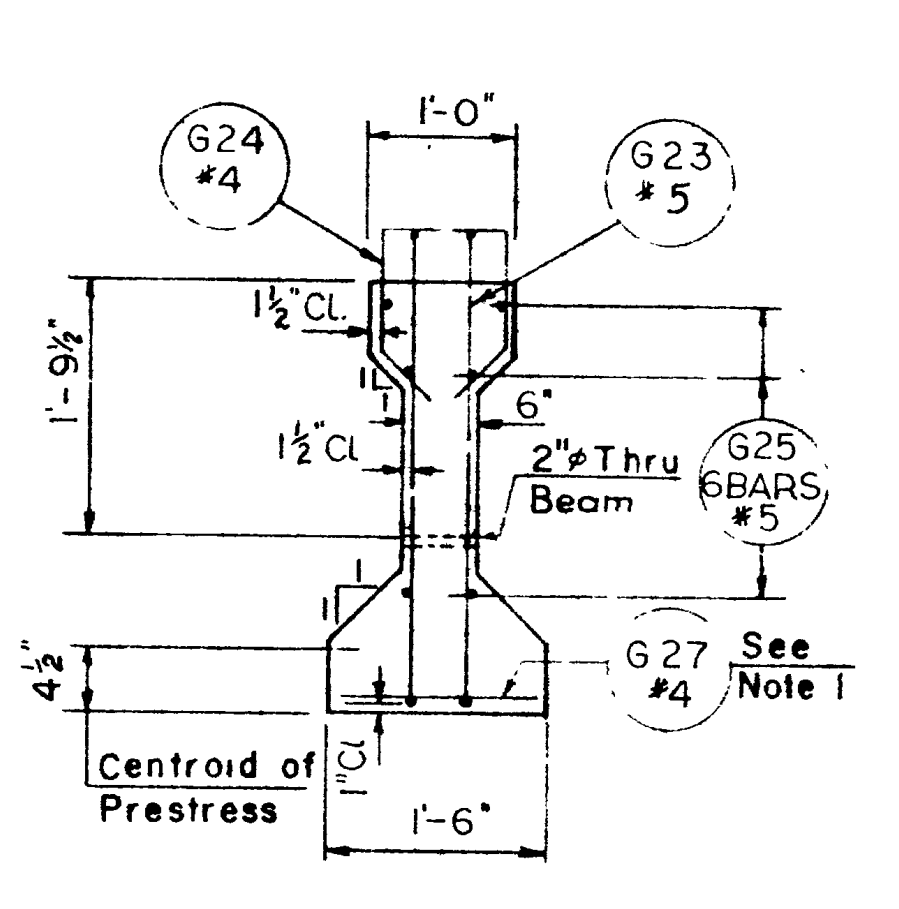
With Post-Tensioning
 NOTE: Unless otherwise shown, all dimensions are out to out of bars



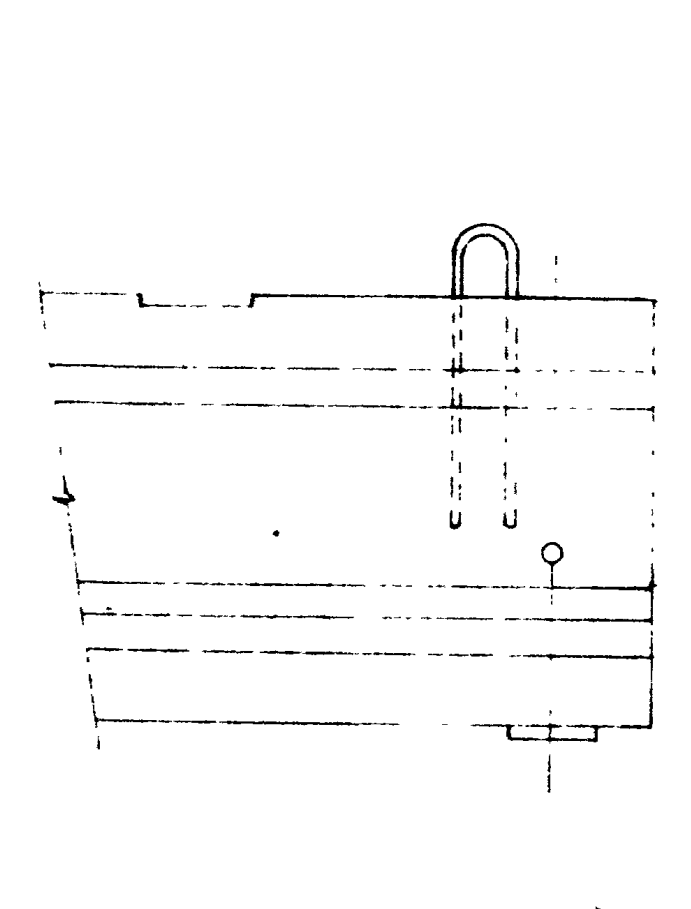
SECTION A-A WITH POST-TENSIONING



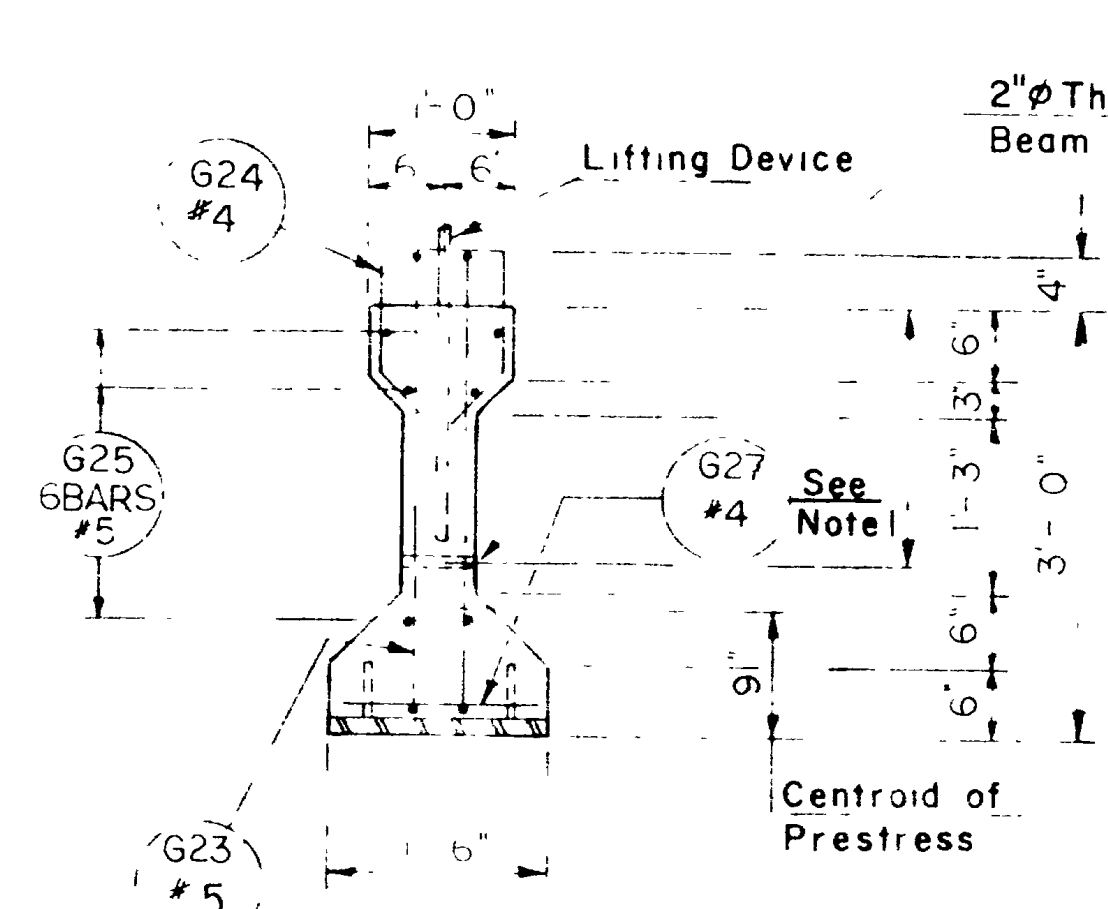
POST-TENSIONED END-ELEVATION



SECTION C-C TYPICAL ENTIRE LENGTH WITH PRETENSIONING

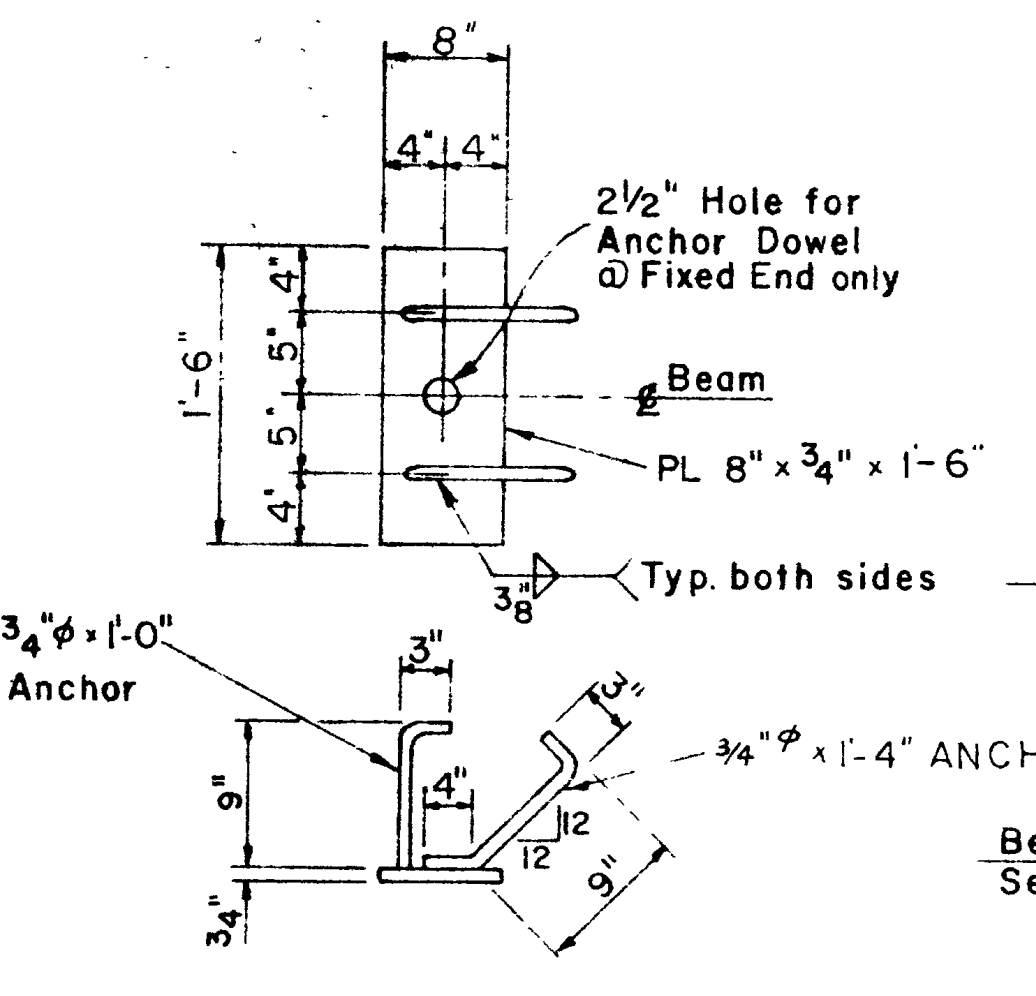


PRETENSIONED END-ELEVATION

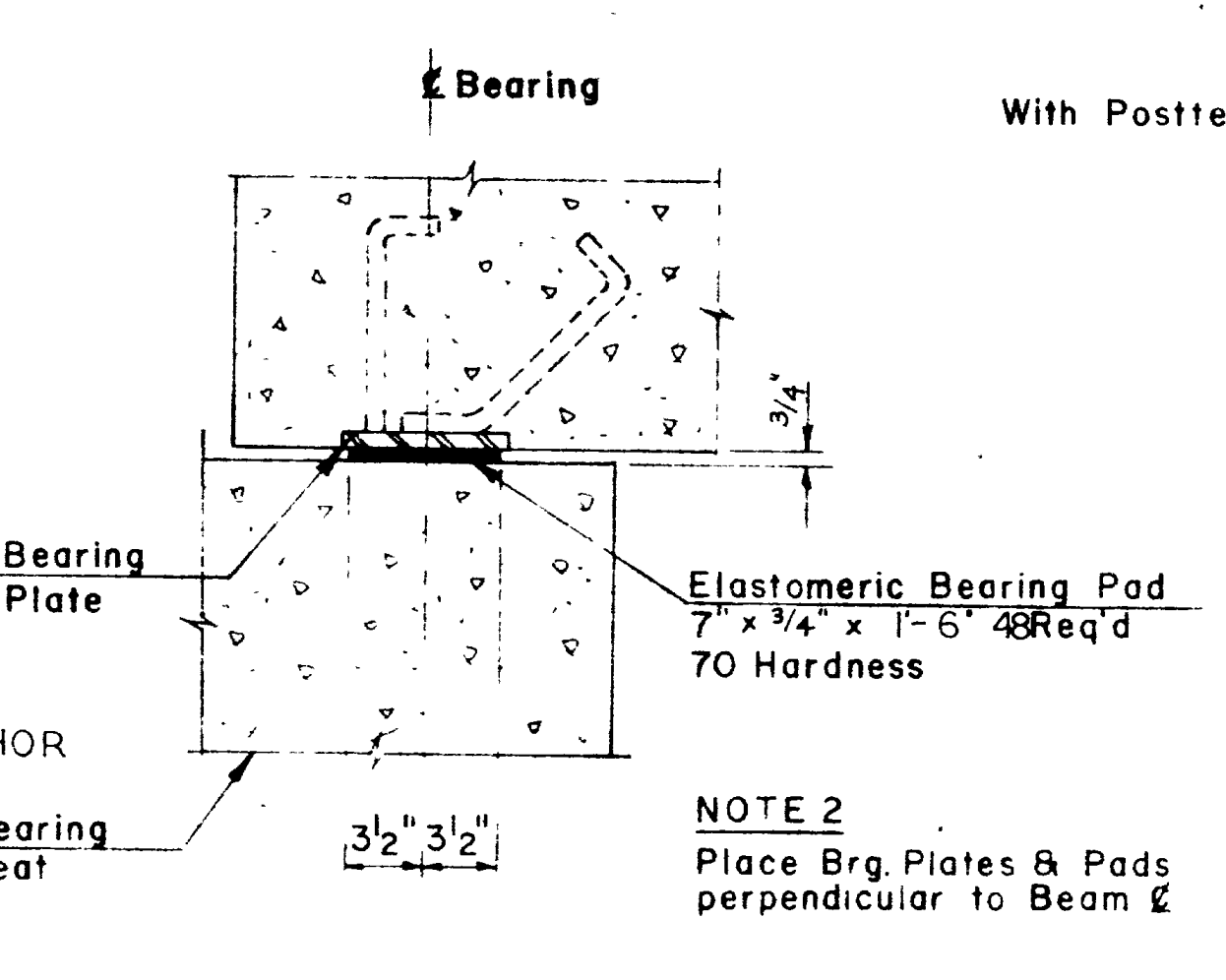


SECTION E-E WITH PRETENSIONING

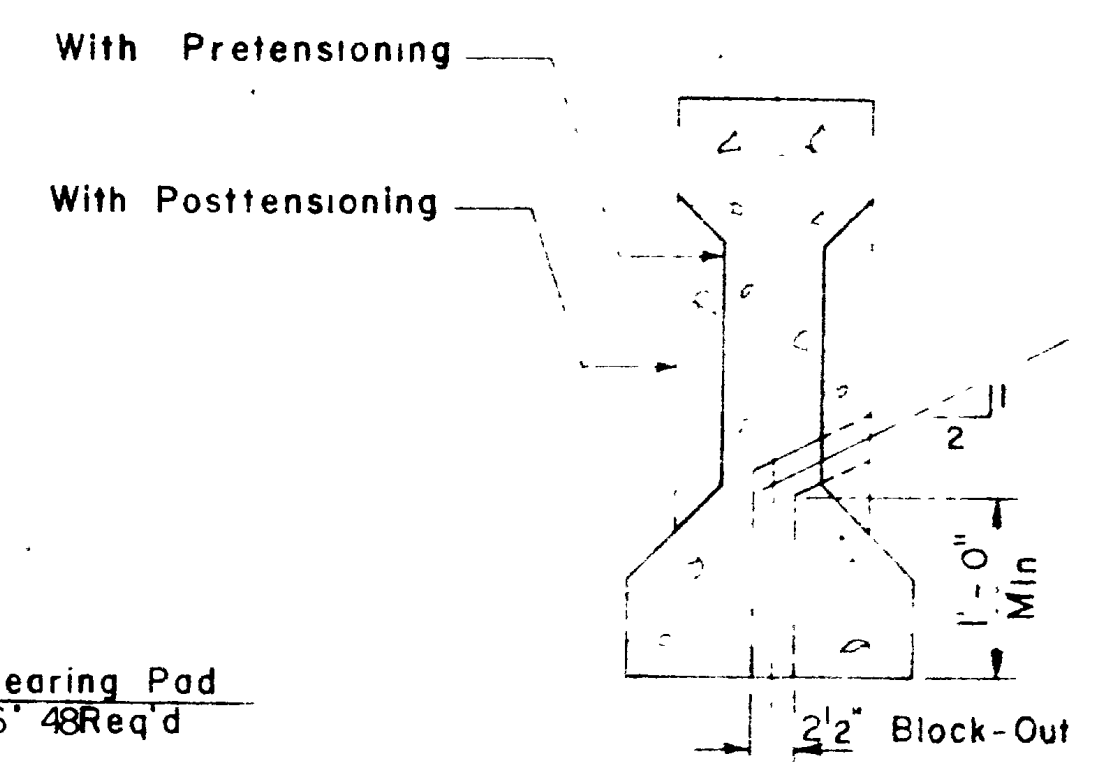
NOTE 1
 Bar #27 shall be placed immediately above lower row of prestressing strands



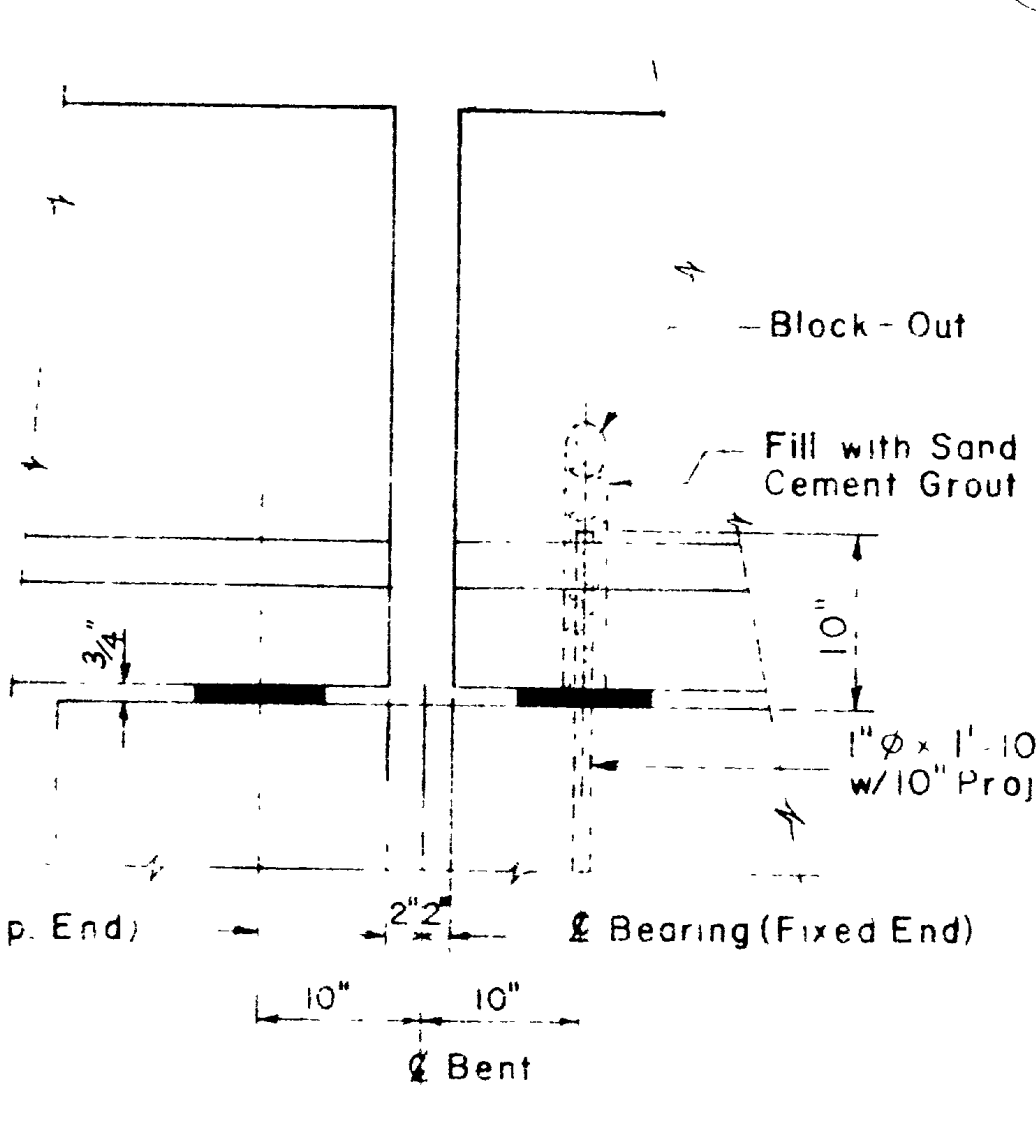
BEARING PLATE DETAIL



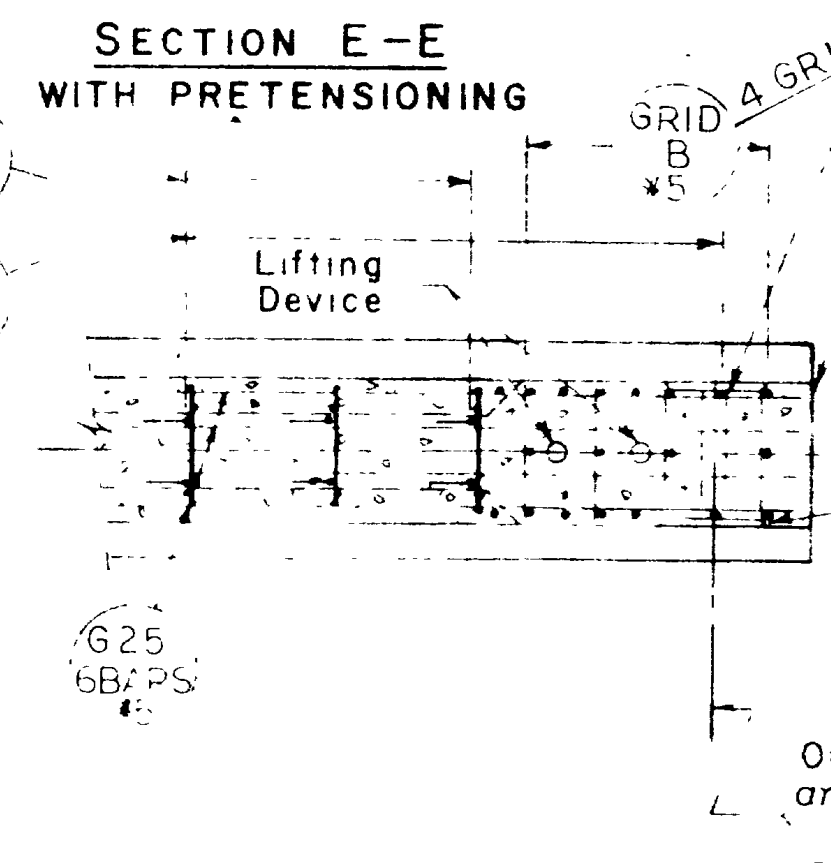
BEARING DETAIL



SECTION THRU BRG

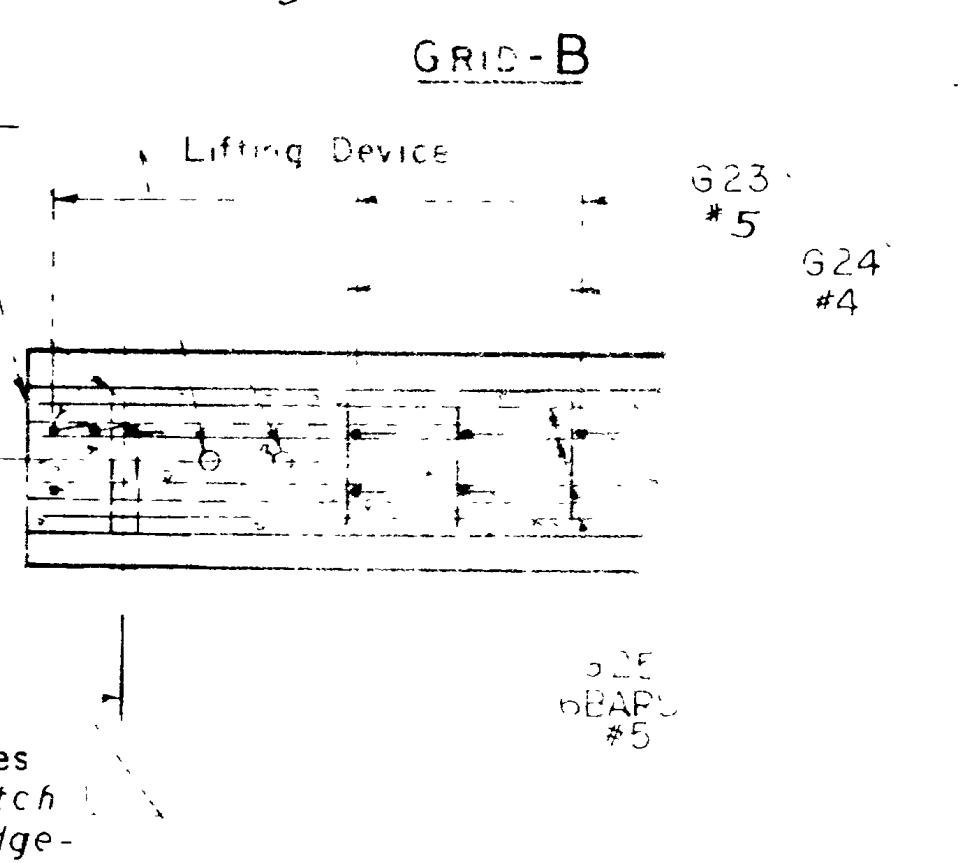


BEAM ANCHOR DETAILS



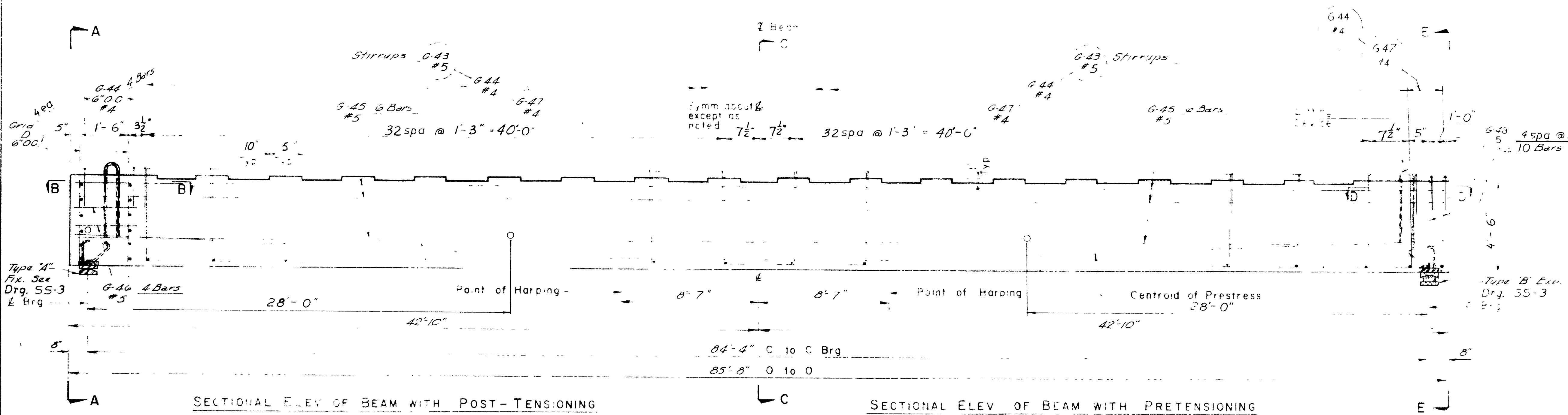
SECTION B-B END BLOCK DETAILS WITH POST-TENSIONING

AASHTO Type II Beam
 Final prestressing forces after losses = 404,000 LBS
 $f_{cL} = 5,000$ psi
 0 Exterior Beams required Mark B2
 24 Interior Beams required



SECTION D-D END DETAILS WITH PRETENSIONING

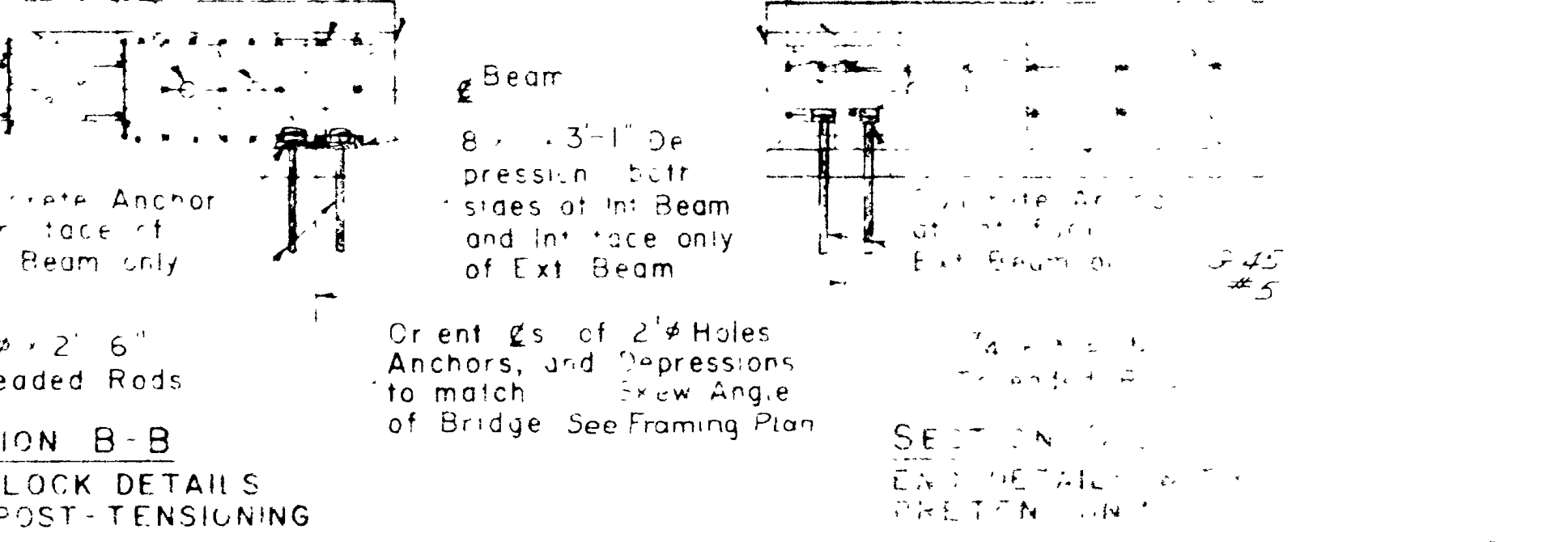
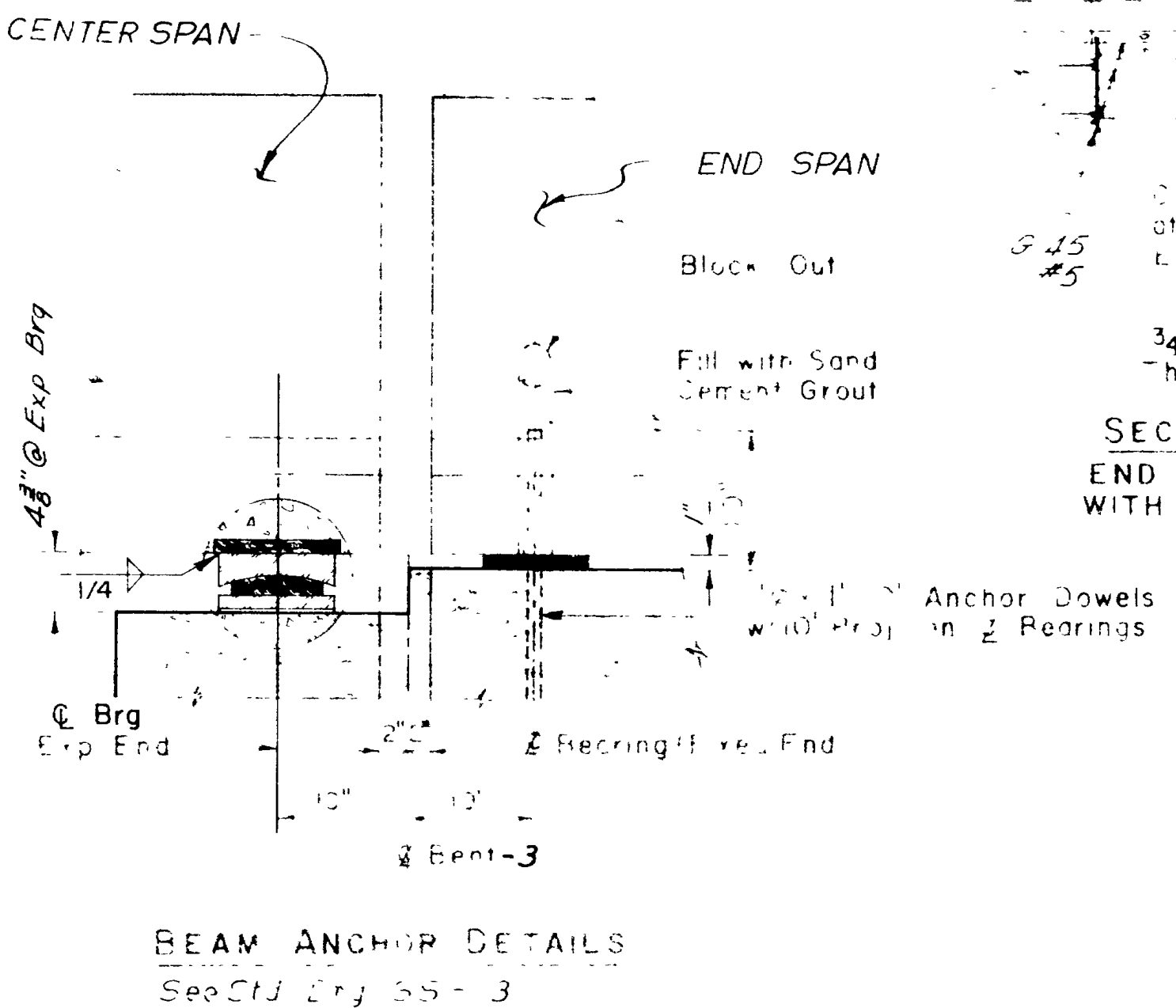
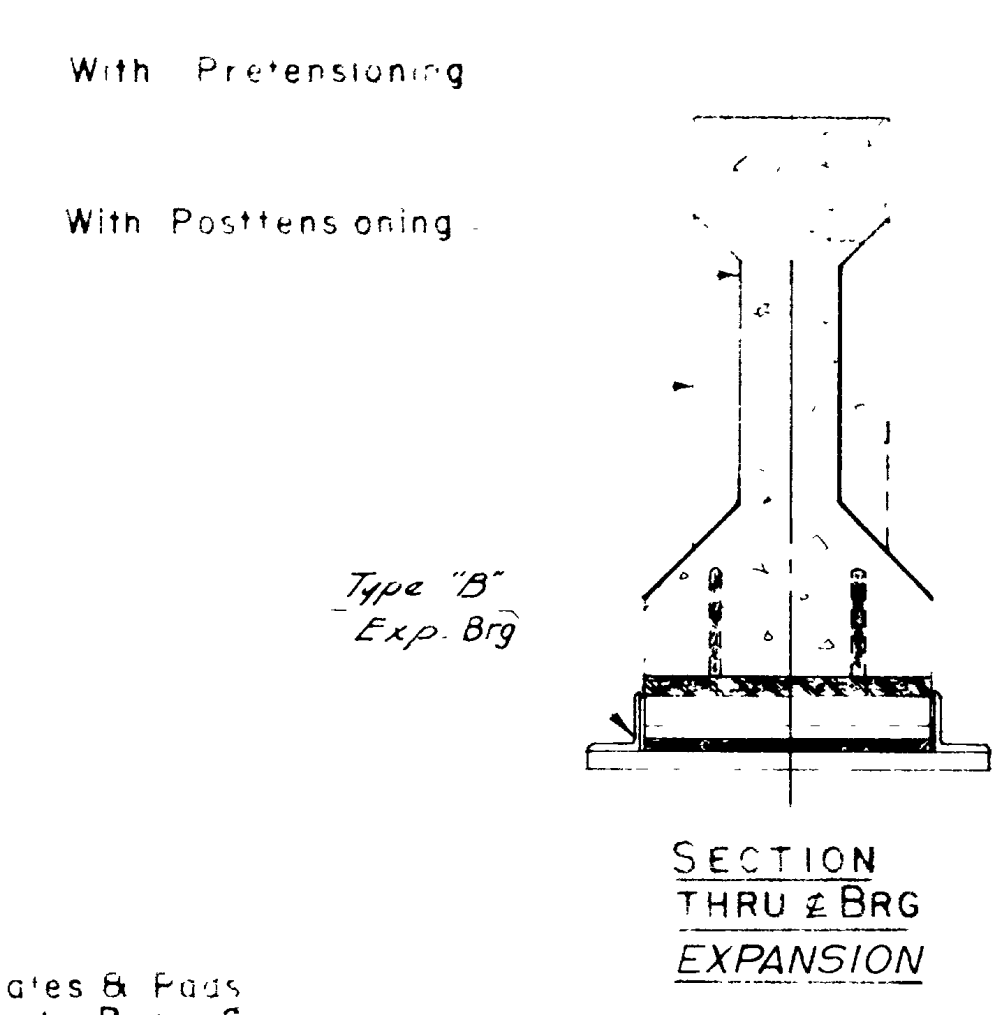
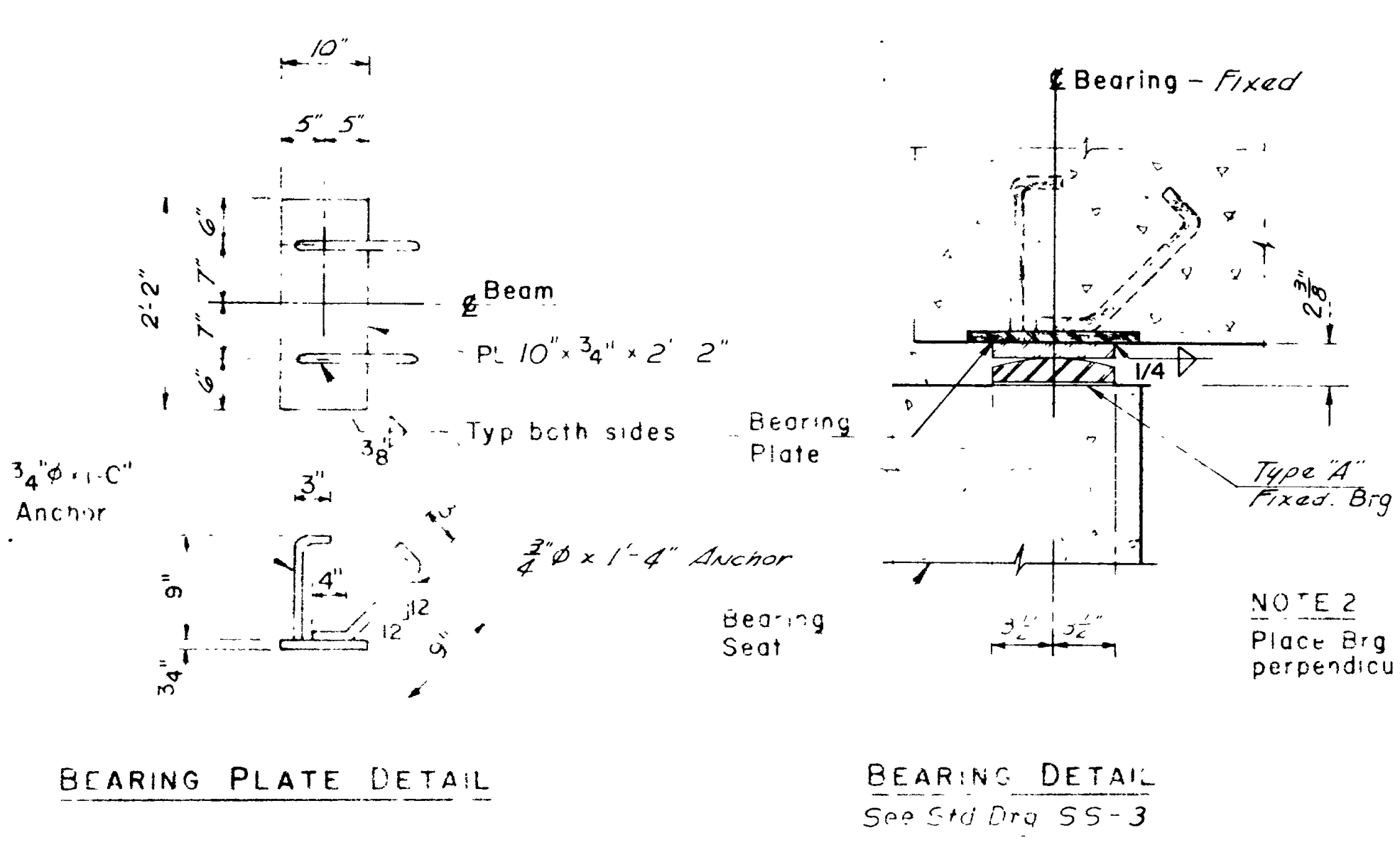
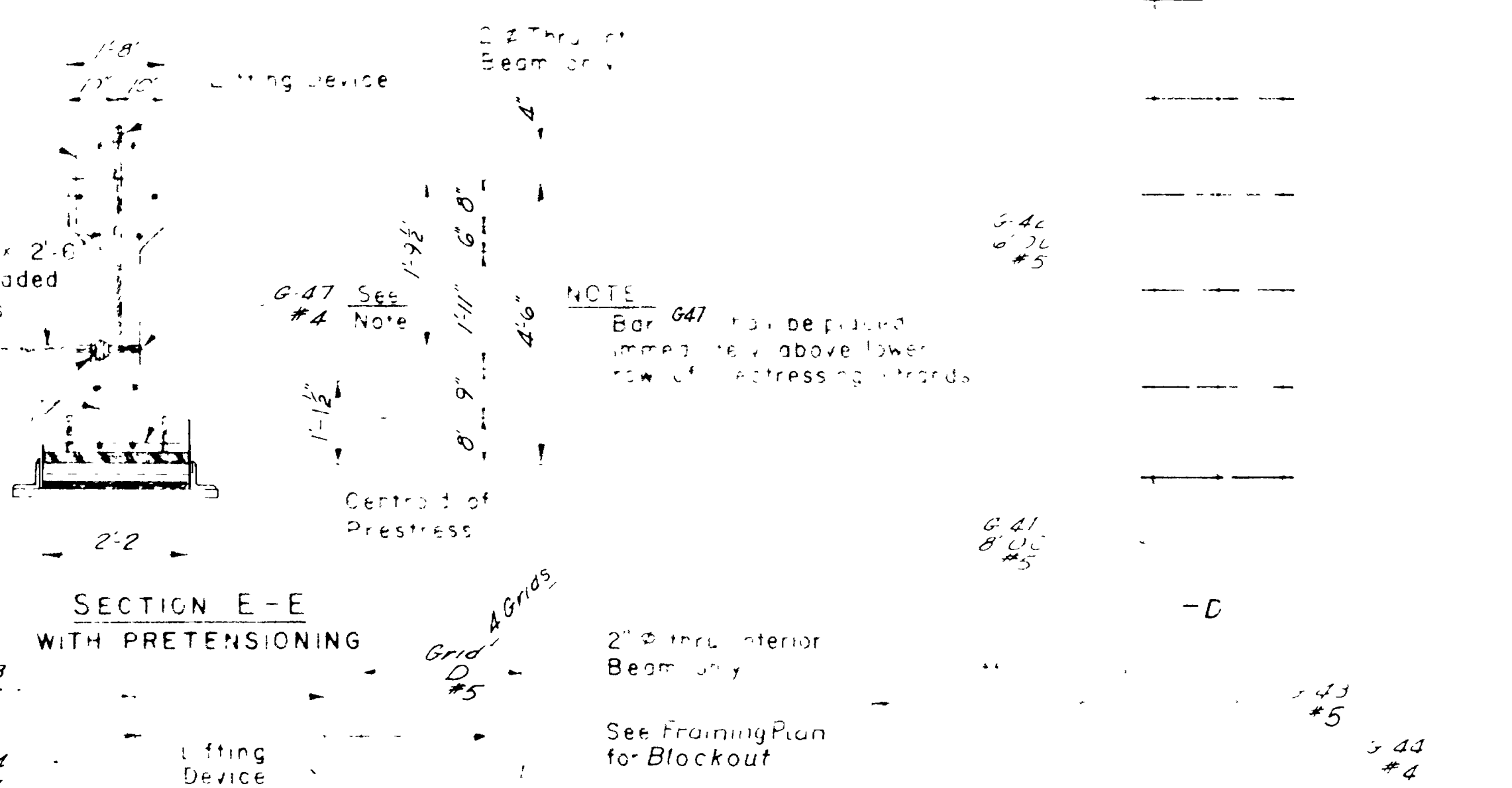
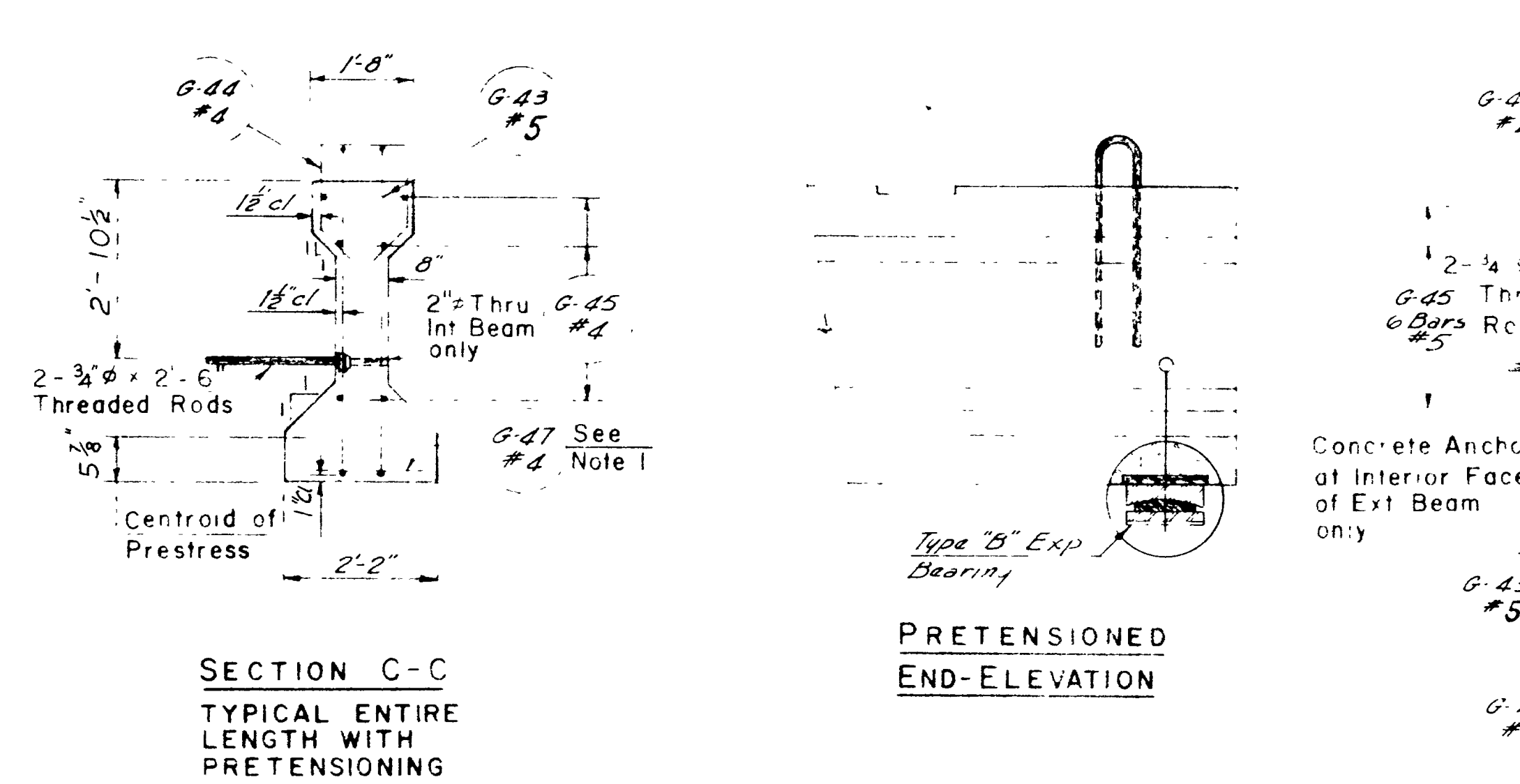
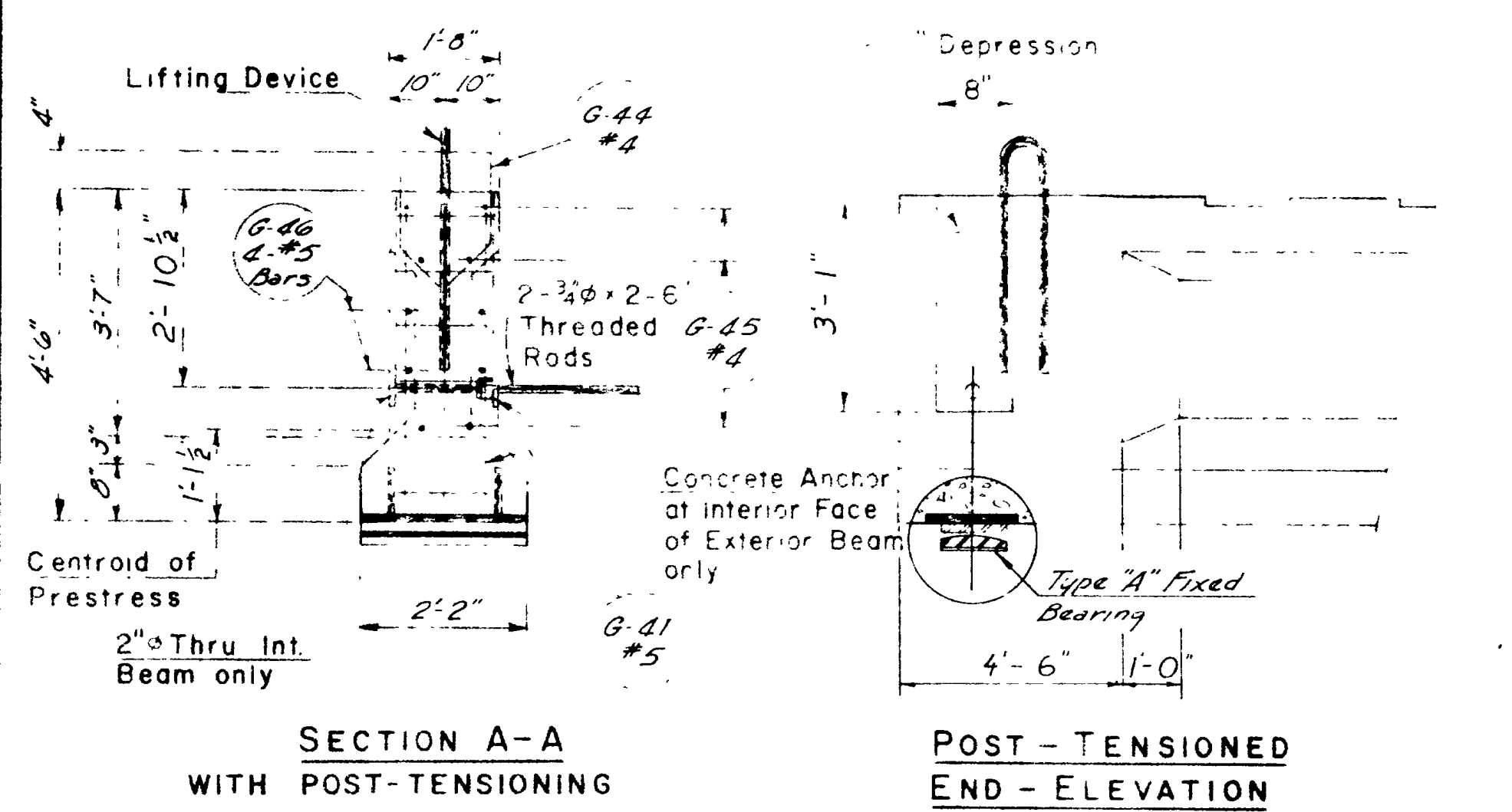
UTAH STATE DEPARTMENT OF HIGHWAYS
 STRUCTURE: DESIGN
 DAVIS-WEBER Co. LINE 6 TO 4400 So.
 5600 SOUTH INTERCHANGE
 PRESTRESSED BEAM DETAILS
 MW HGC I-15-8/20/334
 UMS HGC
 HJS HGC 869+8730
 M. Christian Weber
 WEBER



REINFORCING			
BAR SIZE	NO.	LENGTH	WEIGHT
G-41 #5	4	24	
G-42 #5	1	7.2	
G-43 #5	2	136	132
G-44 #4	5	78	74
G-45 #5	6		
G-46 #5	8		
G-47 #4	2	78	20
G-48 #5	4	20	

* With 1/4" dia. hole

NOTE: are cut to length



AASHTO Type IV Beam
 Final prestressing forces after losses = 950,000 Lbs
 -c = 5000 psi
 3 Exterior Beams required Mark B3
 4 Interior Beams required Mark B4

UTAH STATE DEPARTMENT OF TRANSPORTATION

DAVIS-WEBER Co. LINE TO 4400 So. 5600 So INTERCHANGE

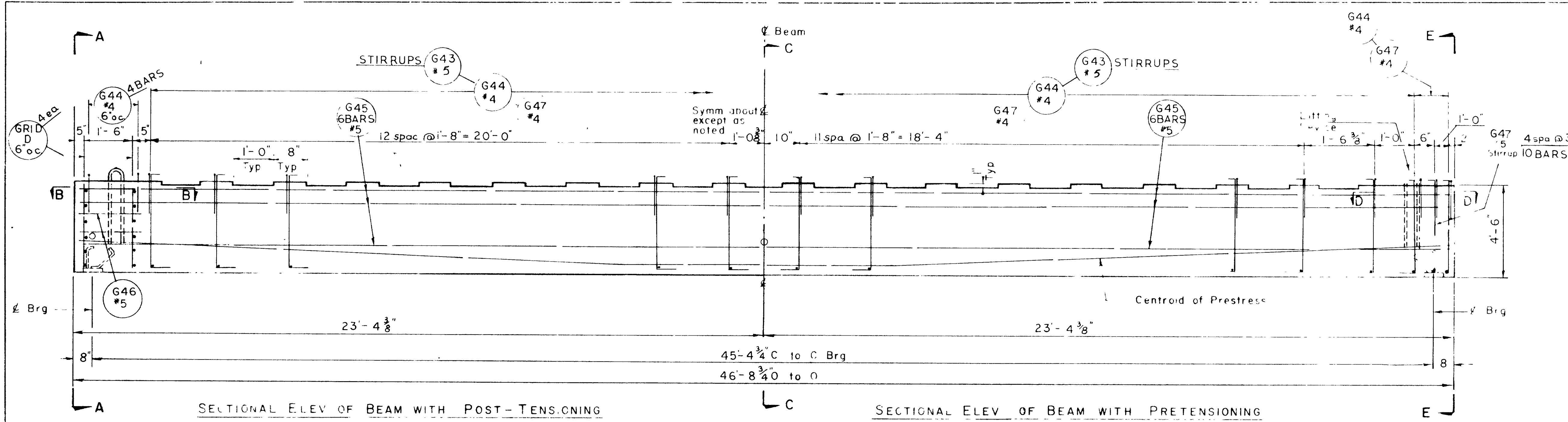
PRESTRESSING

DATE: 10/20/34

H.S.E. 869+87.30

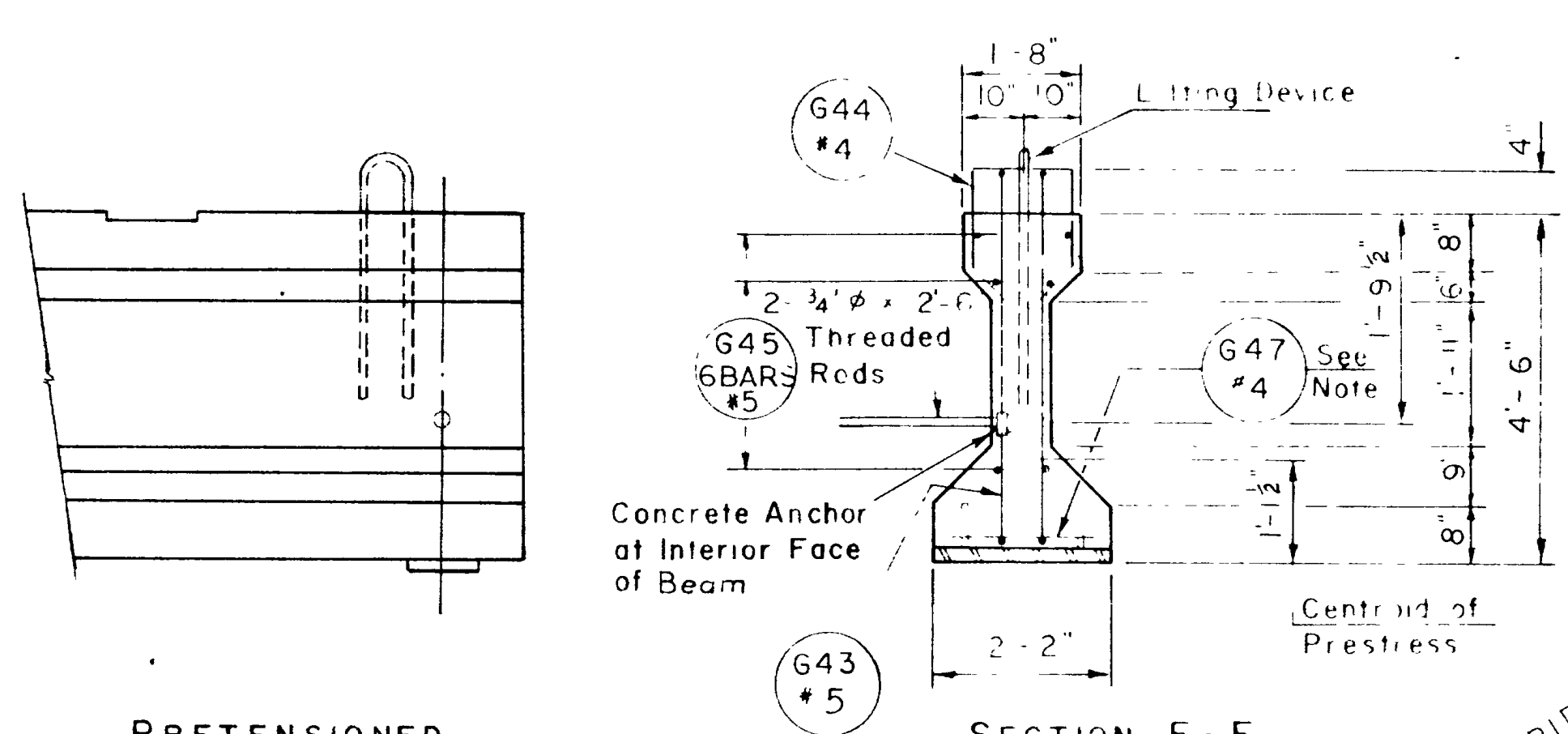
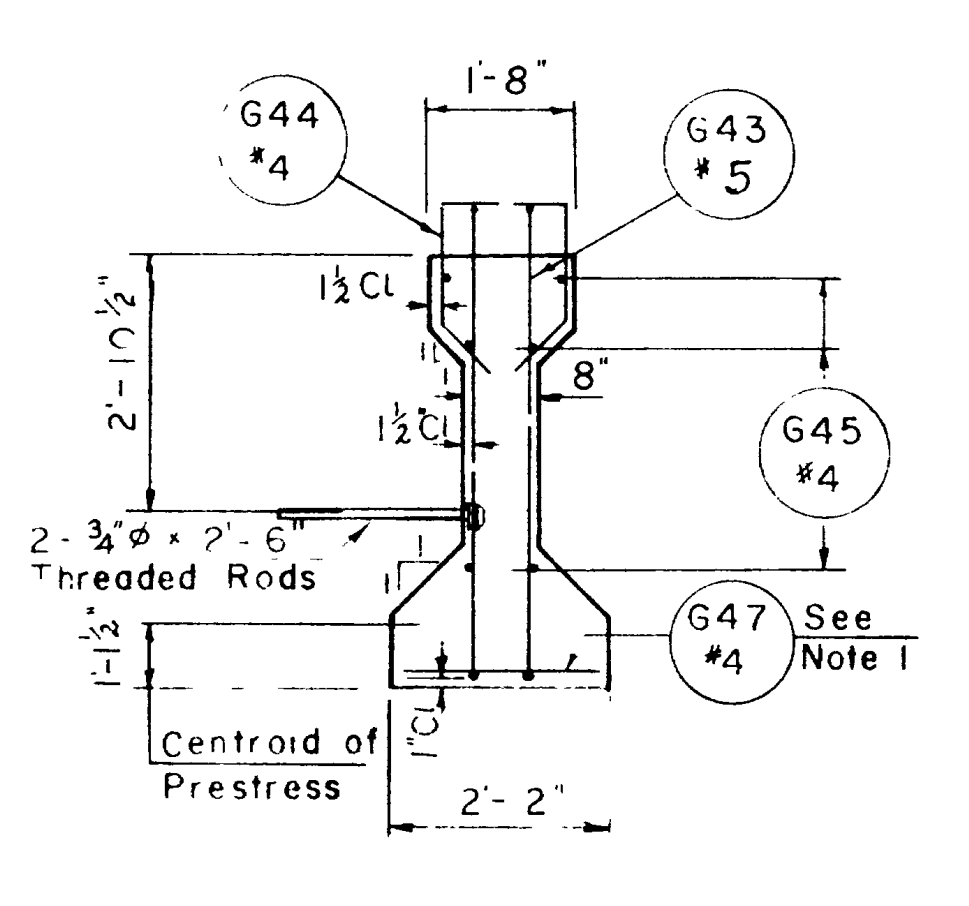
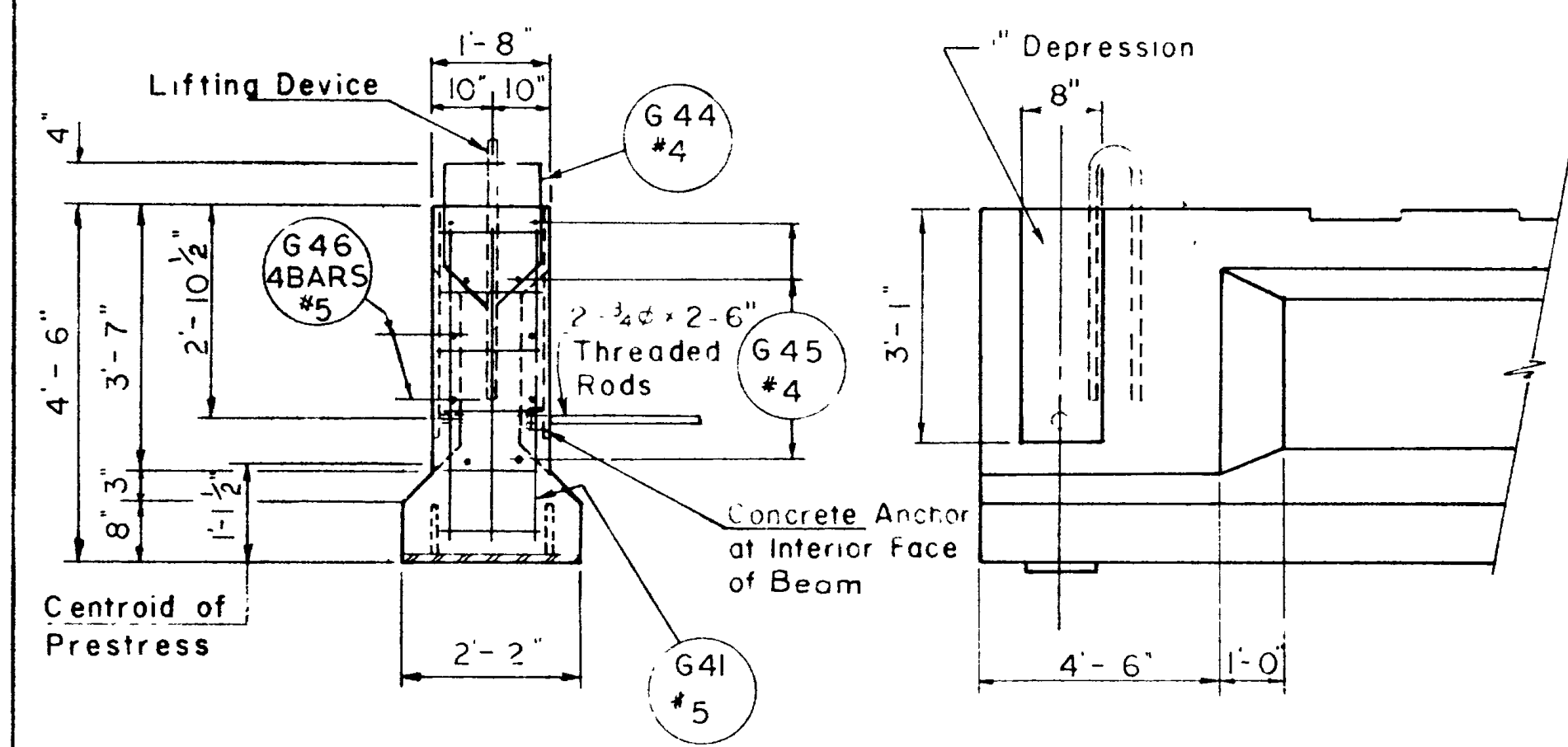
WB 12

F 66 13 20



REINFORCING BAR SCHEDULE (PER BEAM)						
MARK	SIZE	LEN	NO	FT	WT	SKETCH
G41	5	4'-2"	-	24		
G42	5	1'-6"	-	72		
G43	5	6'-0"	56	52		4'-9"
G44	4	5'-3"	38	34		1'-5"
G45	5	46'-6"	6	6		
G46	5	1'-10"	-	8		
G47	4	2'-0"	38	26		
G48	5	4'-11"	20	2		4'-9"

* With Post-Tensioning
 NOTE: Bars G41, G42, G43, G44, G45, G46, G47, G48 are cut off at ends.



NOTE: Bar G47 will be provided immediately above lower row of prestressing steel.

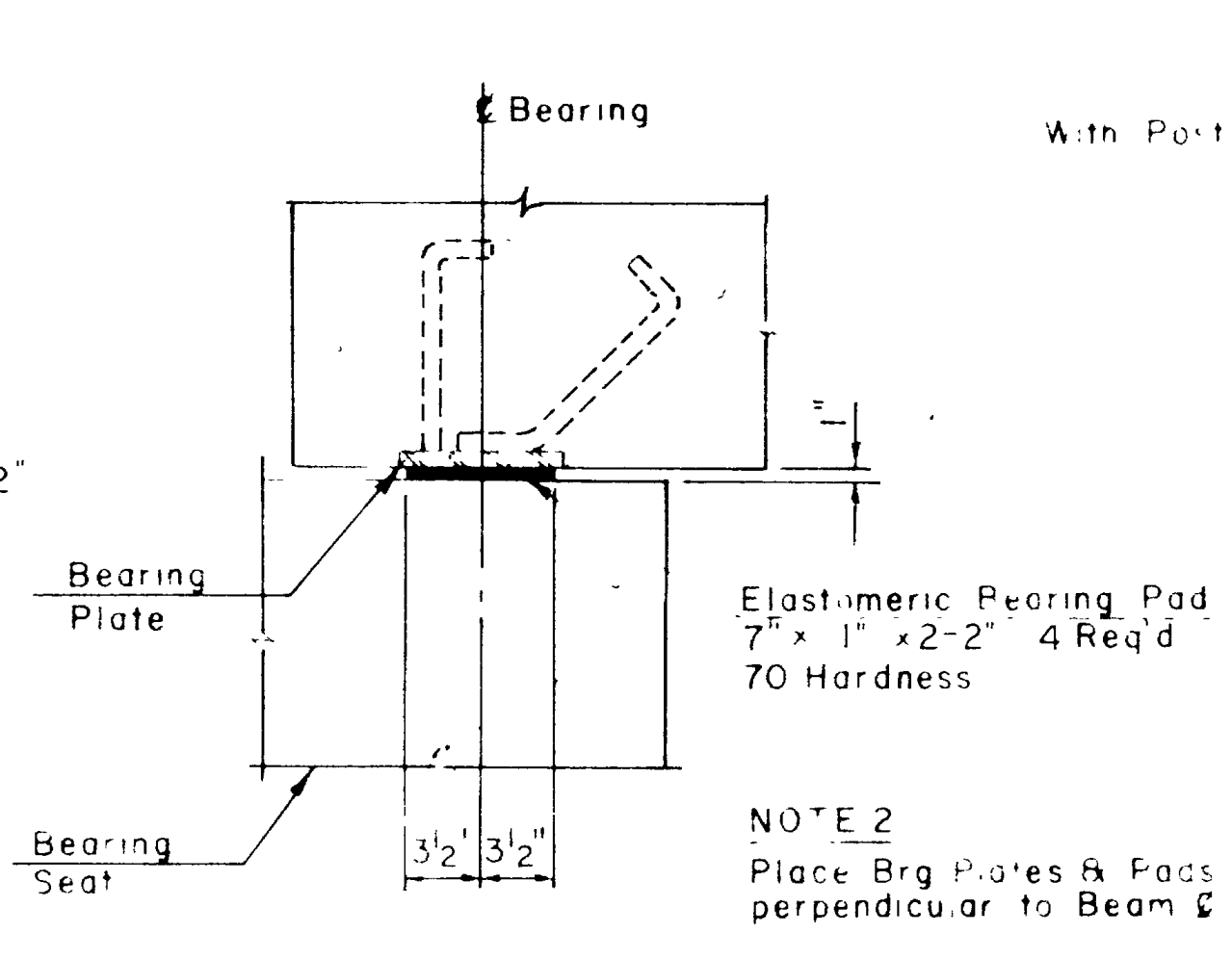
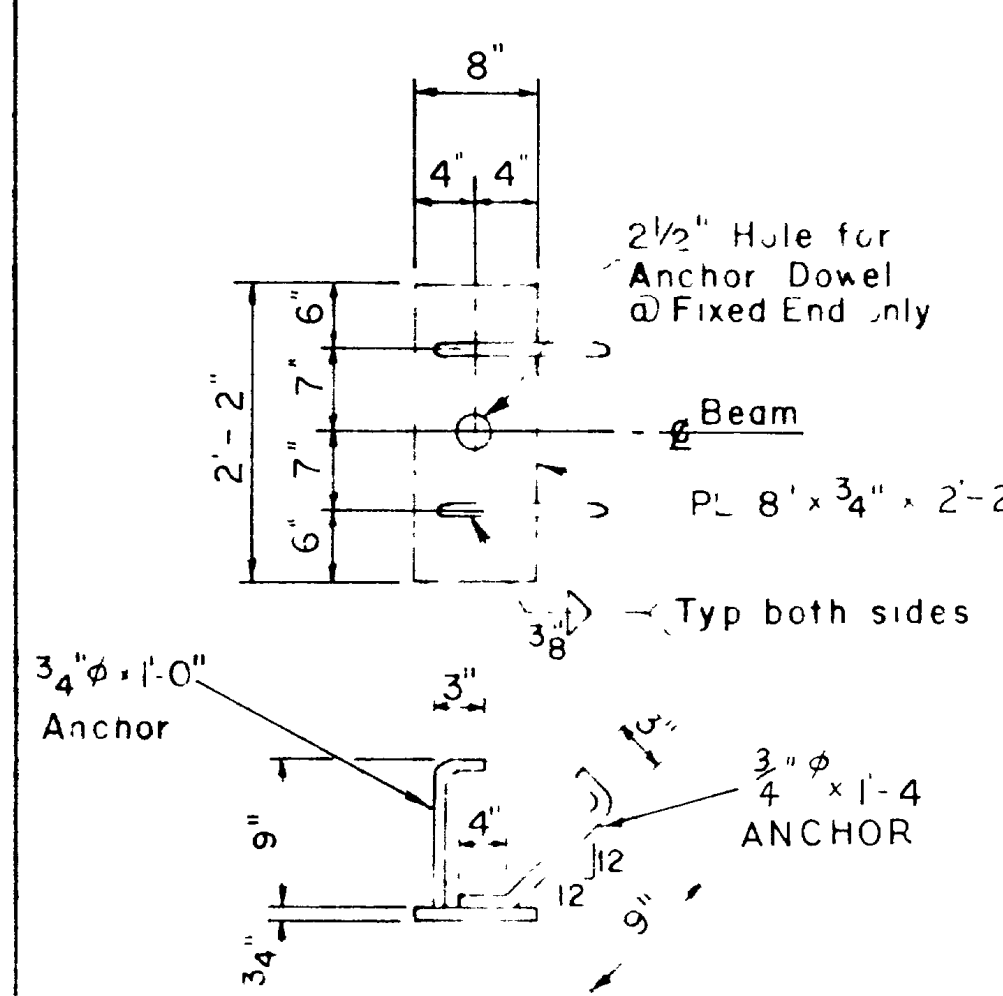
SECTION A-A WITH POST-TENSIONING

POST-TENSIONED END-ELEVATION

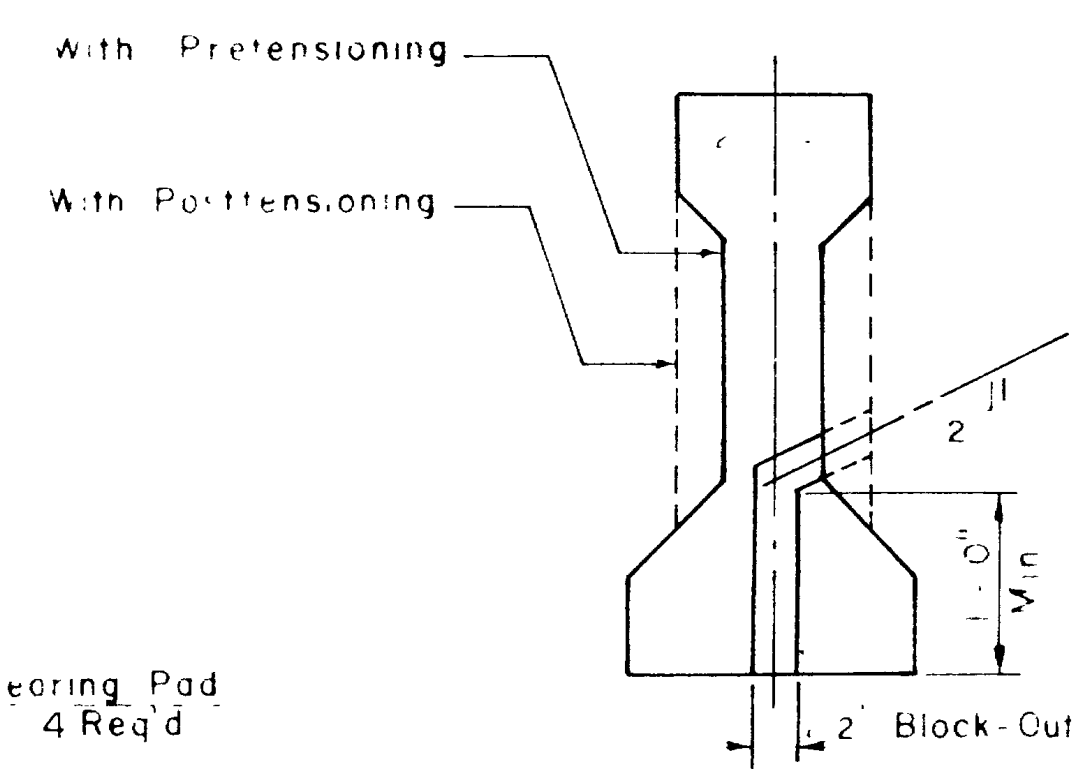
SECTION C-C TYPICAL ENTIRE LENGTH WITH PRETENSIONING

PRETENSIONED END-ELEVATION

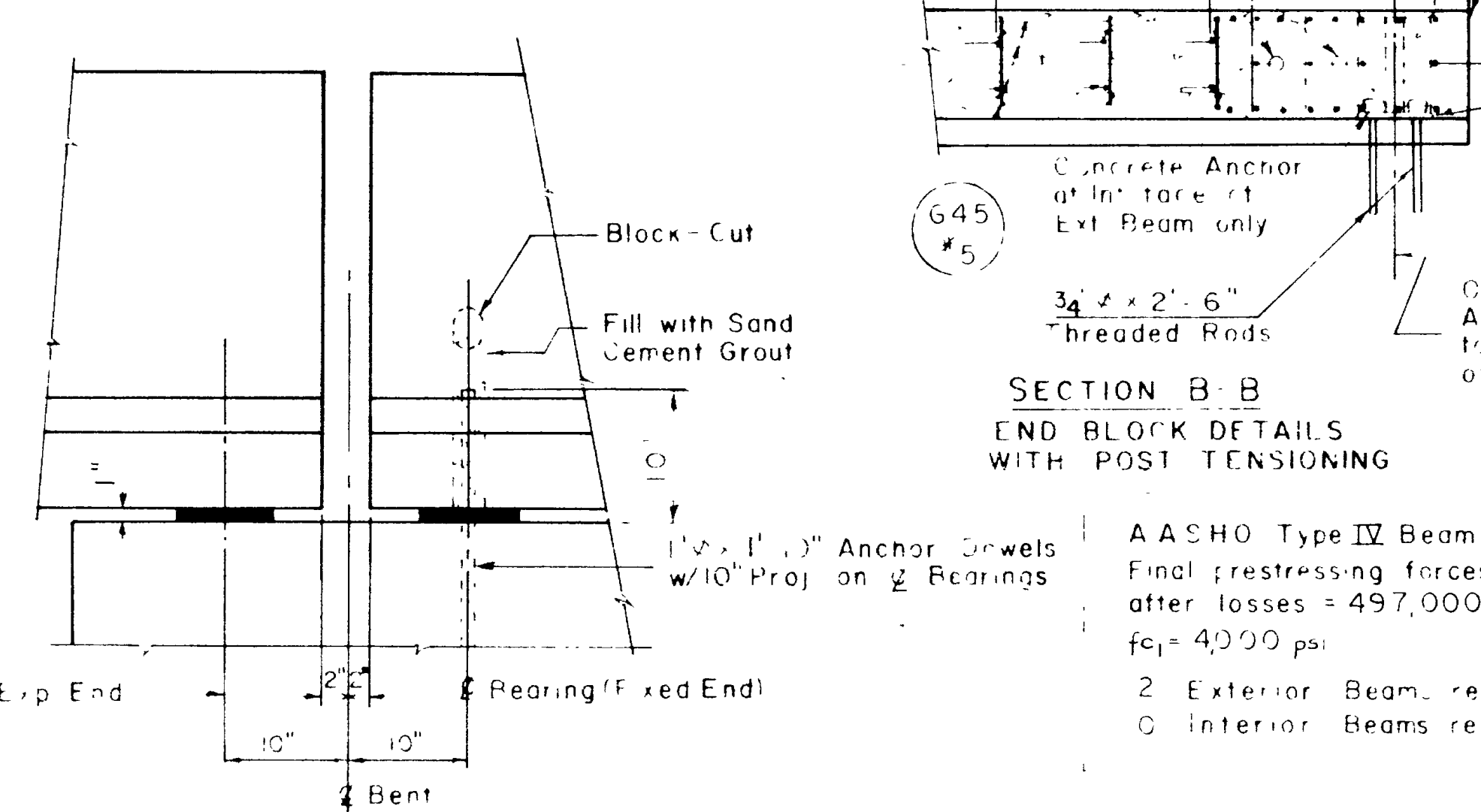
SECTION E-E WITH PRETENSIONING



NOTE 2: Place Brg Plates & Pads perpendicular to Beam &



SECTION THRU BRG



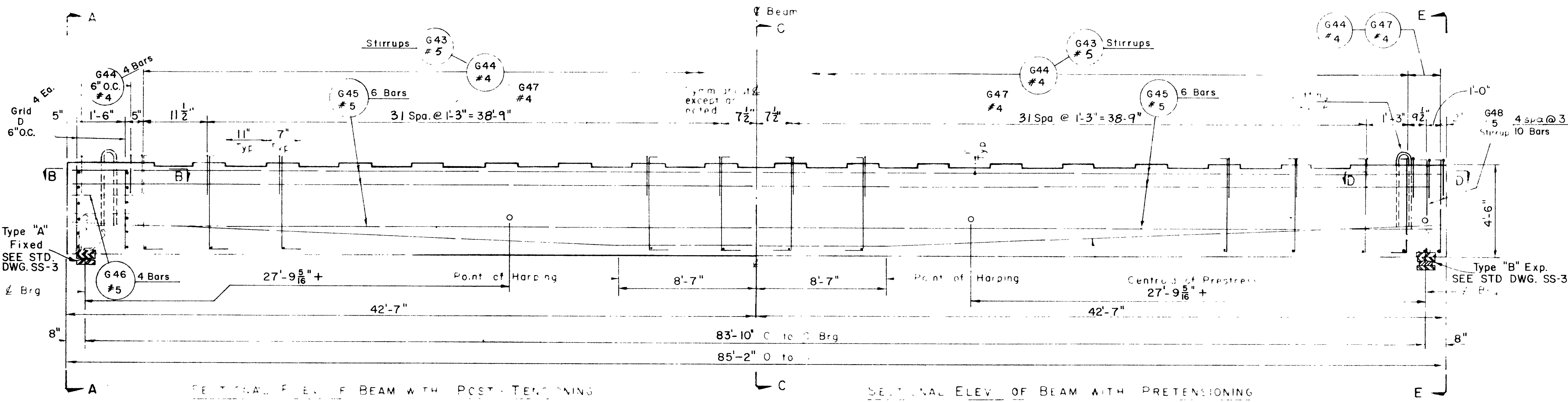
BEAM ANCHOR DETAILS

SECTION B-B END BLOCK DETAILS WITH POST-TENSIONING

AASHTO Type IV Beam
 Final prestressing forces after losses = 497,000 LBS
 $f_{ci} = 4,000$ psi
 2 Exterior Beams required
 0 Interior Beams required

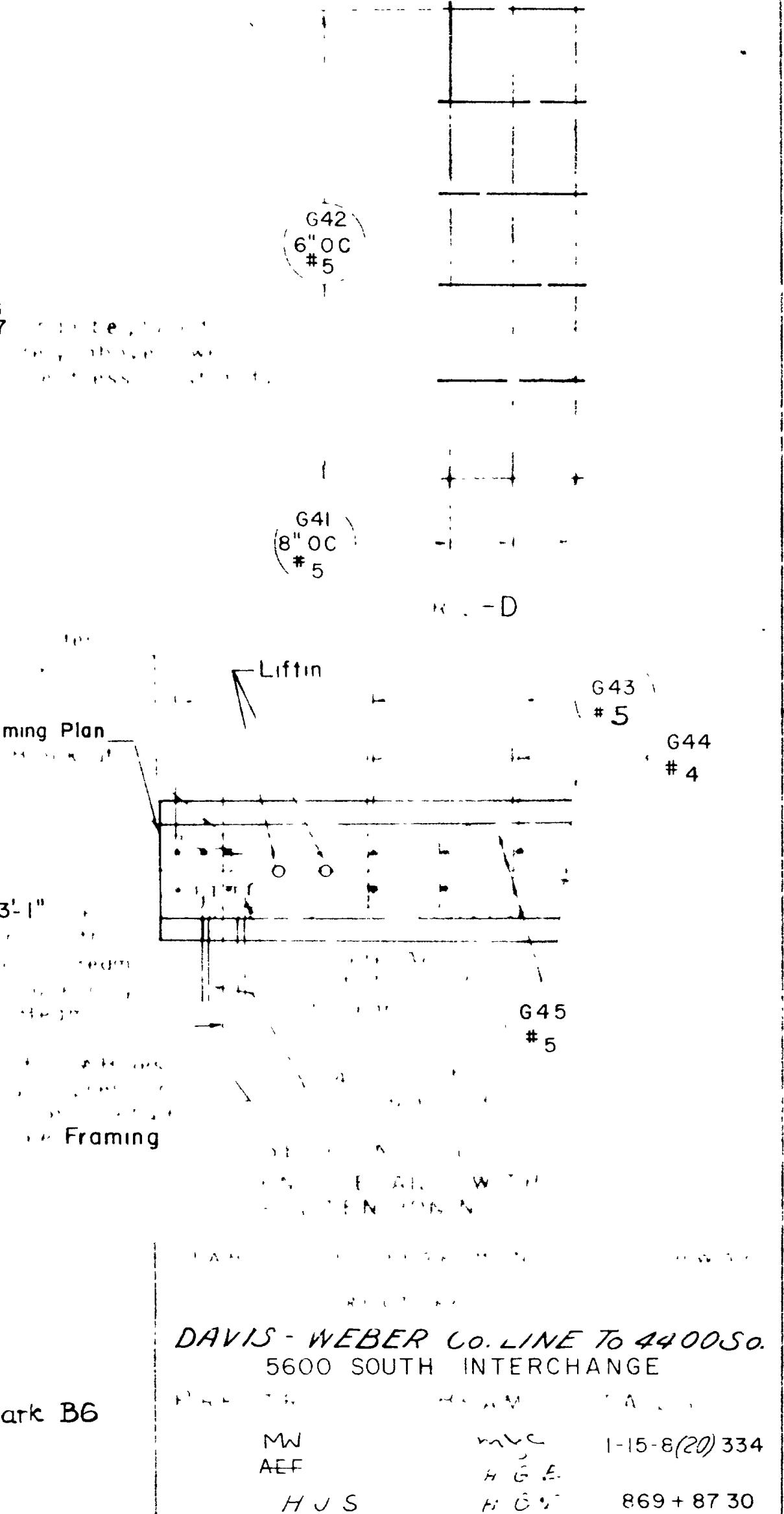
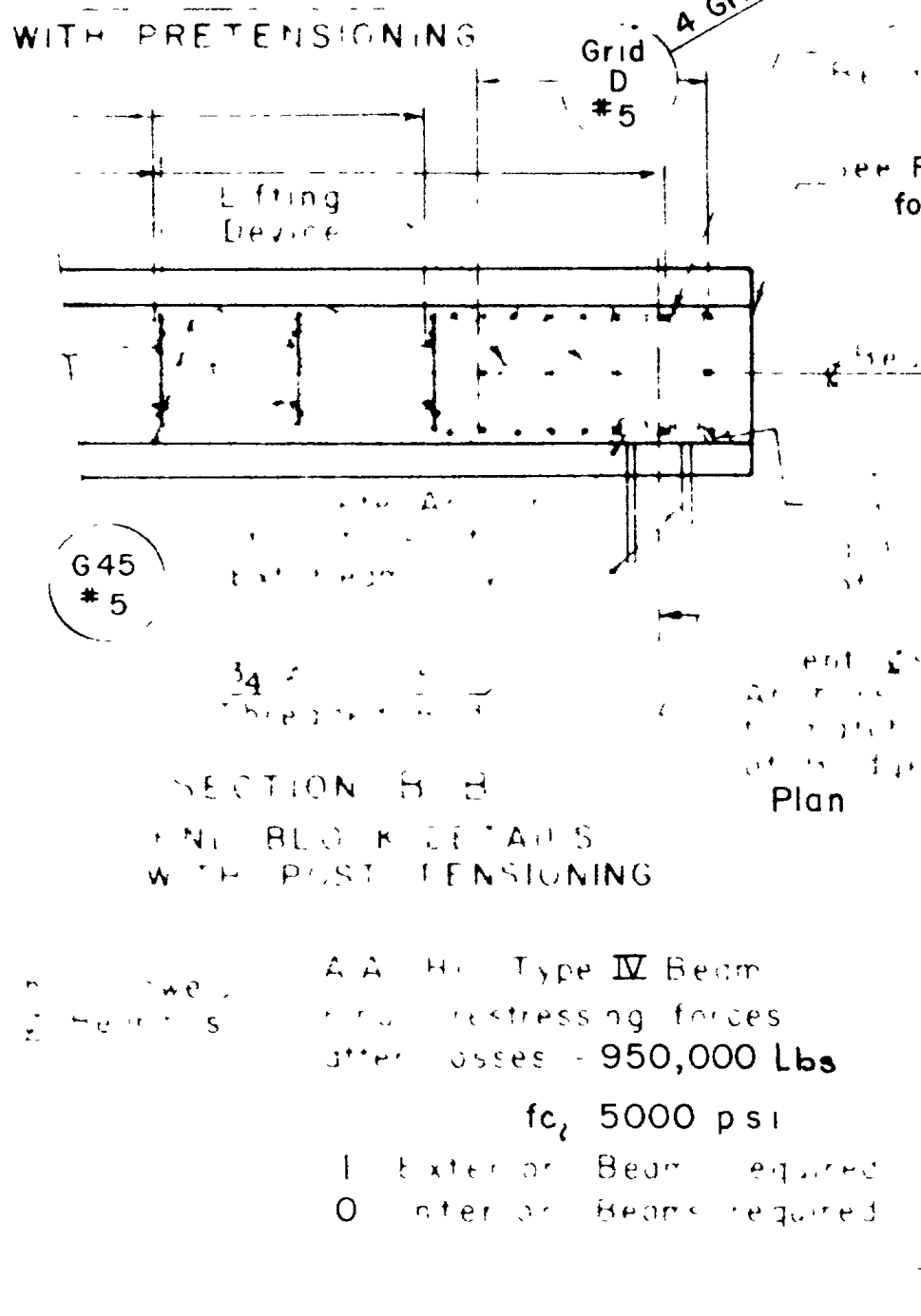
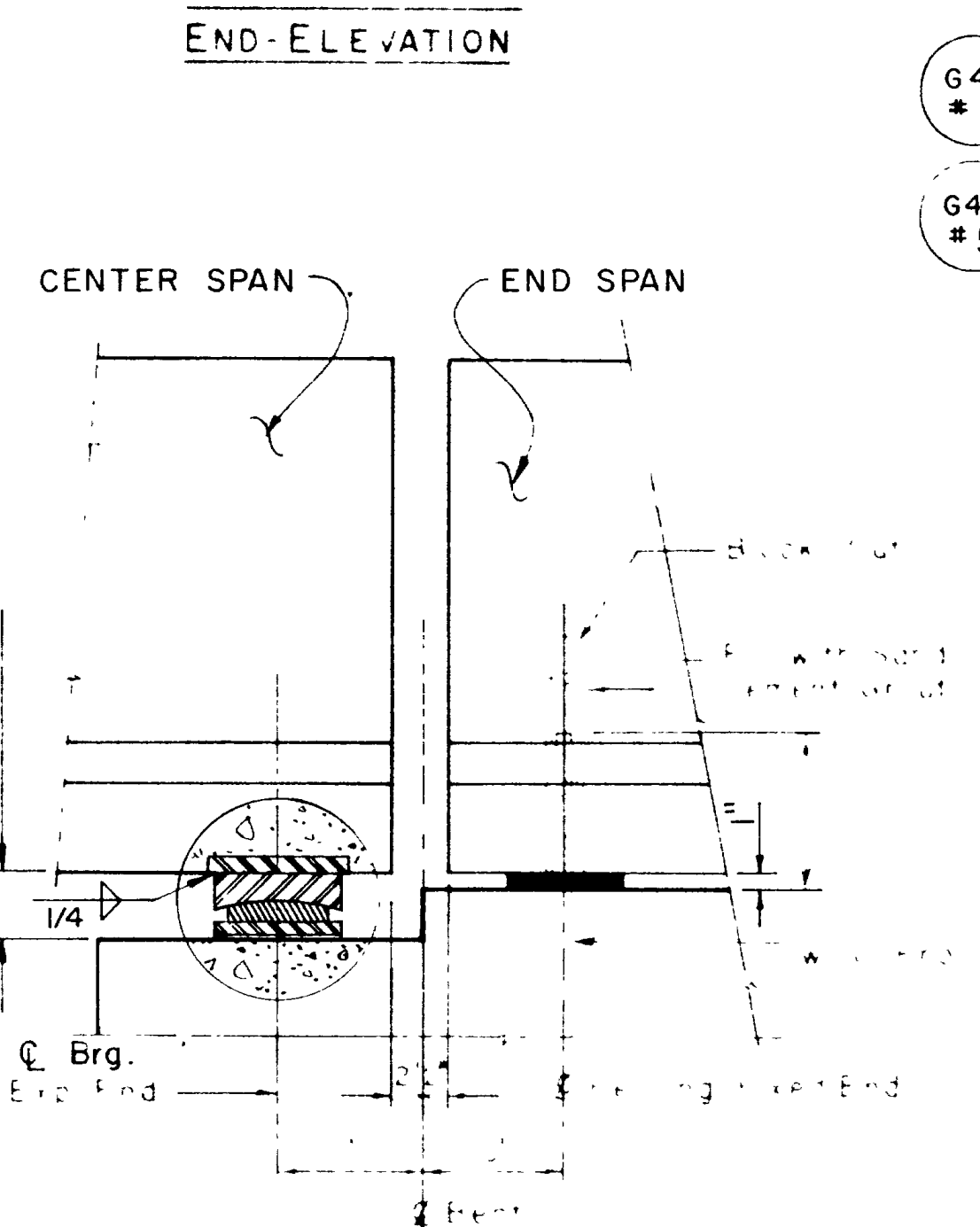
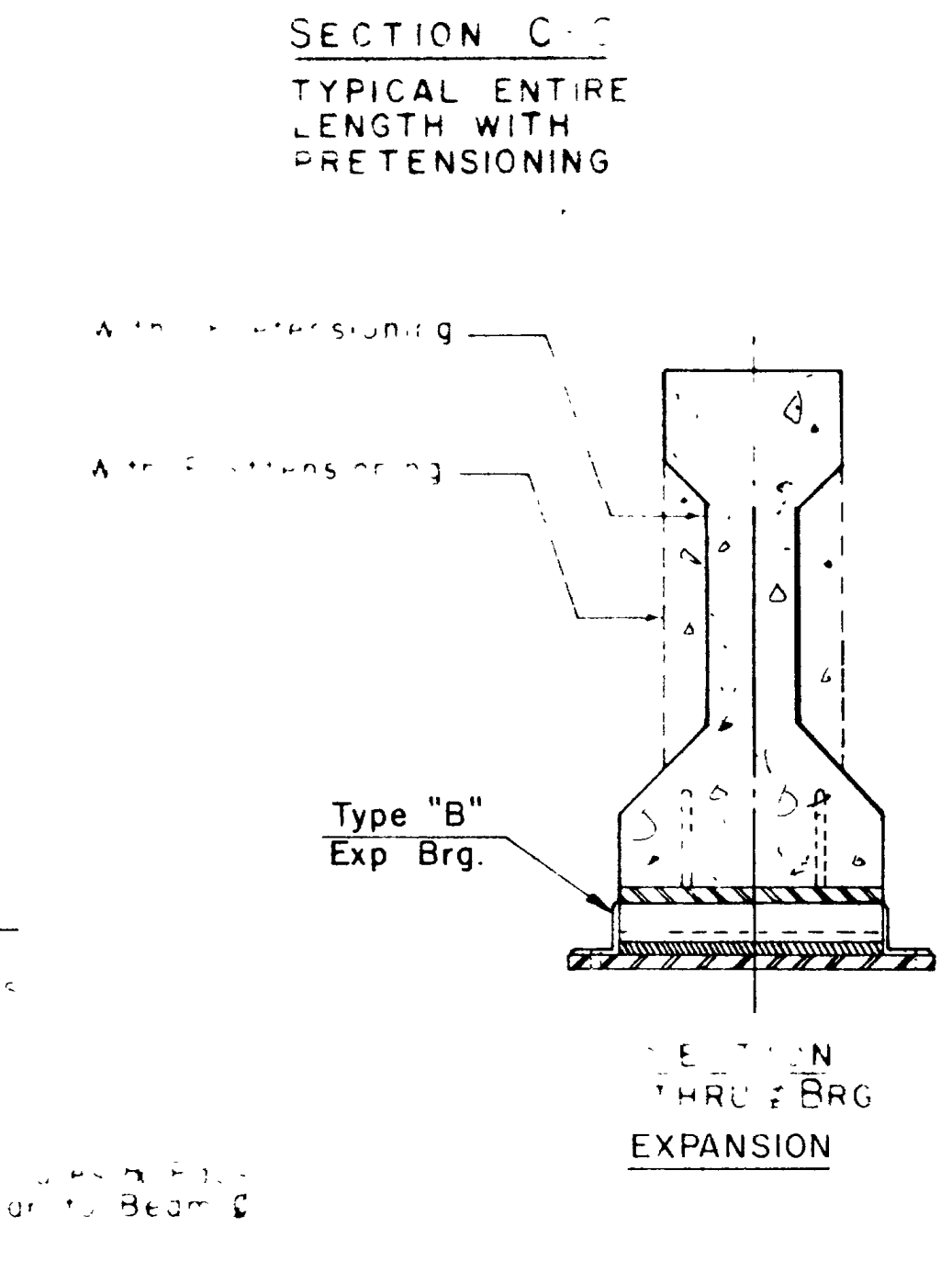
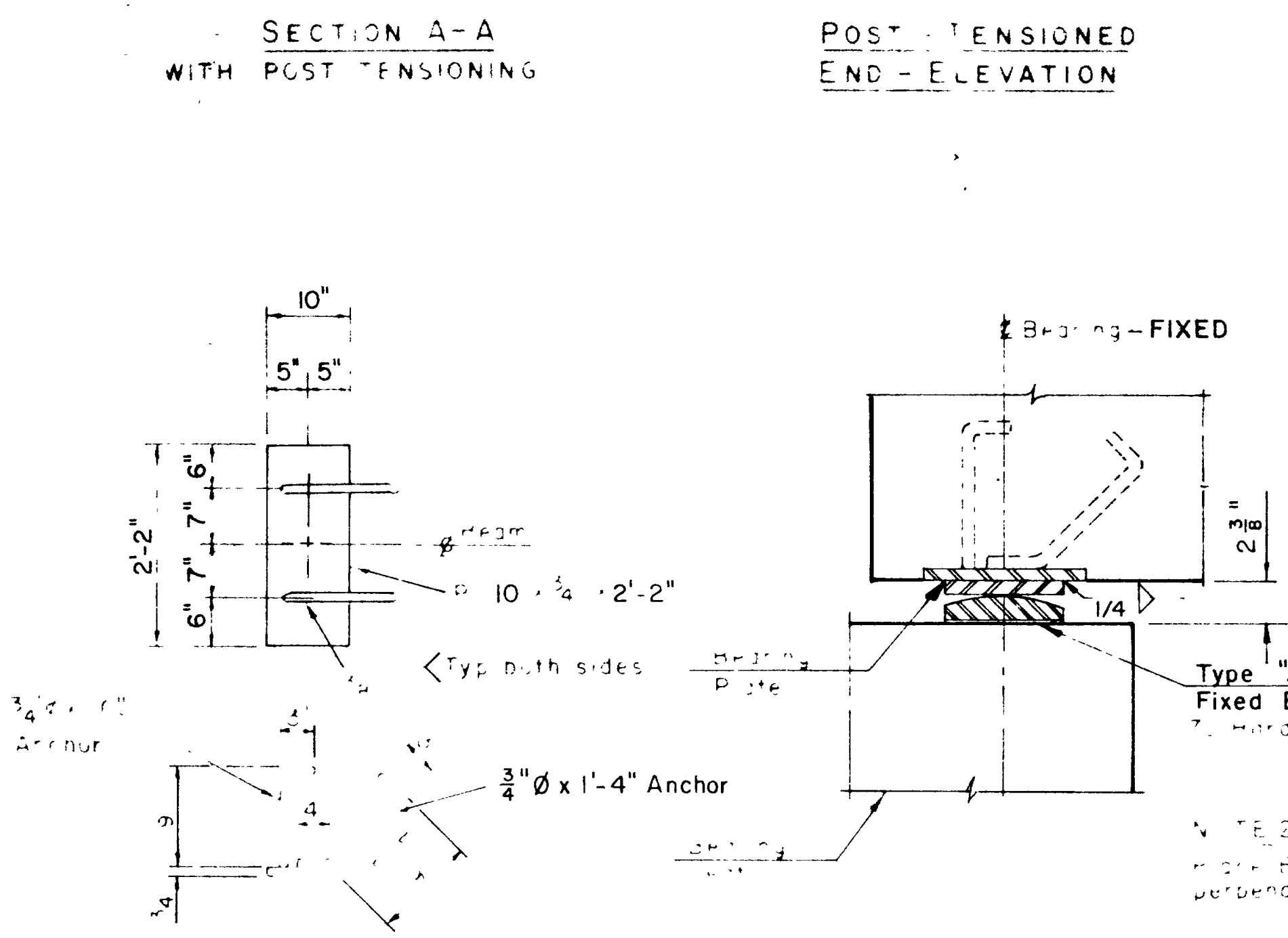
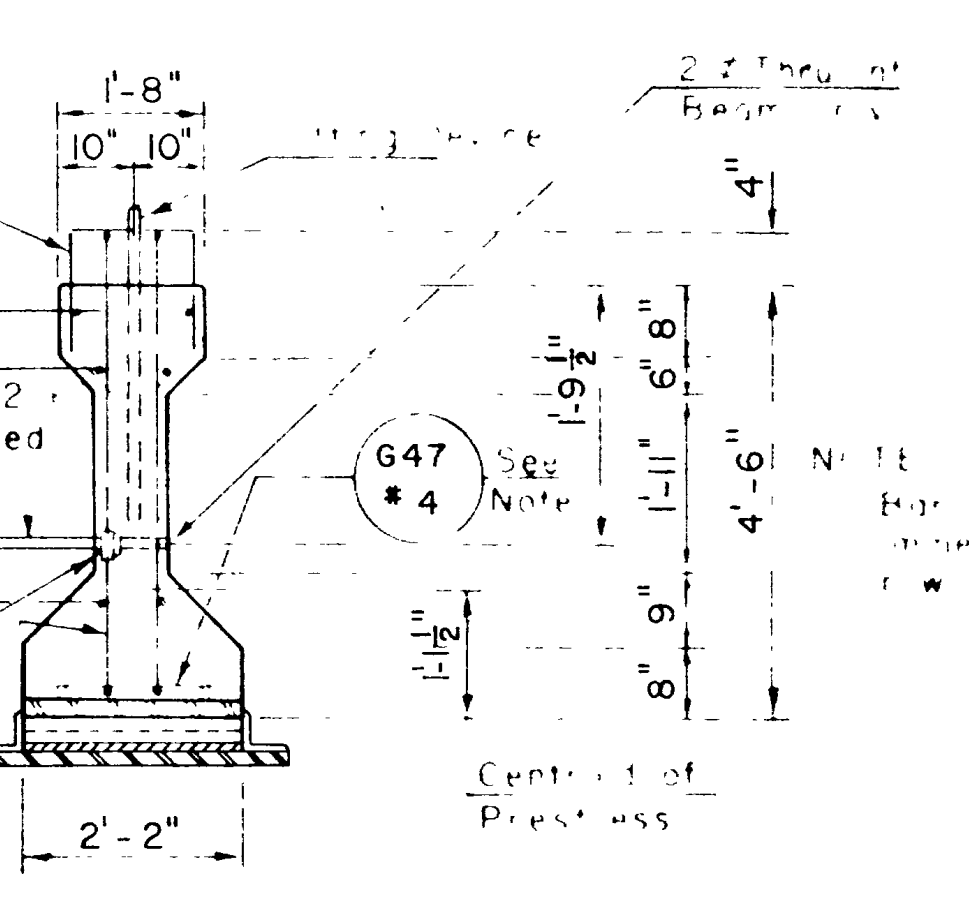
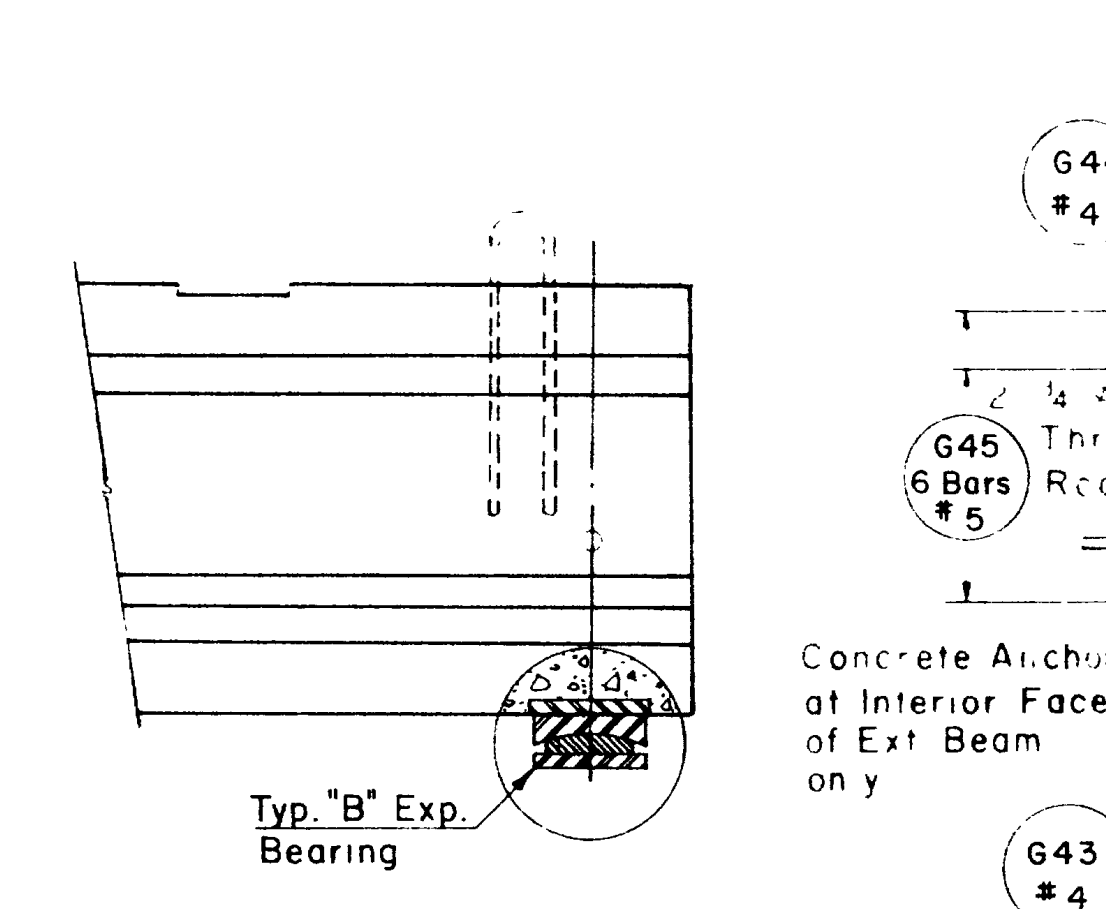
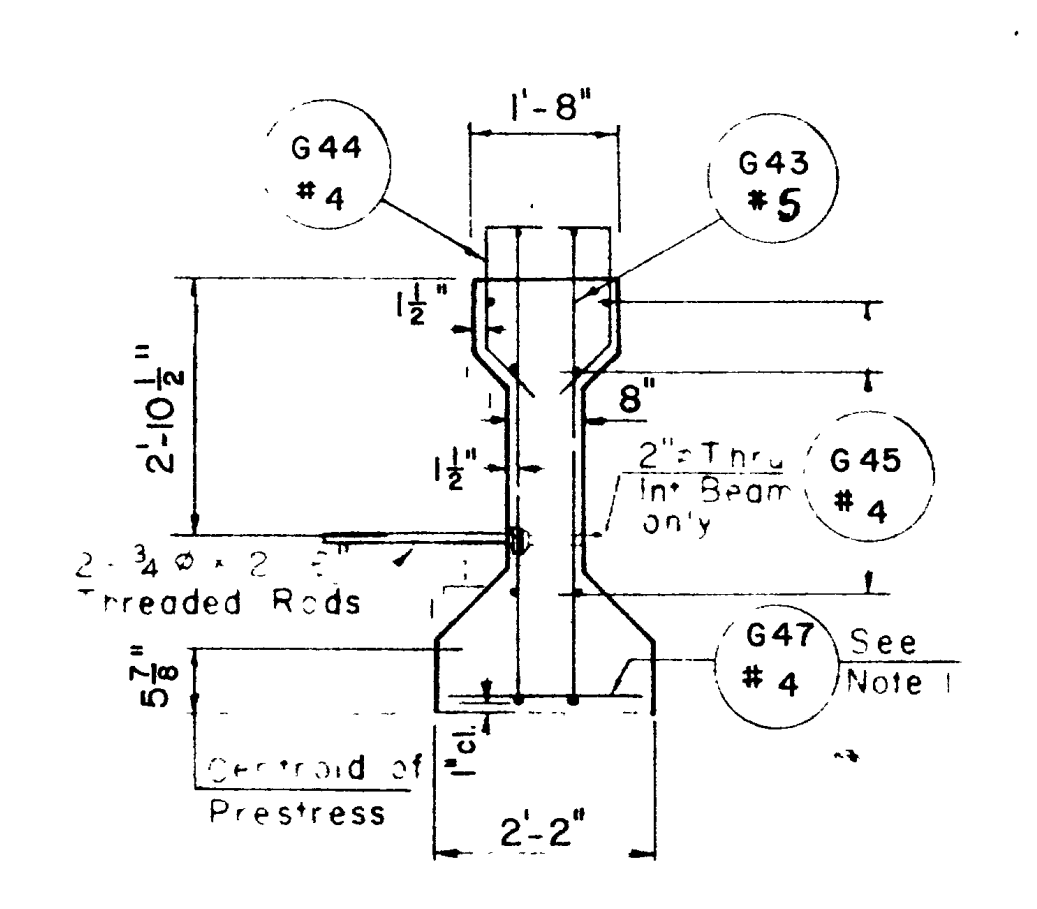
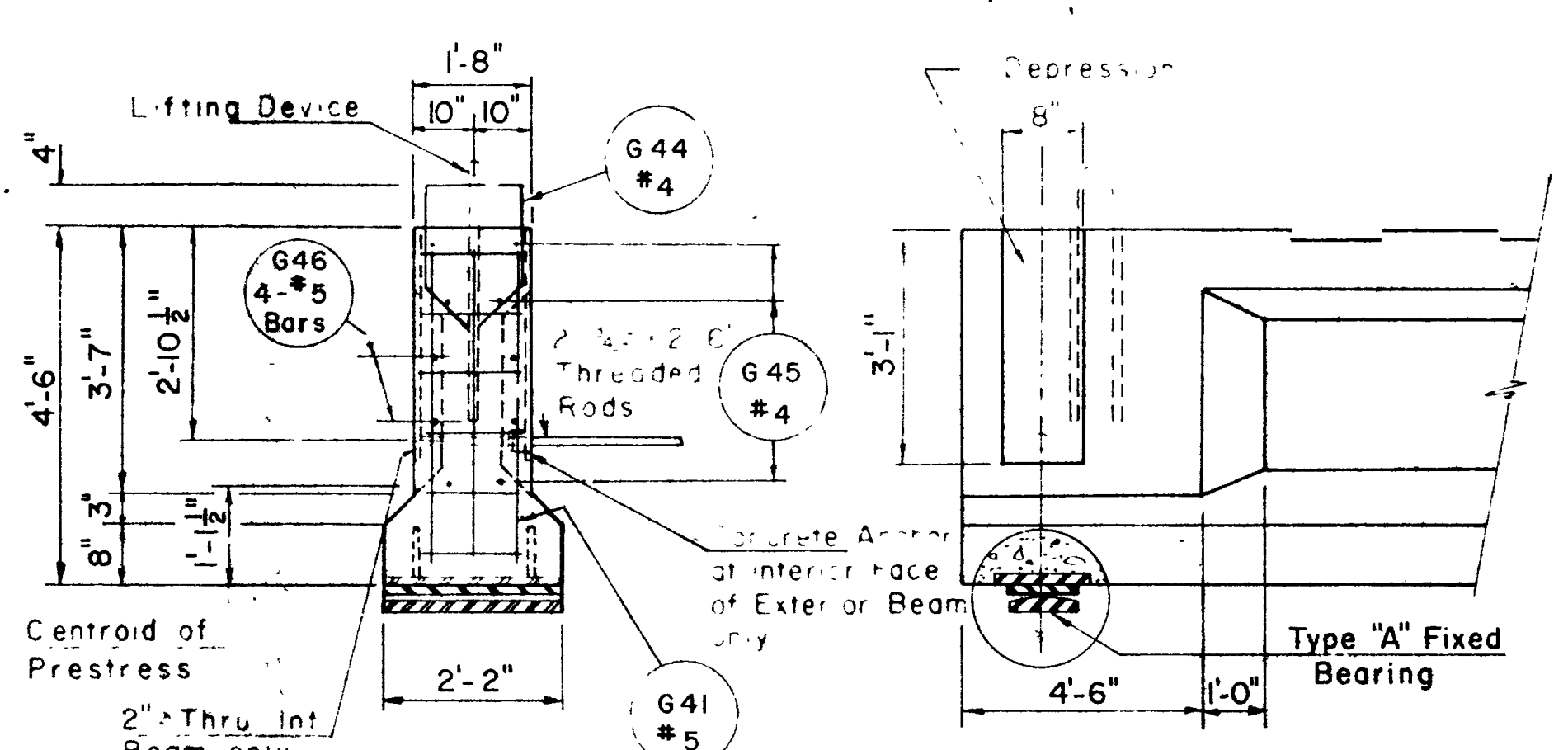
Mark B5

DAVIS-WEBER Co. LINE 70 4400 So.
 5600 SOUTH INTERCHANGE
 P.O. BOX 1000
 NW UMS H 61 H5-8120334
 HUS H 61 869+8730
 S. J. CHRISTIANSON WEBER



REINFORCING BAR SCHEDULE FOR BEAM									
MAP No.	SIZE	LENGTH	QUANTITY	REMARKS	QUANTITY	REMARKS	QUANTITY	REMARKS	QUANTITY
G41	5	4'-2"	24						
G42	5	1'-6"	72						
G43	5	6'-0"	132	132					
G44	4	5'-1"	76	74					
G45	5	84'-11"	6	6					
G46	5	1'-10"	8						
G47	4	2'-0"	76	66					
G48	5	4'-11"	20						

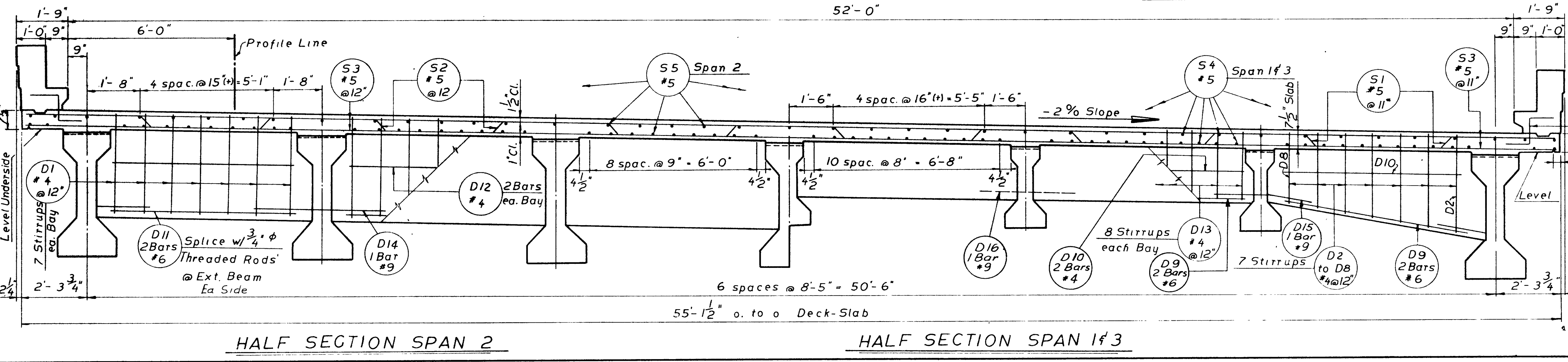
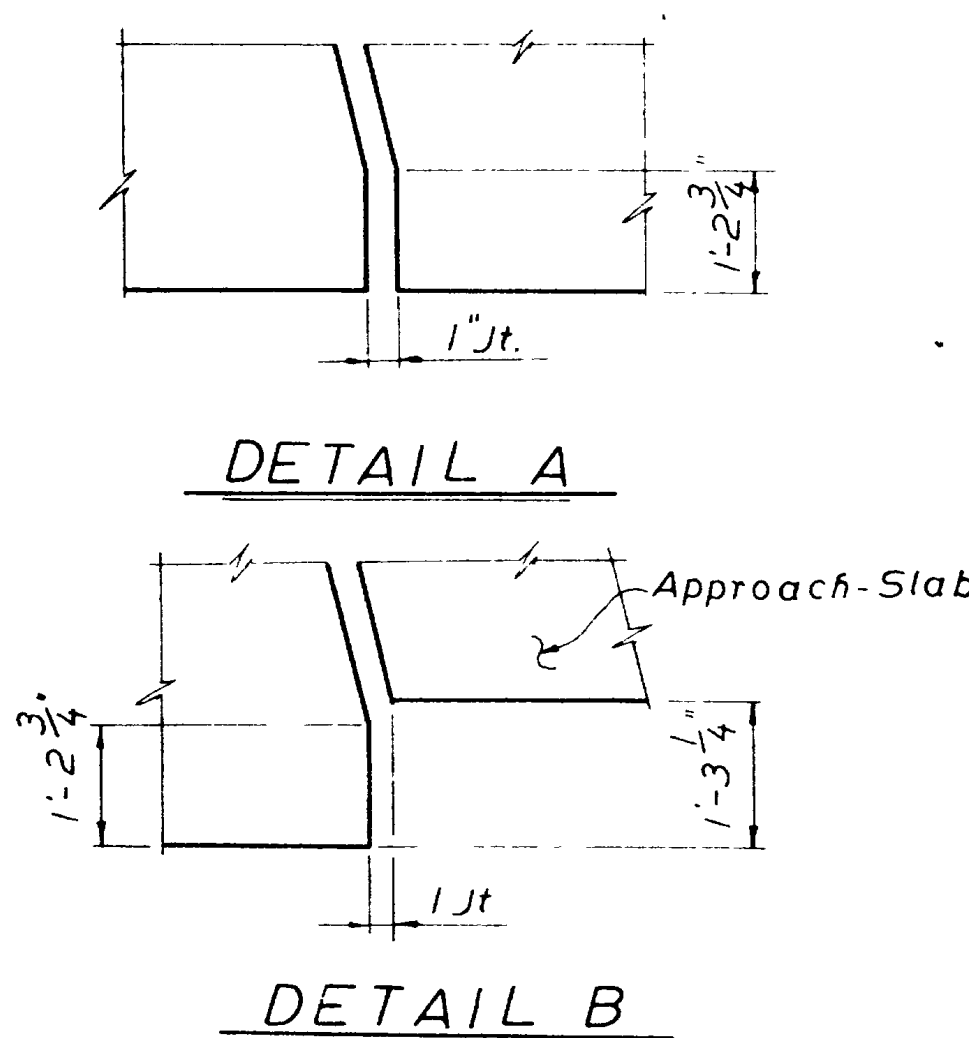
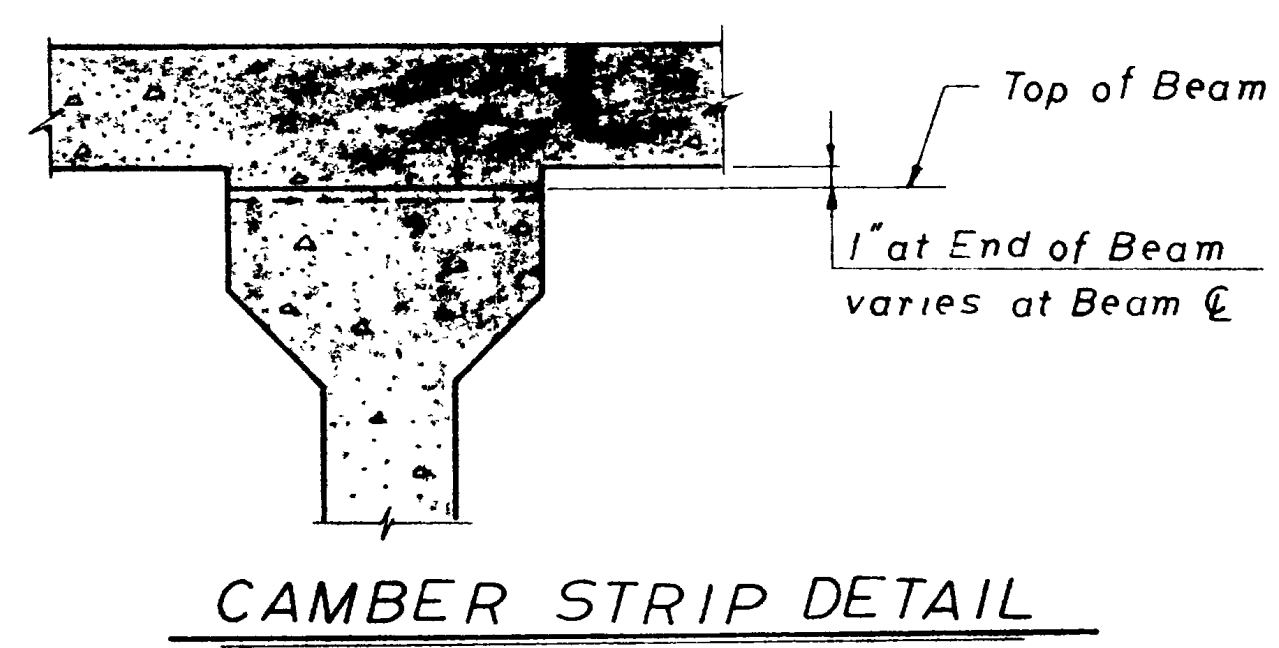
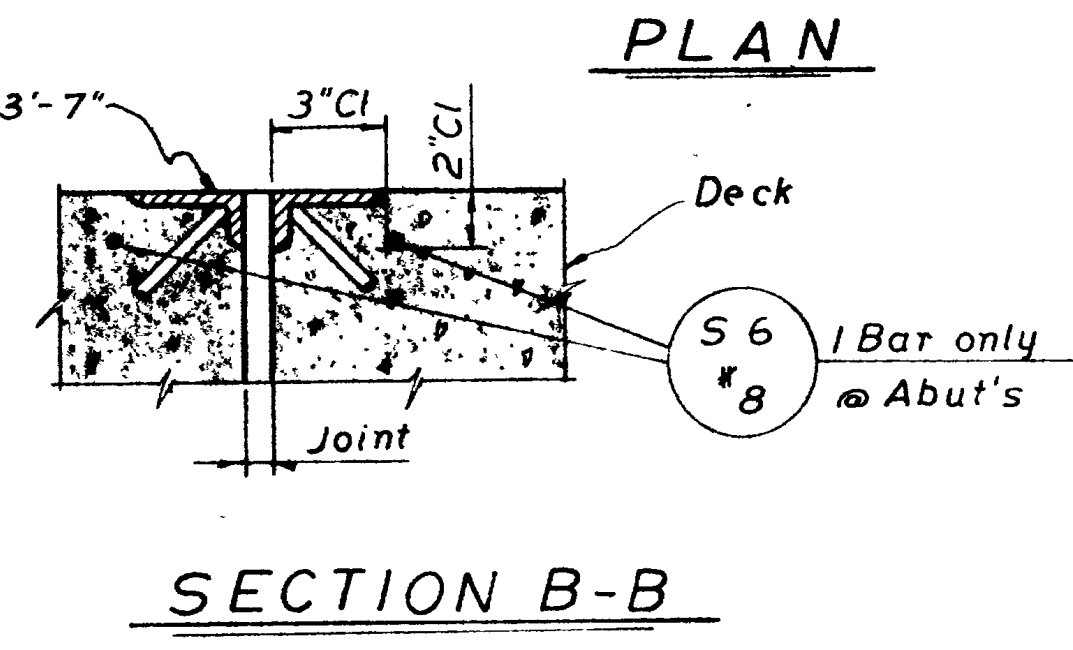
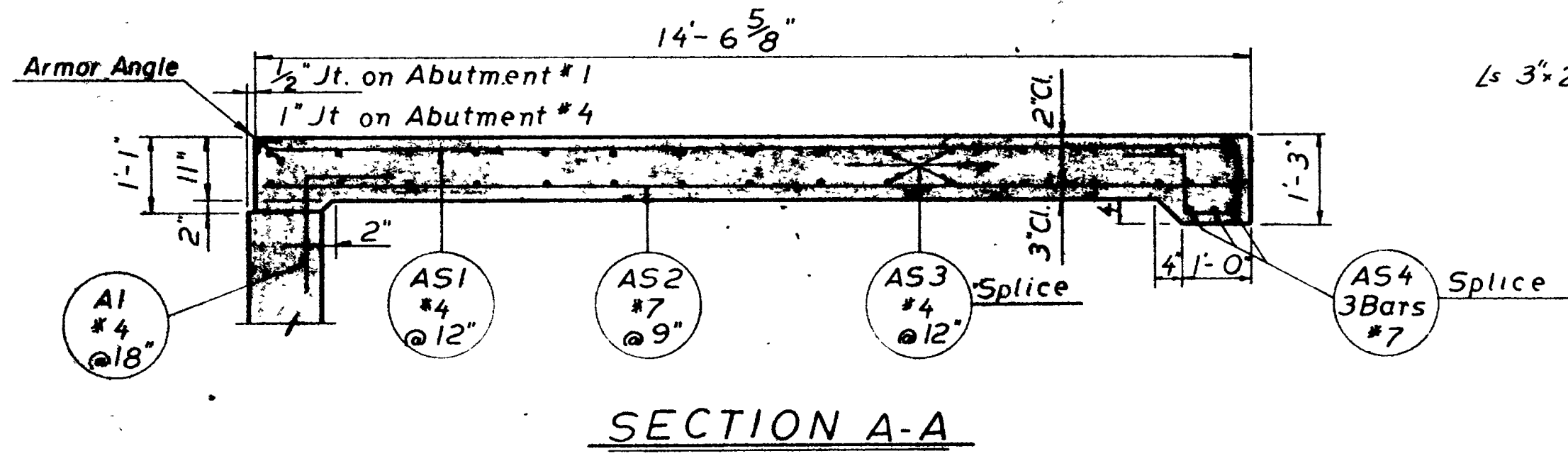
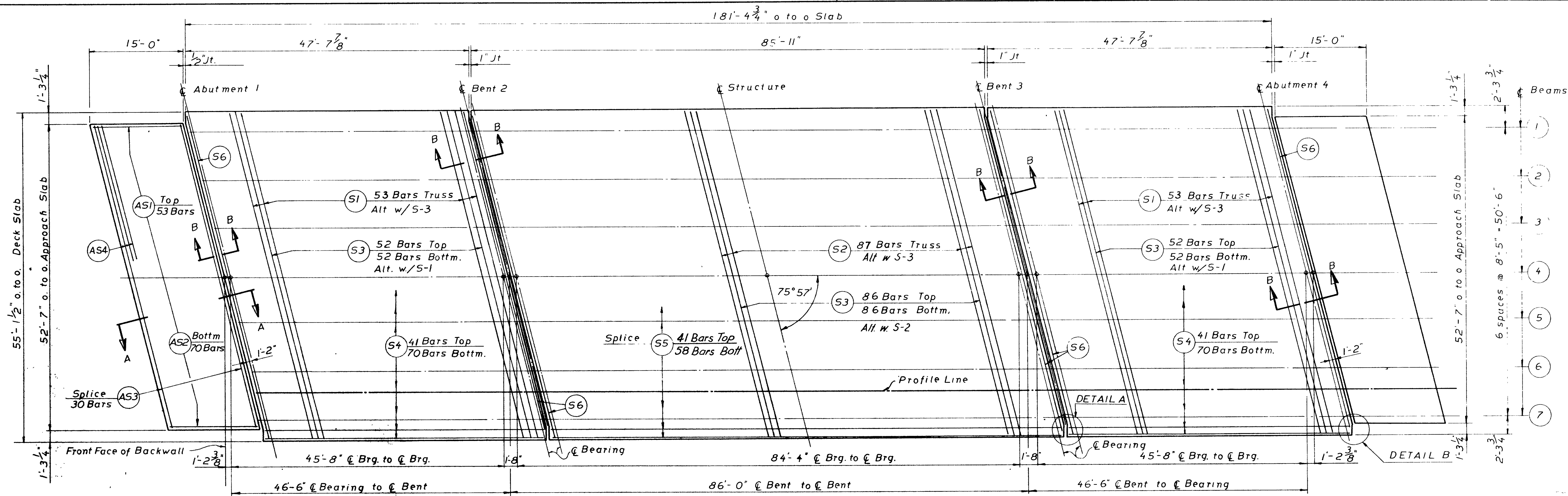
* With Post Tensioning
 Note: Bars shown otherwise shown are out of the beam



AA - Type IV Beam
 Prestressing forces
 after losses - 950,000 Lbs
 f_c 5000 psi
 1 Exterior Beam required
 0 Interior Beams required

DAVIS-WEBER Co. LINE TO 4400 So.
 5600 SOUTH INTERCHANGE
 MW
 AEF
 HUS
 1-15-8(20) 334
 H.G.E.
 869+8730
 WEBER

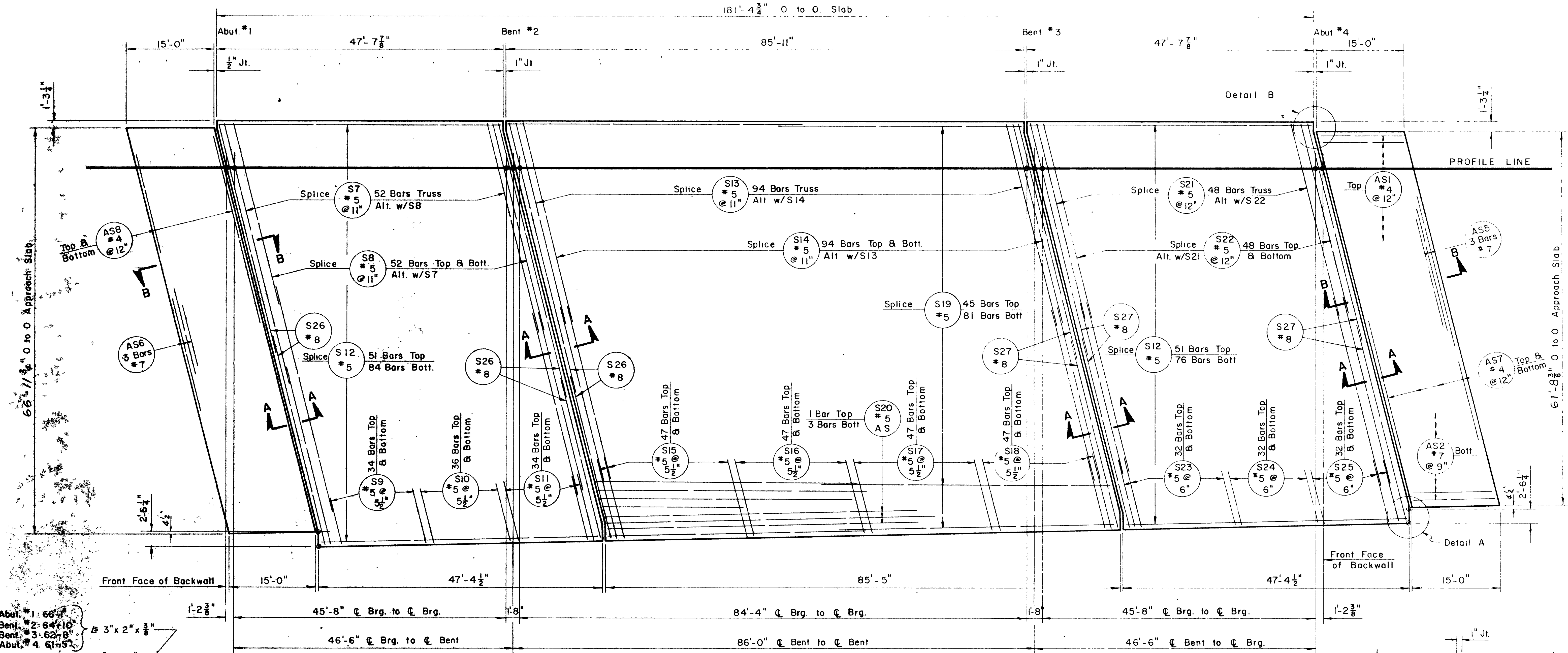
REVISOR BY DATE
 REVISOR BY DATE
 REVISOR BY DATE
 REVISOR BY DATE



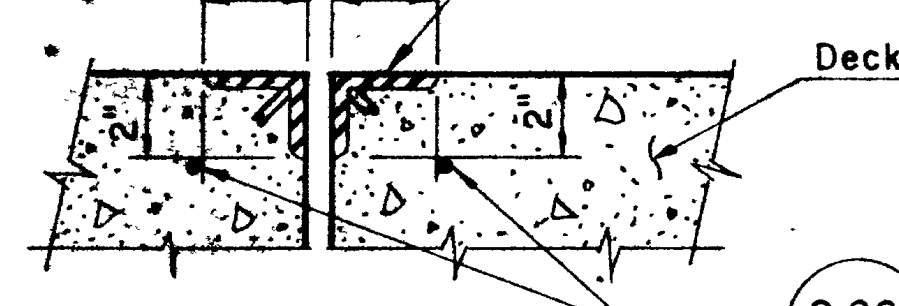
UTAH STATE DEPARTMENT OF HIGHWAYS			
SALT LAKE CITY, UTAH			
STRUCTURES DIVISION			
DAVIS-WEBER Co. LINE TO 4400 So.			
5600 SOUTH INTERCHANGE			
DECK DETAIL SBL			
DESIGNED BY MW	CHECKED BY MJC	1-15-8(20)334	PROJECT NUMBER
DRAWN BY U.M.S.	CHECKED BY H.J.S.		869+87.30
QUANTITIES BY H.J.S.	CHECKED BY H.G.E.		STATION
APPROVED BY MJC	GROUP LEADER		WEBER COUNTY
APPROVED DATE Sept. 4, 1963	DATE		
BR NO	DRG NO F-66	16 OF 26	

DATE _____
 REVISIONS
 DATE _____
 DATE _____
 DATE _____

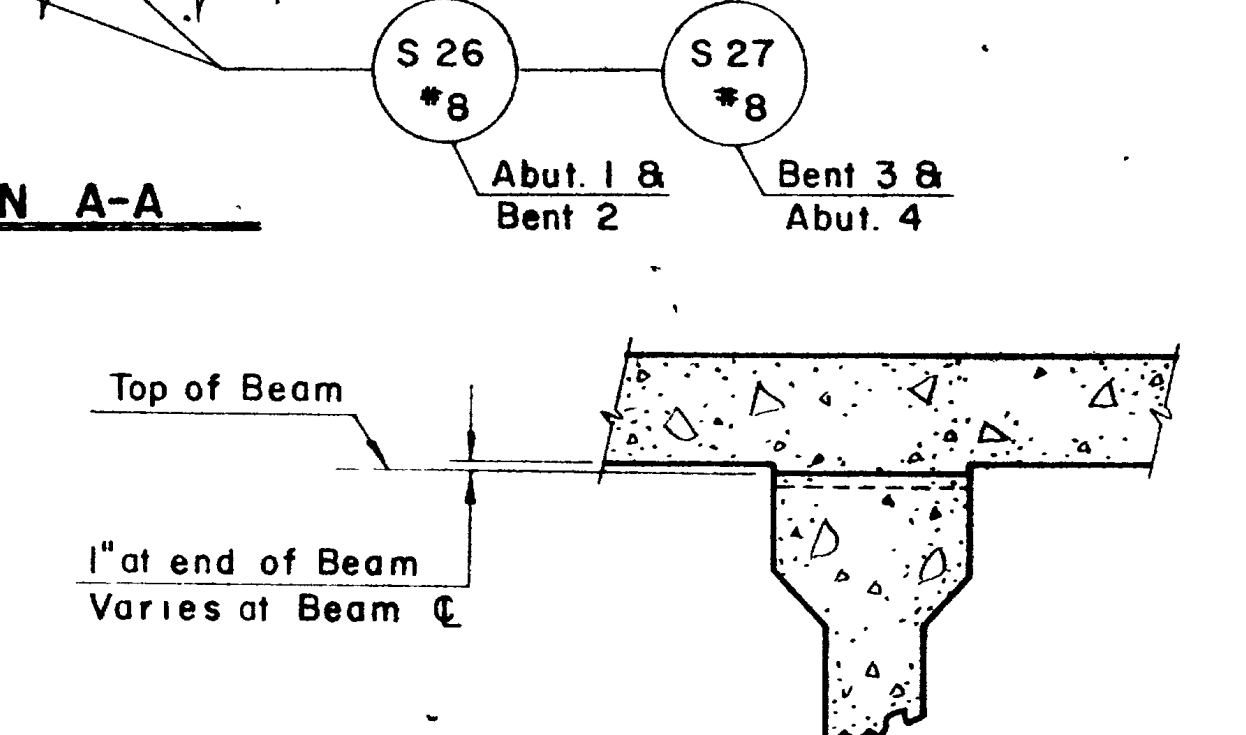
181'-4 3/4" O to O Slab



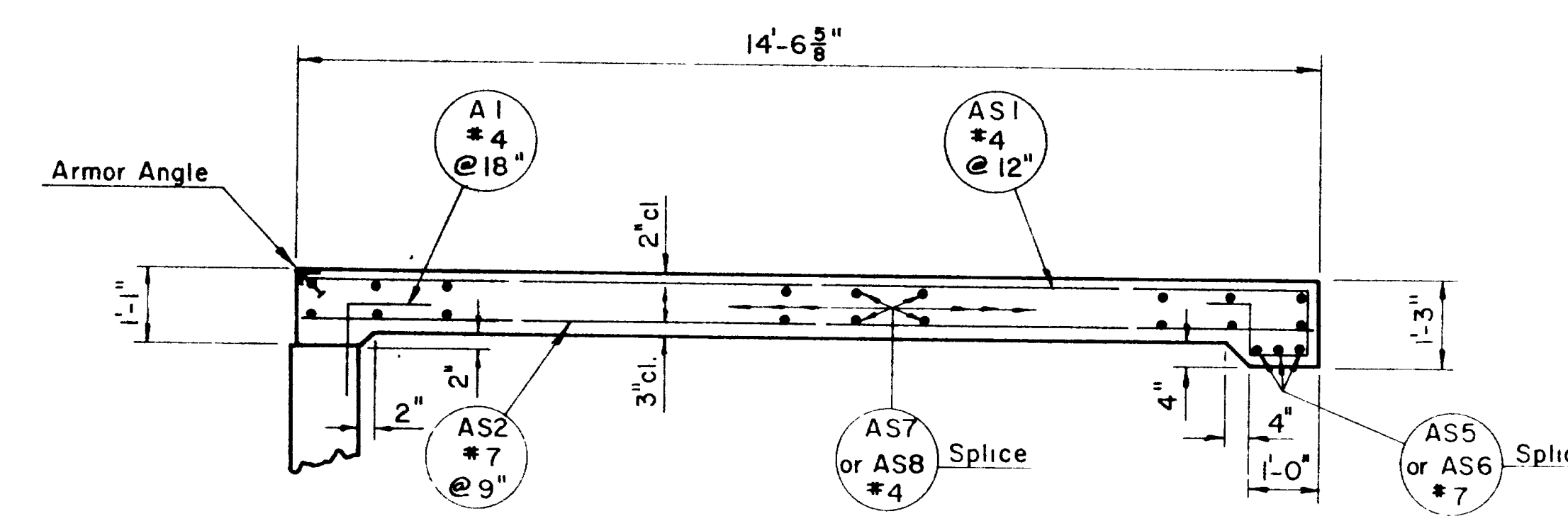
DECK PLAN



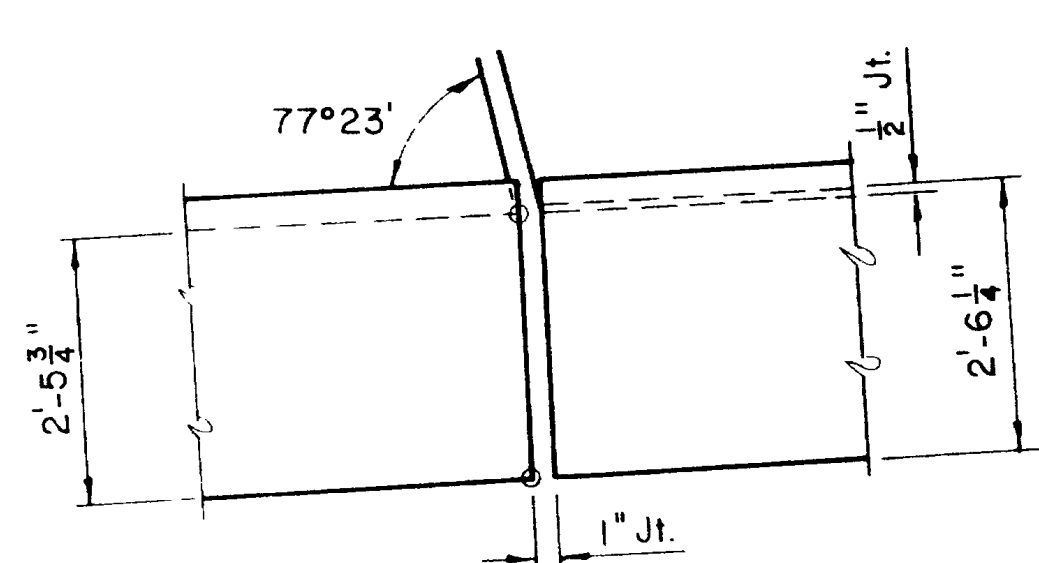
SECTION A-A



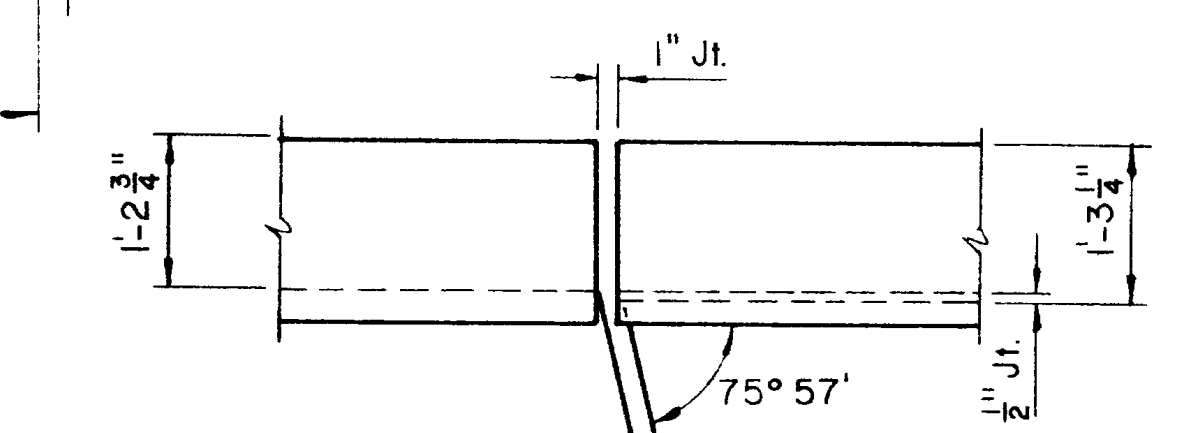
CAMBER STRIP DETAIL



APPROACH SLAB SECTION B-B

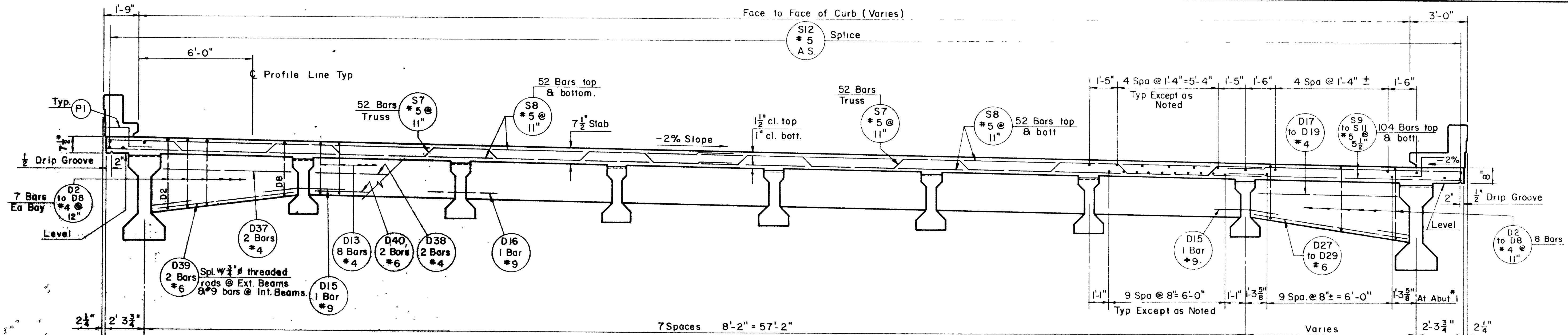


DETAIL A

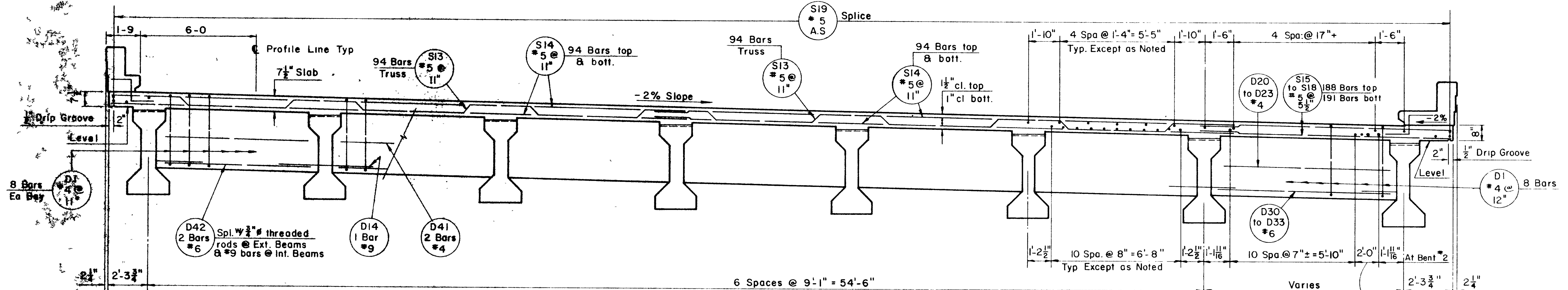


DETAIL B

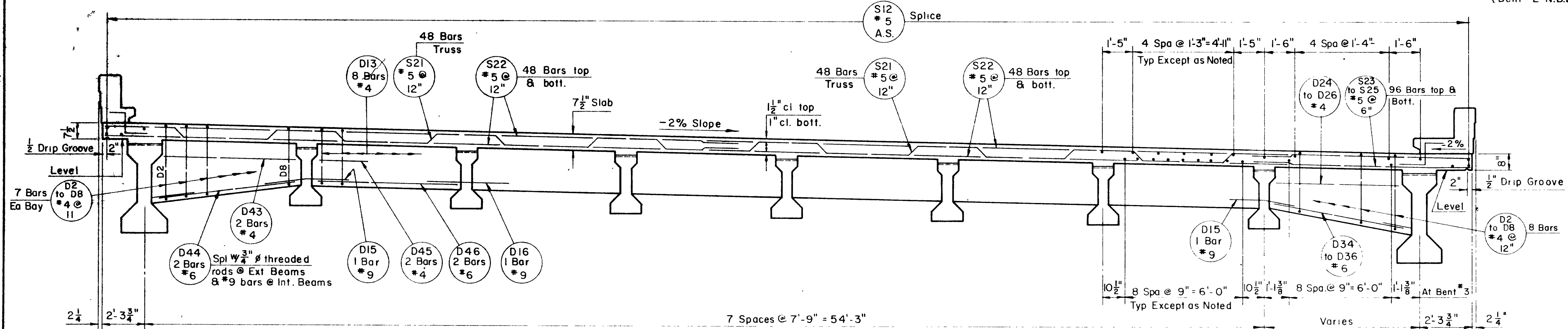
UTAH STATE DEPARTMENT OF HIGHWAYS		
SALT LAKE CITY, UTAH		
STRUCTURES DIVISION		
DAVIS-WEBER Co. LINE TO 4400 So.		
5600 SOUTH INTERCHANGE		
DECK DETAIL - N.B.L.		
DESIGNED BY: MN	CHECKED BY: MJC	1-15-8(20) 334
DRAWN BY: AEF	CHECKED BY: H.J.S.	PROJECT NUMBER
QUANTITIES BY: H.J.S.	CHECKED BY: H.G.E.	869+87.30
APPROVAL: <i>[Signature]</i>	GROUP: <i>[Signature]</i>	STATION
APPROVED: <i>[Signature]</i>	DATE: <i>[Signature]</i>	WEBER
BR NO	DRG NO	F-66
		17 OF 26



SECTION THRU DECK (SPAN-1)



SECTION THRU DECK (SPAN-2)



SECTION THRU DECK (SPAN-3)

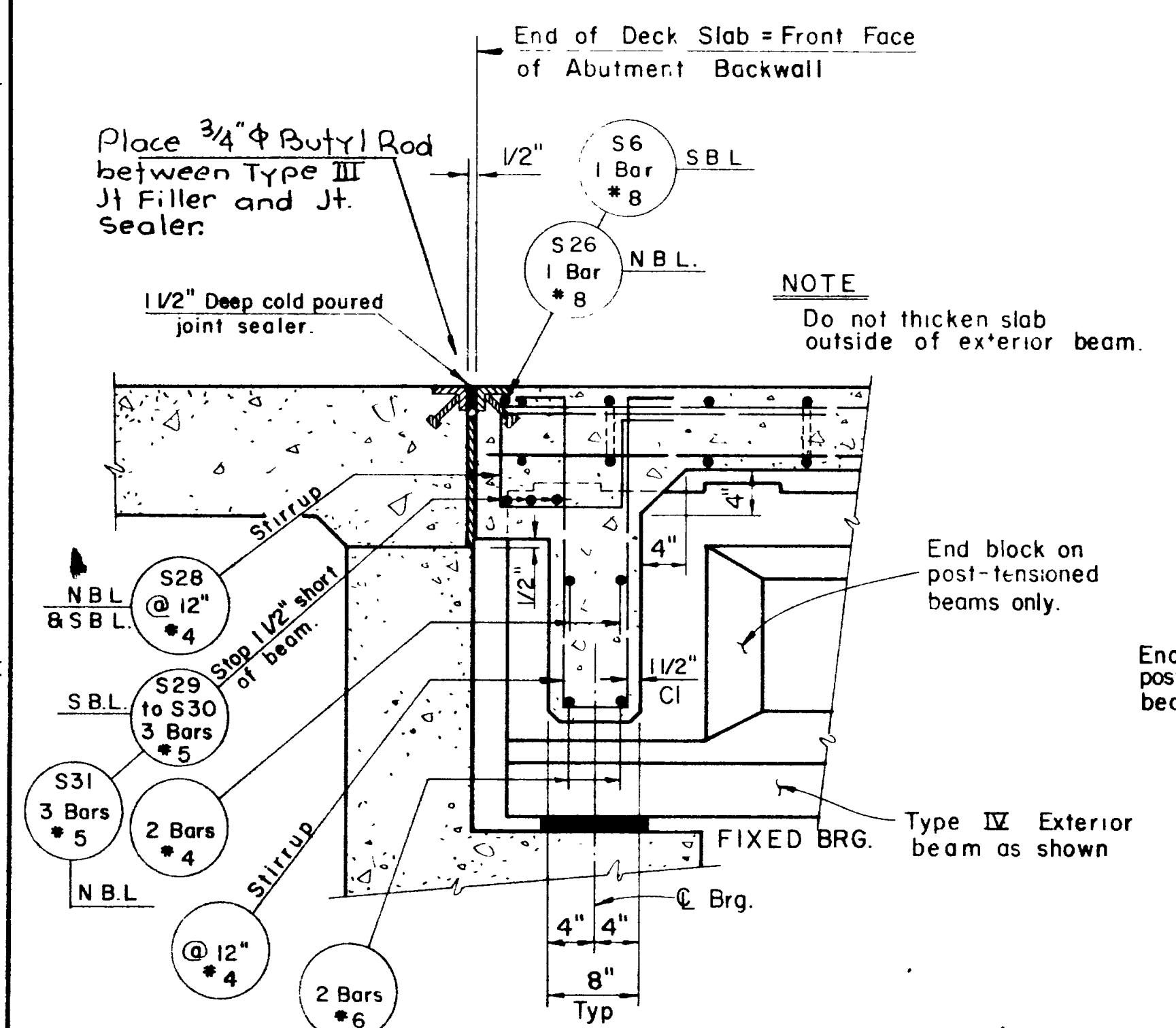
UTAH STATE DEPARTMENT OF HIGHWAYS			
SALT LAKE CITY, UTAH			
STRUCTURES DIVISION			
DAVIS-WEBER Co. LINE To 4400 So.			
5600 SOUTH INTERCHANGE			
DECK SECTIONS N.B.L.			
DESIGNED BY MW	CHECKED BY mxc	PROJECT NO. 1-15-8(20)334	
DRAWN BY AEF	CHECKED BY HJS	PROJECT NUMBER 869+8730	
APPROVAL	RECOMMENDED BY mxc	DATE	STATION
APPROVED			WEBER
BR	NO. F-66		18 OF 26

REVISOR: DATE

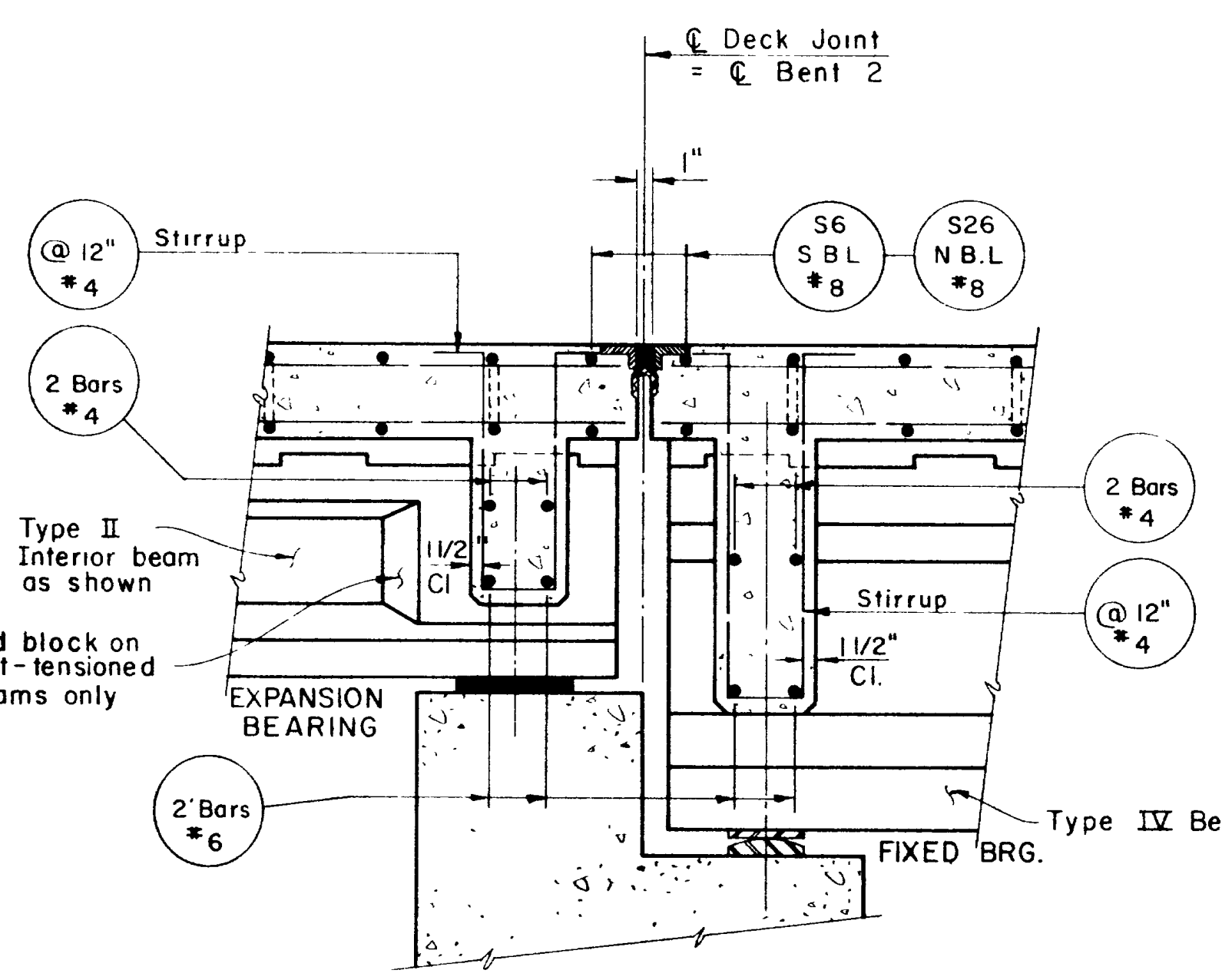
REVISOR: DATE

REVISOR: DATE

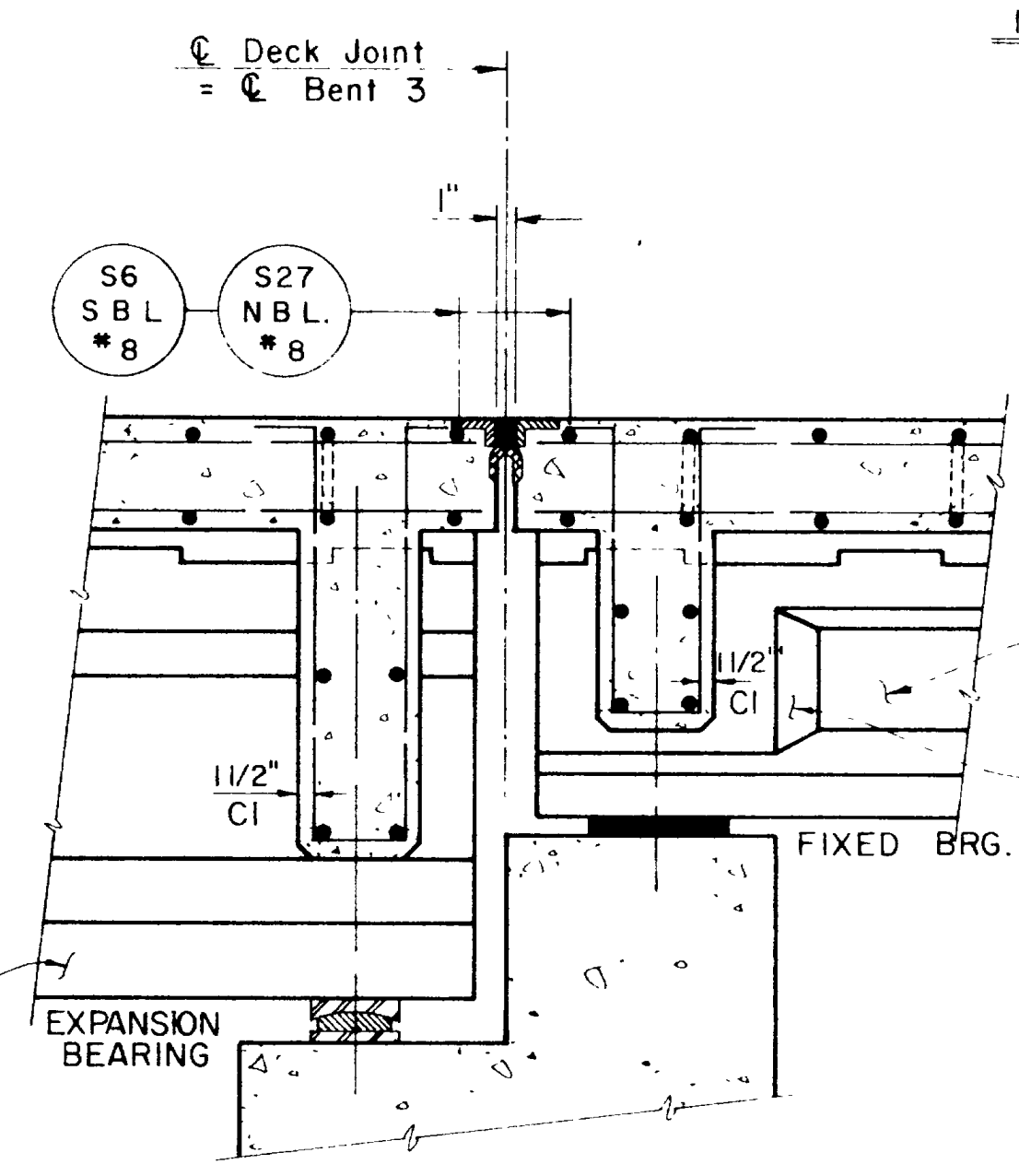
REVISOR: DATE



SECTION AT ABUTMENT #1
(Normal to Backwall)

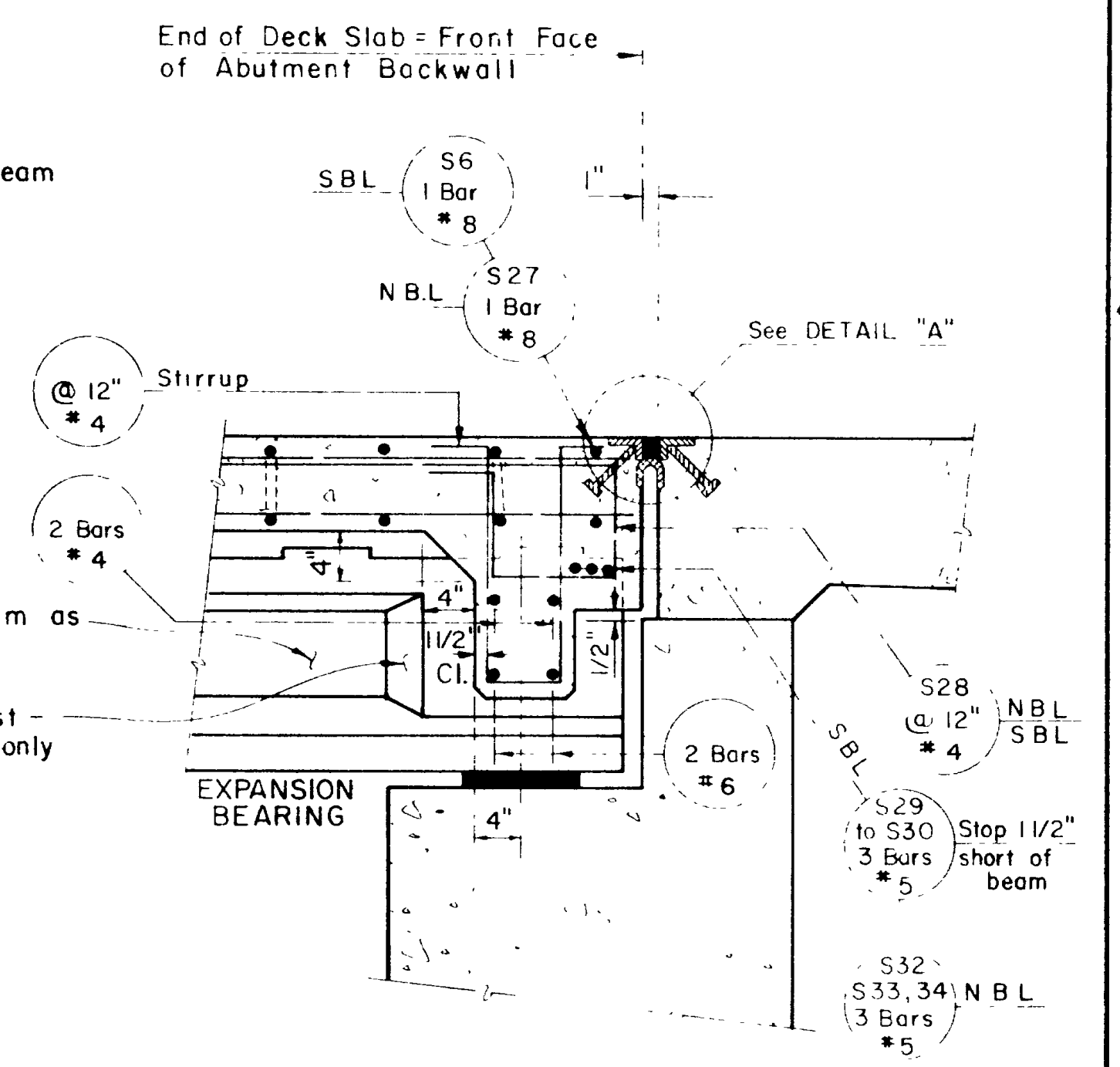


SECTION AT BENT #2
(Normal to Diaphragm)

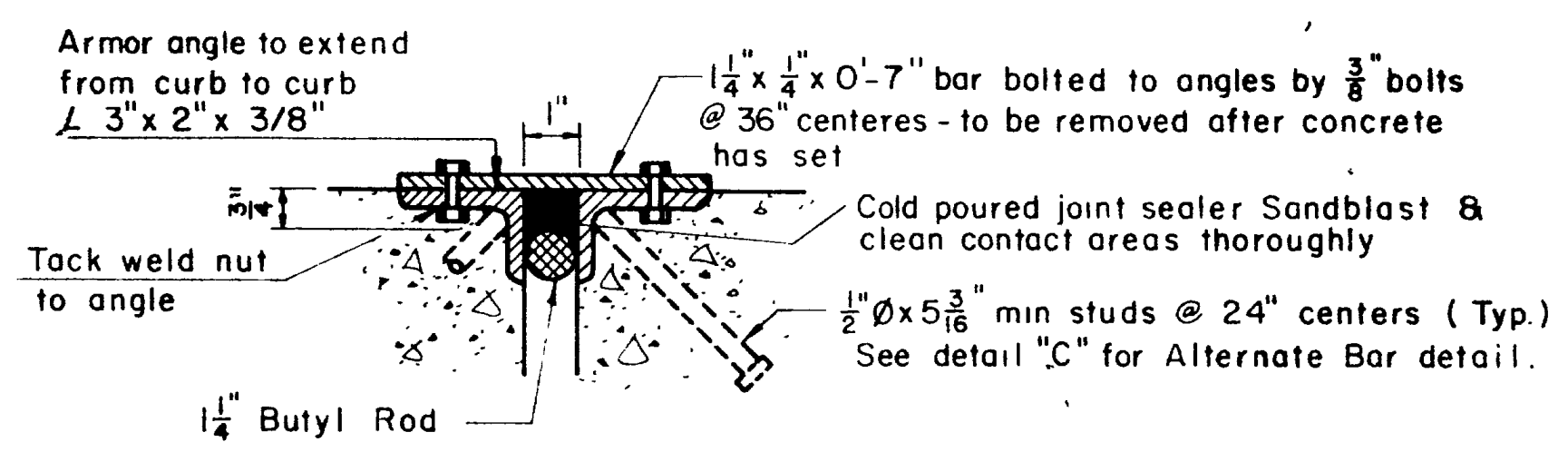


SECTION AT BENT #3
(Normal to Diaphragm)

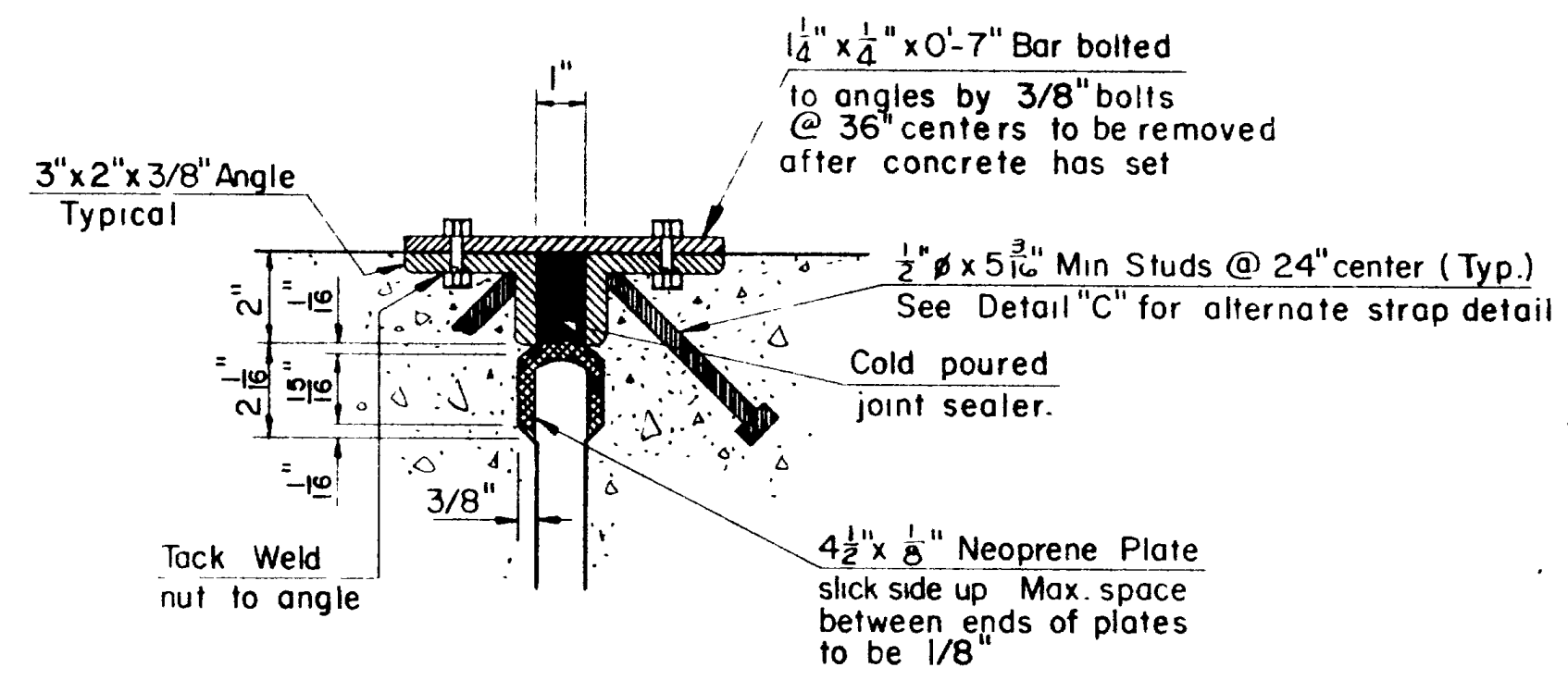
NOTE
Do not thicken slab outside of exterior beam



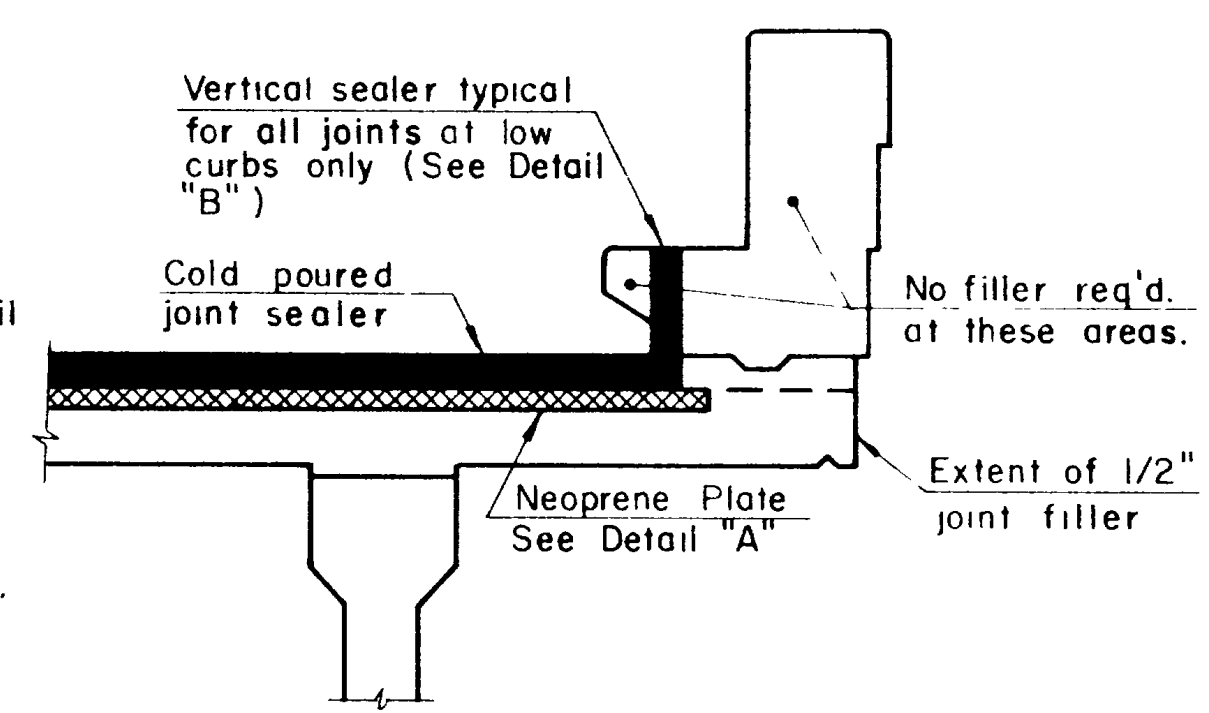
SECTION AT ABUTMENT #4
(Normal to Backwall)



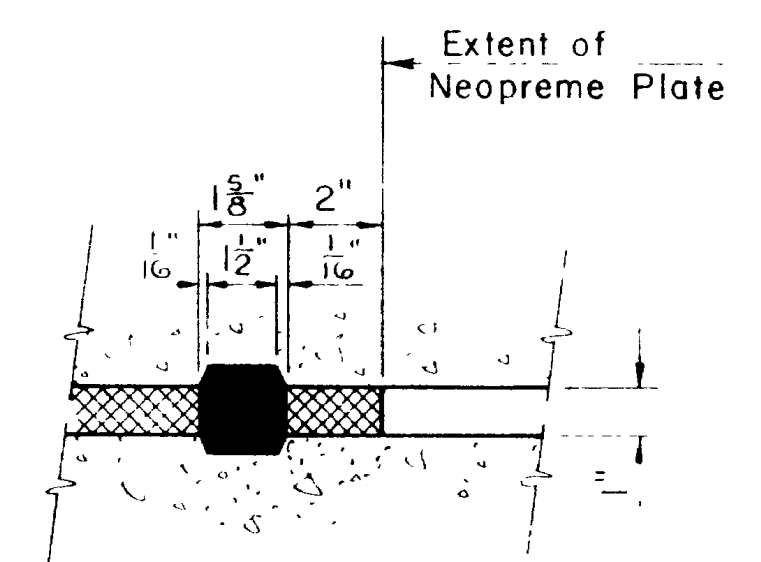
ALTERNATE DETAIL



DETAIL "A"



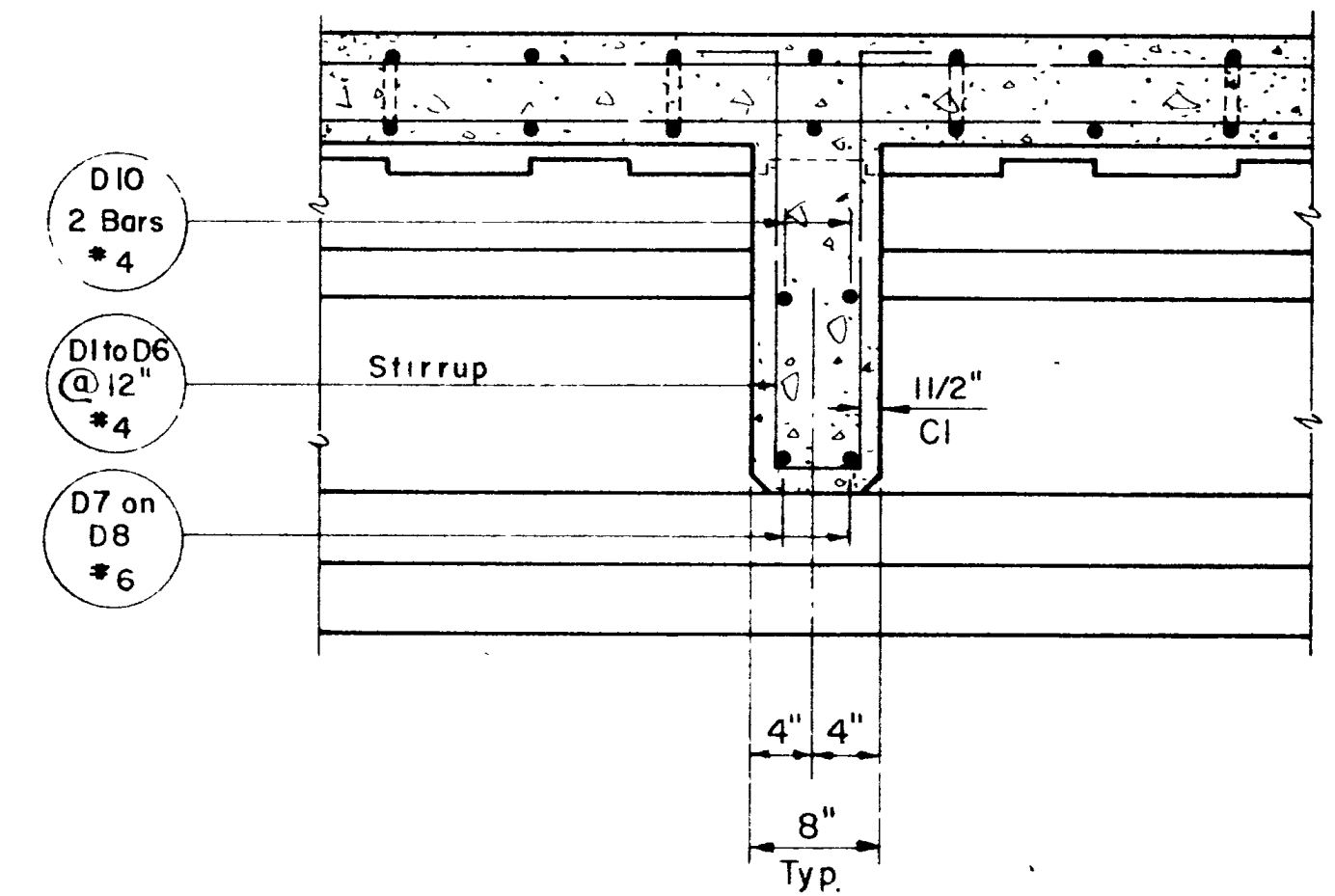
SECTION AT JOINT



DETAIL "B"

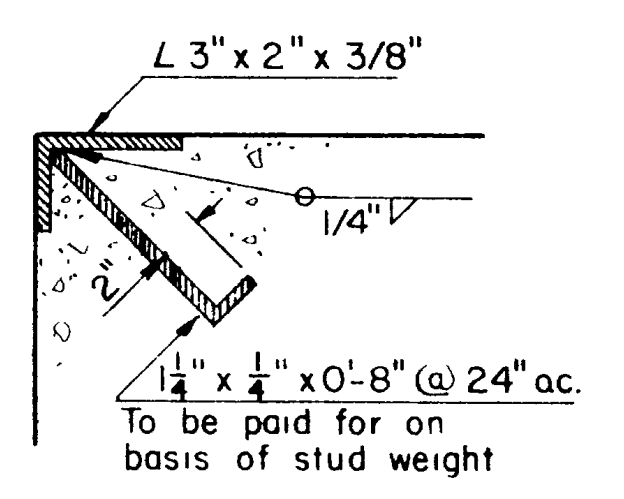
NOTES

- 1 Bridge seat bearing areas (2' larger than Elastomeric Pad) shall be finished high and rubbed or ground to grade within 1/16" ± No grouting
- 2 For Beam details see sheets No 11 to 15

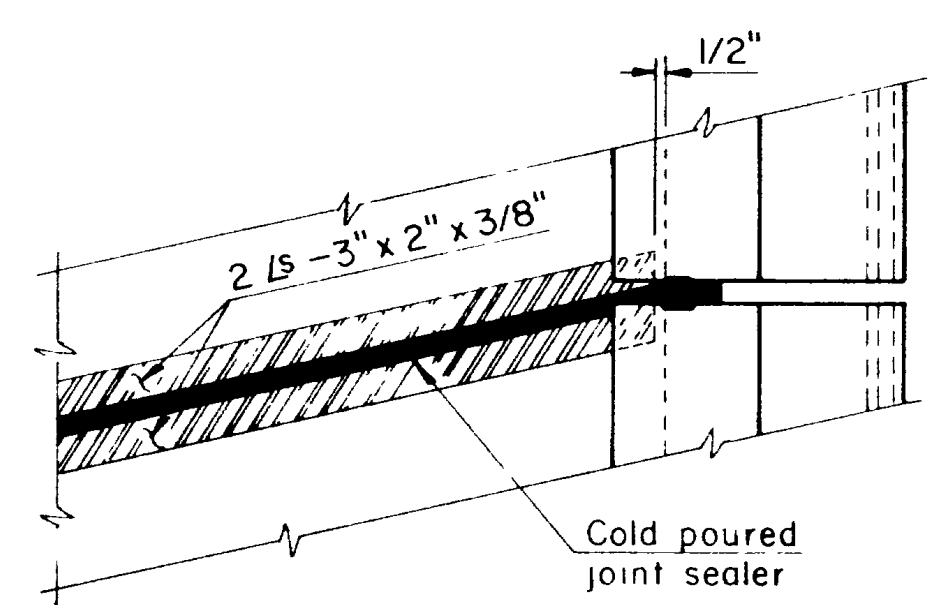


SECTION AT INTERMEDIATE DIAPHRAGMS
(Normal to Diaphragm)

Note:
For reinf steel in diaphragm for NBL, See Sht #18



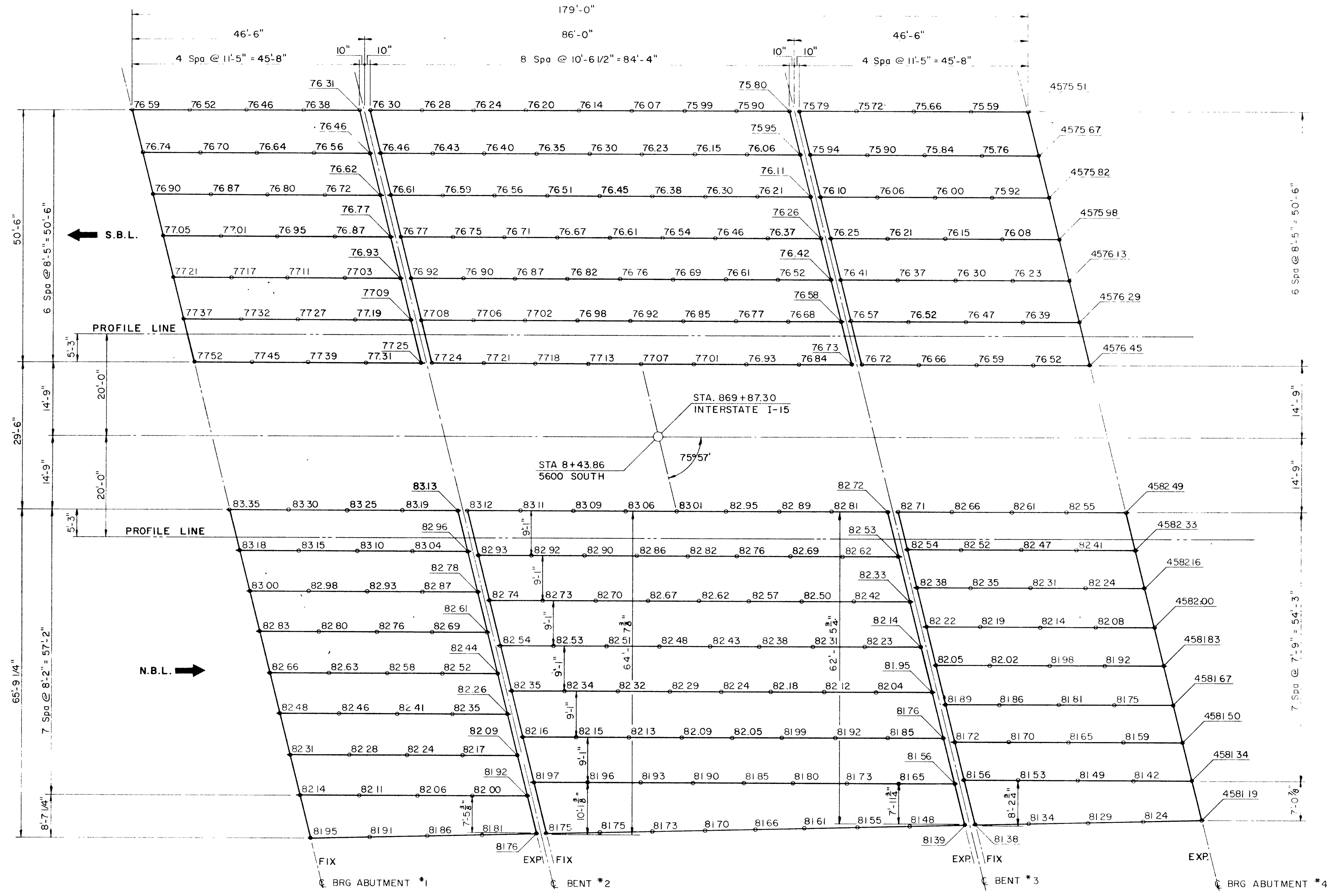
DETAIL "C"
(Alternate Bar)



PLAN AT JOINT

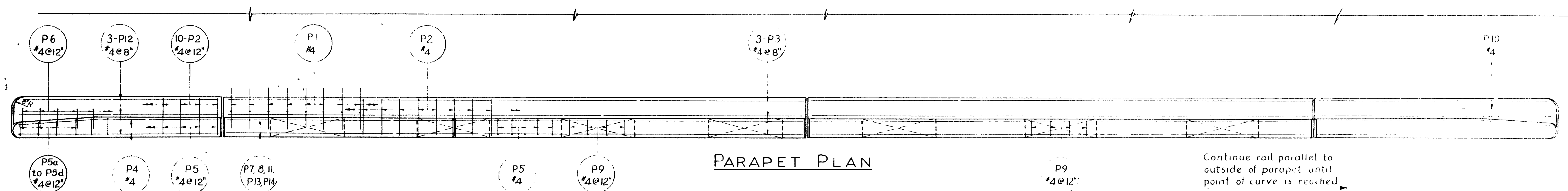
UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION			
DAVIS- WEBER Co. LINE To 4400 So. 5600 SOUTH INTERCHANGE DIAPHRAGM AND JOINT DETAILS			
DESIGNED BY: MW	CHECKED BY: HJS	DATE: 1-15-8(20)334	
DRAWN BY: AEF	CHECKED BY: HJS	FIG. NO. 11	
QUANTITIES BY: HJS	REVIEWED BY: HGE	869 + 87 30	
APPROVAL: [Signature]	DATE: []	WEBER	
BR NO:	F-66	19 26	

REVISOR BY: DATE
 REVISOR BY: DATE
 REVISOR BY: DATE
 REVISOR BY: DATE

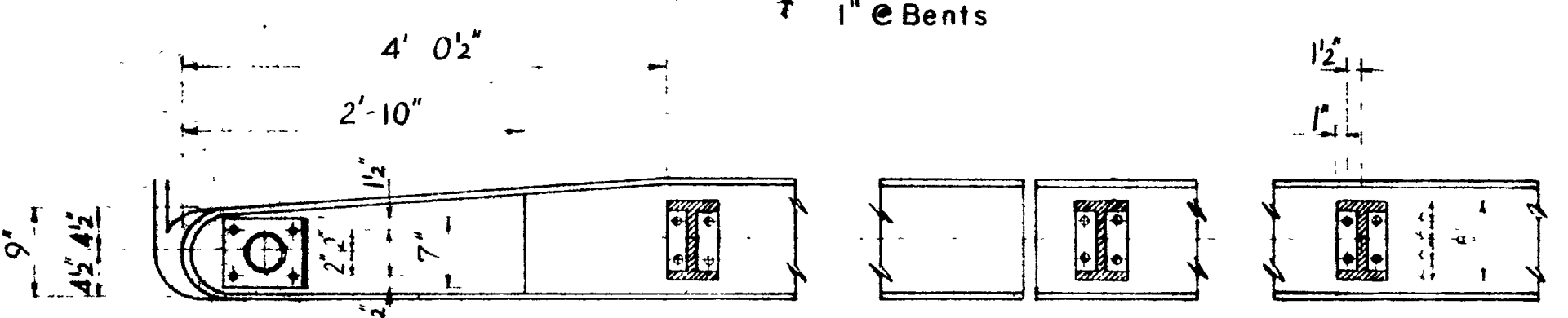
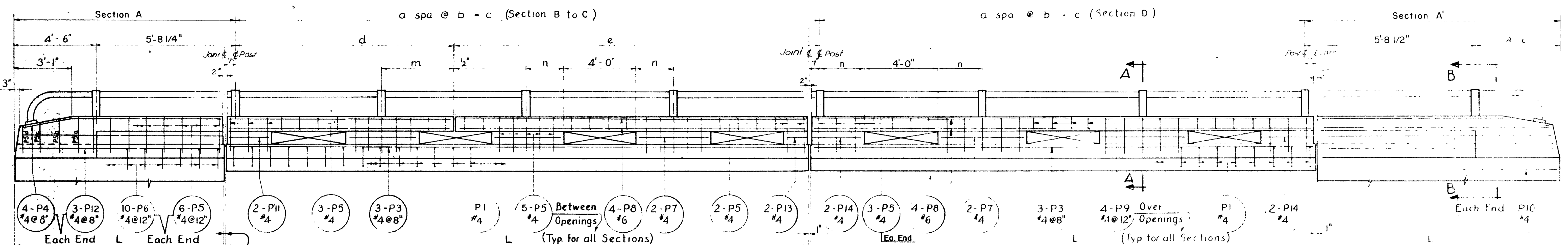
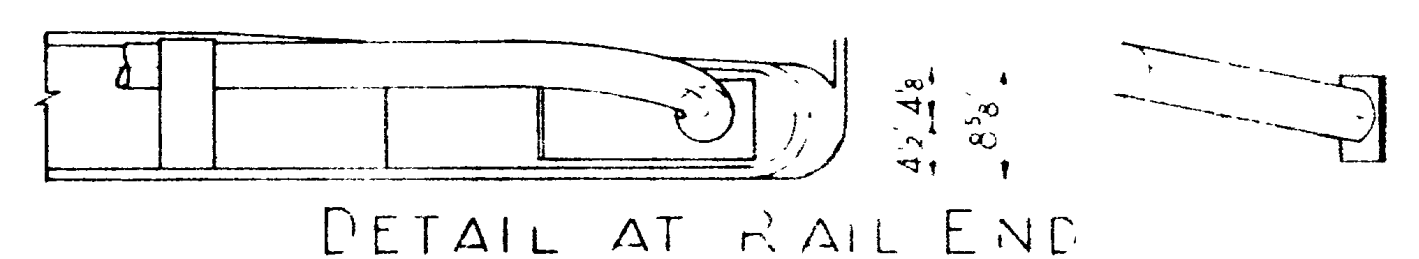


NOTE
 ELEVATIONS SHOWN ARE FINISHED GRADE + 25 SLAB DEAD LOAD DEFLECTION

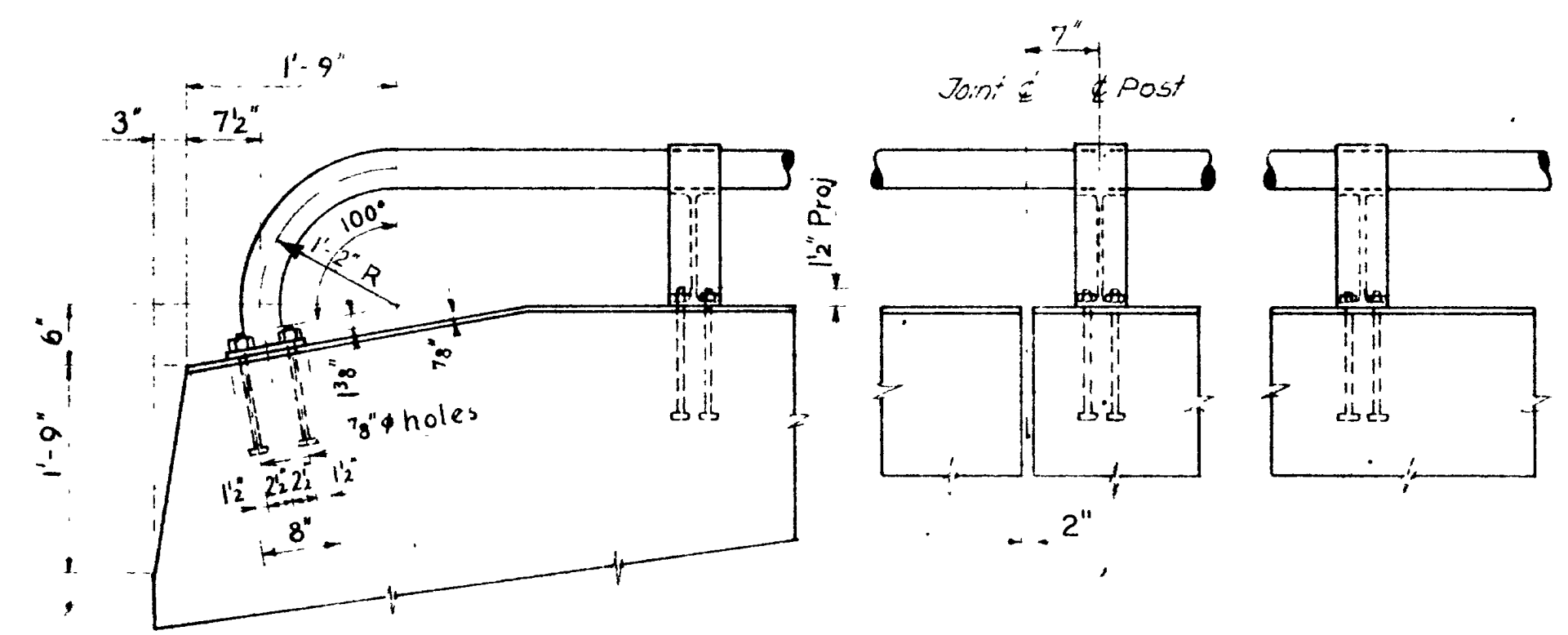
UTAH STATE DEPARTMENT OF HIGHWAYS SALT LAKE CITY, UTAH STRUCTURES DIVISION		
DAVIS-WEBER Co. LINE TO 4400 So. 5600 SOUTH INTERCHANGE SCREED ELEVATIONS		
DESIGNED BY MW	CHECKED BY mjc	1-15-8/201334
DRAWN BY AEF	CHECKED BY HJS	PROJECT NUMBER
QUANTITIES BY FRA	CHECKED BY HJS	869+8730
APPROVAL RECOMMENDED BY mjc	STATIC	WEBER
APPROVED	DATE	COUNTY
BR NO.	DIR. F-66	20th 26



Continue rail parallel to outside of parapet until point of curve is reached

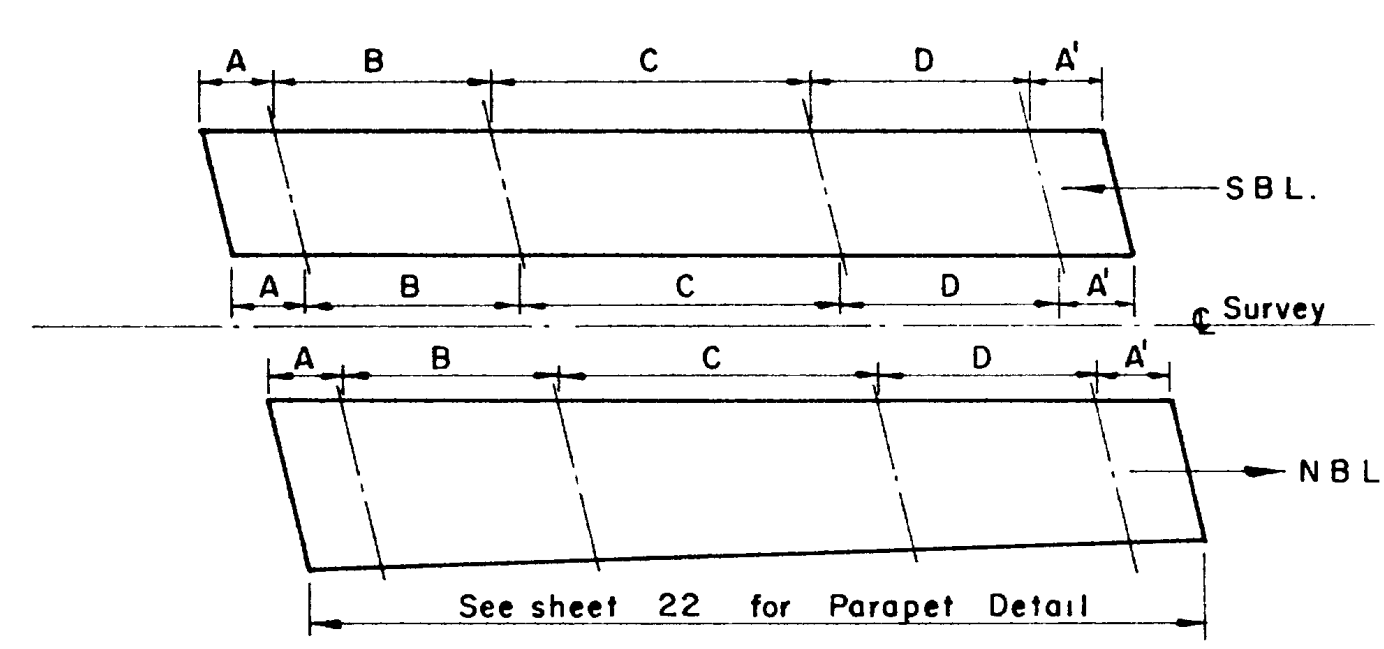


RAILING PLAN

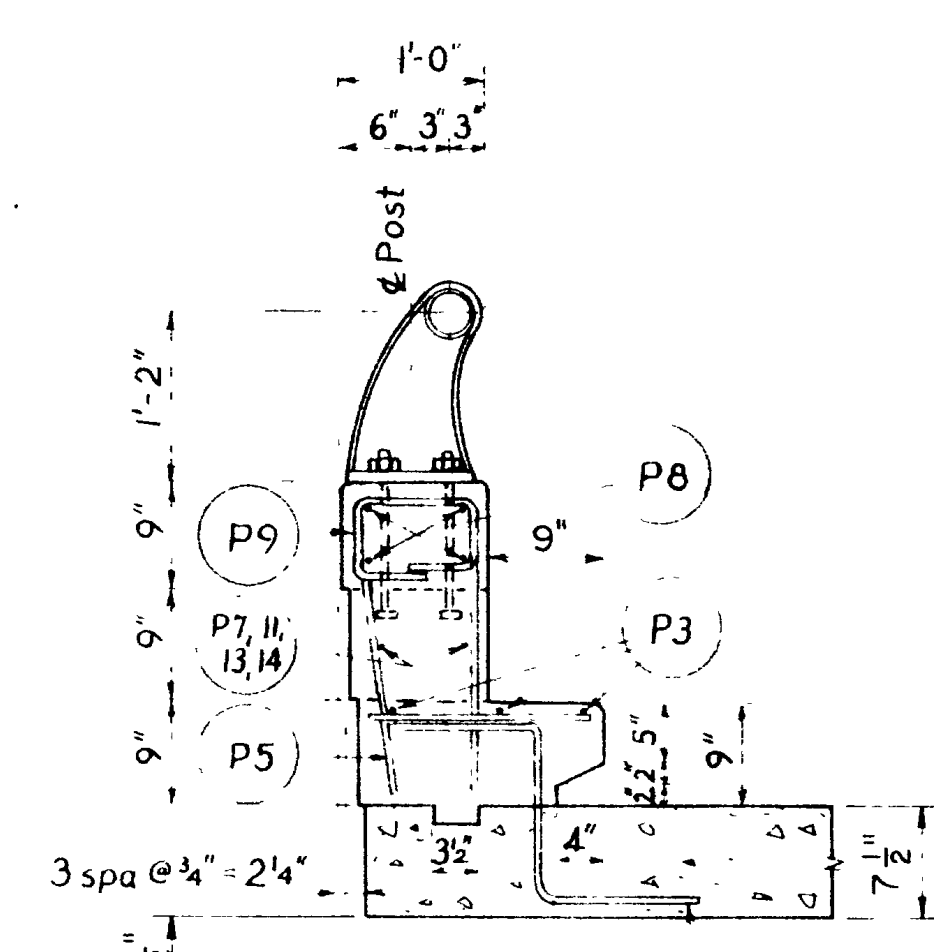


RAILING ELEVATION

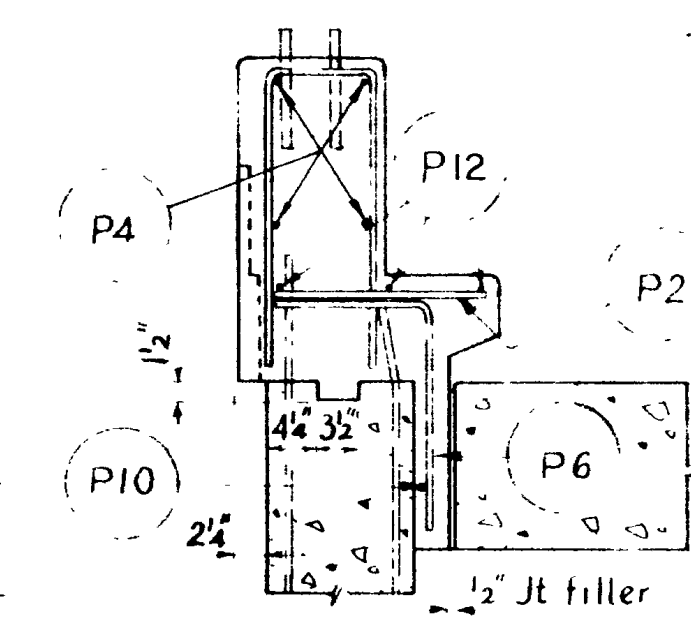
RAILING DETAILS



SCHEMATIC LAYOUT PLAN VIEW



SECTION A-A



SECTION B-B

SCHEDULE

Section	Total No. of Sections	a	b	c	d	e	m	n	i	P1 No.	P2 No.	P5 No.
A	3	-	-	-	-	-	-	-	9'-7"	30	18	12
A'	3	-	-	-	-	-	-	-	9'-7"	30	18	12
B	3	6	7'-11 7/8"	47'-8 3/8"	-	-	-	1'-11 3/8"	47'-7 7/8"	158	158	90
C	3	11	7'-9 3/4"	86'-0"	42'-11 3/4"	42'-11 3/4"	3'-10 3/8"	1'-10 7/8"	85'-11"	268	268	165
D	3	6	7'-9 3/8"	46'-7 1/8"	-	-	-	1'-10 9/16"	47'-7 7/8"	154	154	93

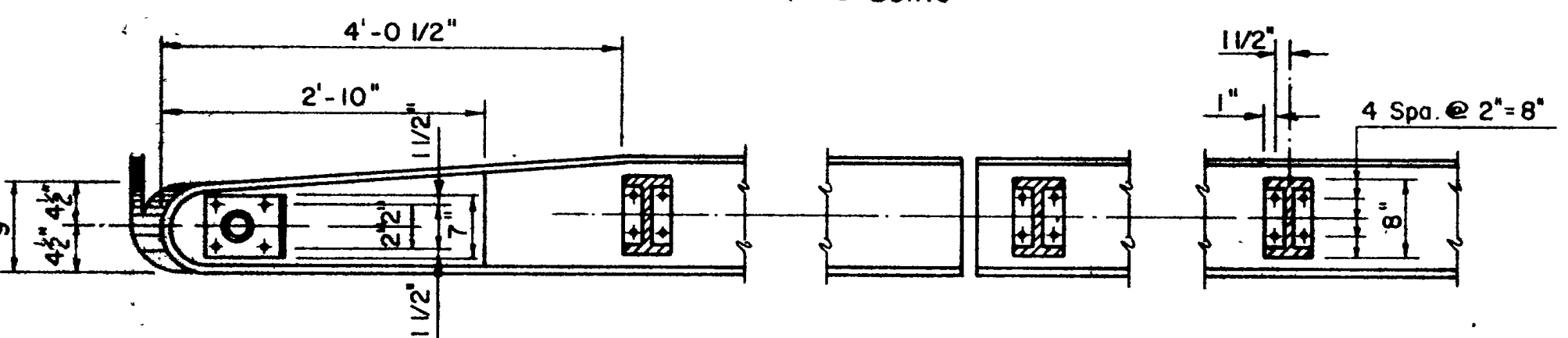
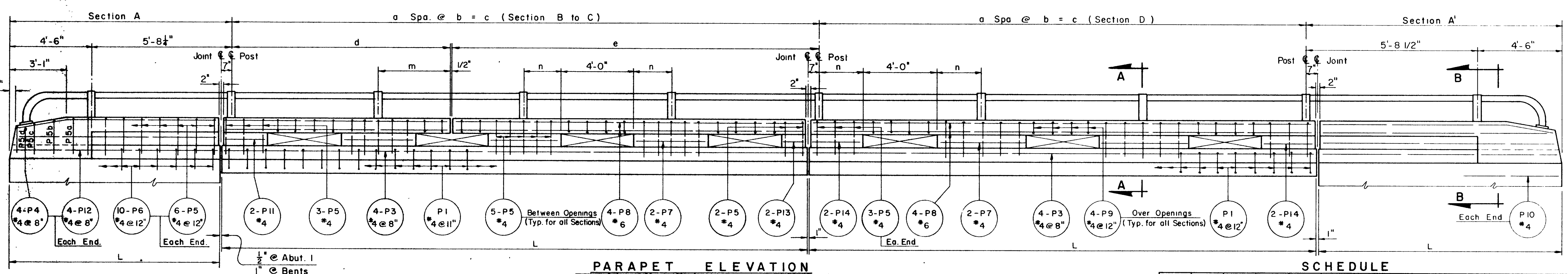
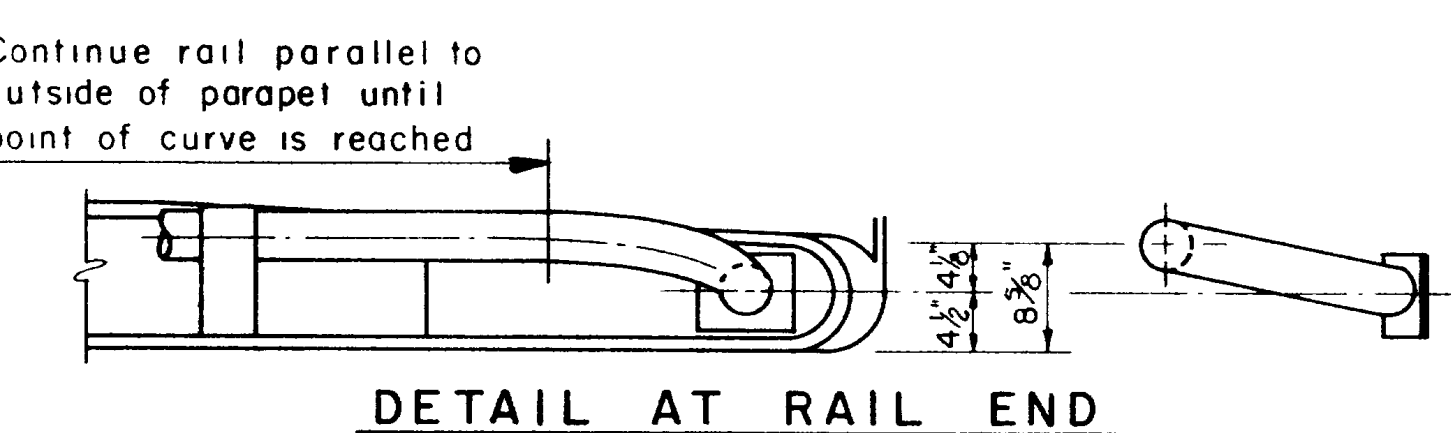
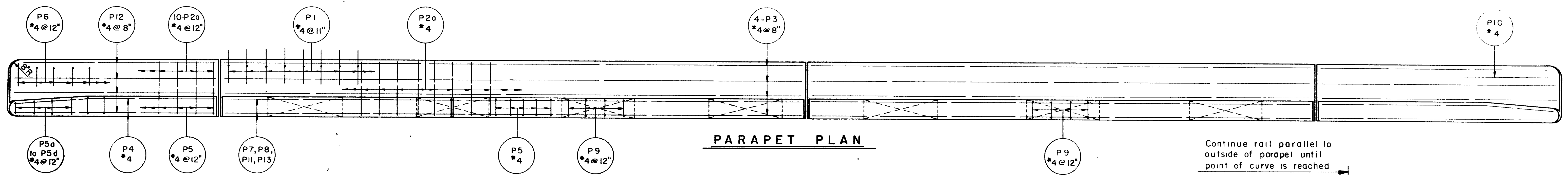
NOTE: see Note 4 for P1, P2, P5 details.

NOTE

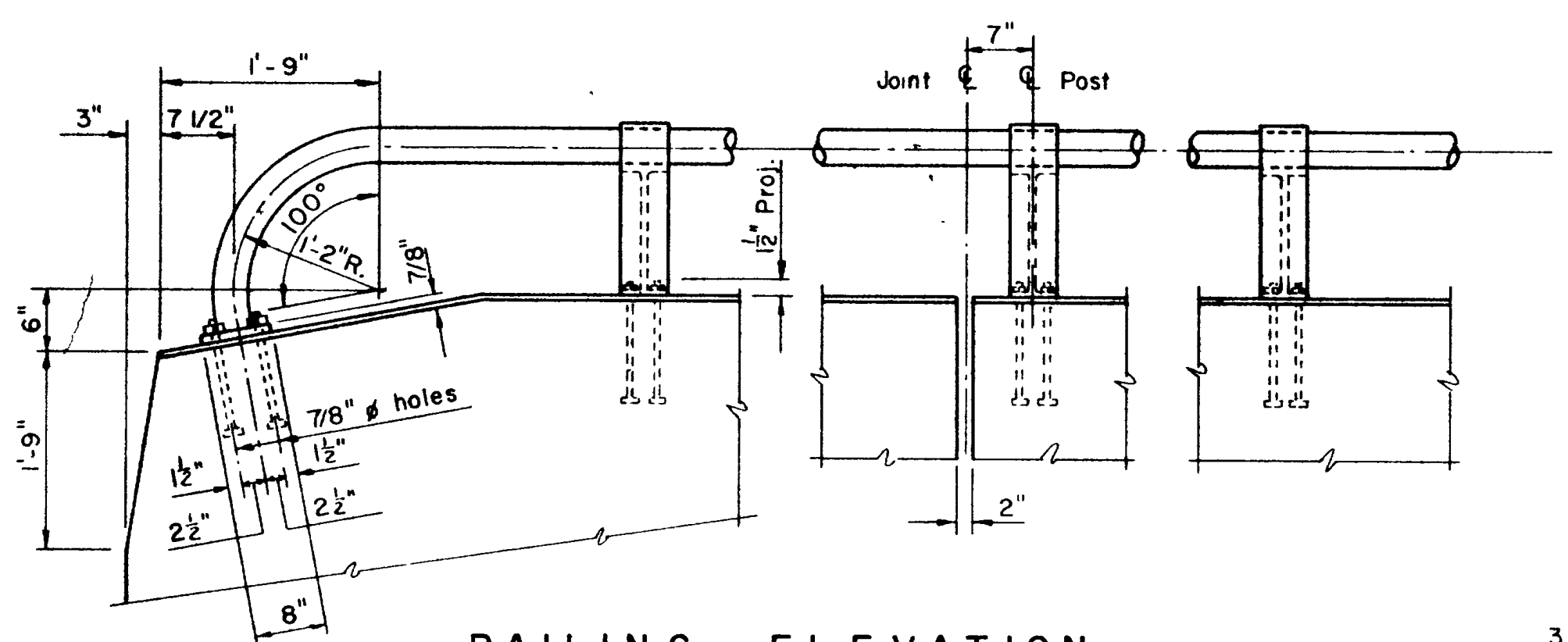
- In reference to the P3 P7 (P11, P13, P14) P8 and P10 bars, P3A bars go in parapet A, P8B in parapet B etc. When there are joints over the openings, the P8 bars may be further divided into P8Ad, P8Be etc. and are placed as shown in the elevation.
- See Sheet MR-1 for Details of Rail Installation.
- Quantity shown in Schedule is Total for all Sections.
- Spacing of P1 and P2 bars shall conform with alternating transverse deck reinforcing. See deck plans.

UTAH STATE DEPARTMENT OF HIGHWAYS
 STRUCTURE DIVISION
 DAVIS-WEBER Co. LINE TO 4400 So.
 5600 SOUTH INTERCHANGE
 PARAPET & RAIL DETAIL
 MW HGE AEF HJS HGE
 HJS HGE
 1-15-8(20)334
 869+8730
 WEBER

REVISIONS
 1. DATE
 2. DATE
 3. DATE

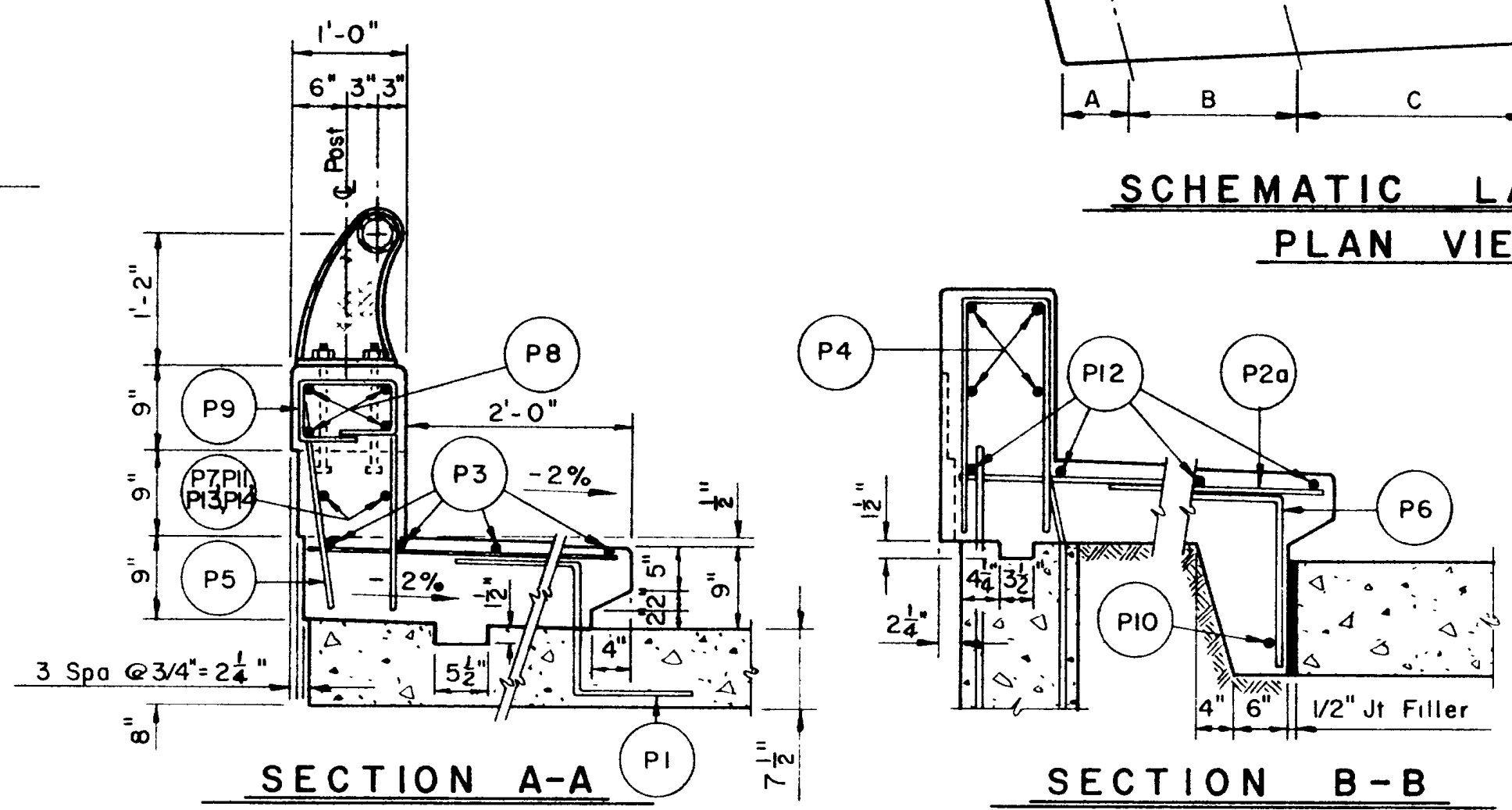


RAILING PLAN



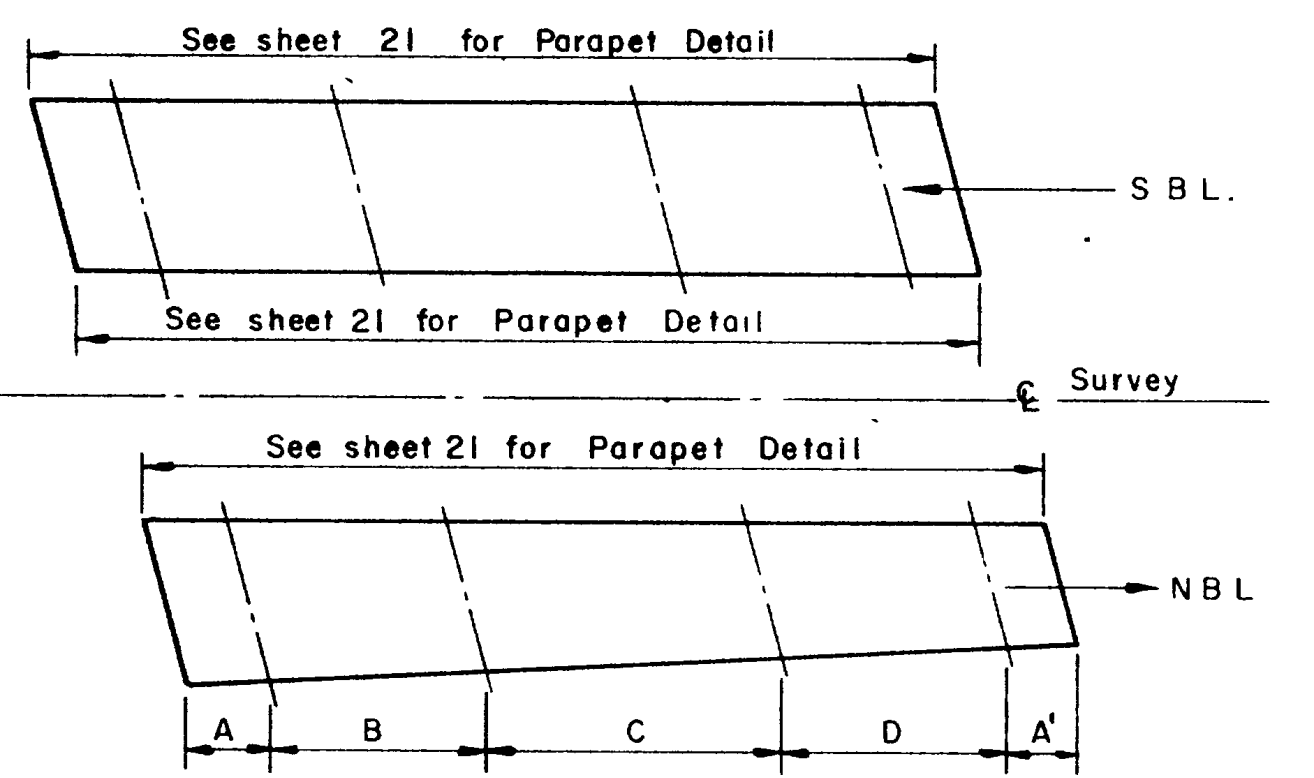
RAILING ELEVATION

RAILING DETAILS



SECTION A-A

SECTION B-B



SCHEMATIC LAYOUT PLAN VIEW

SCHEDULE

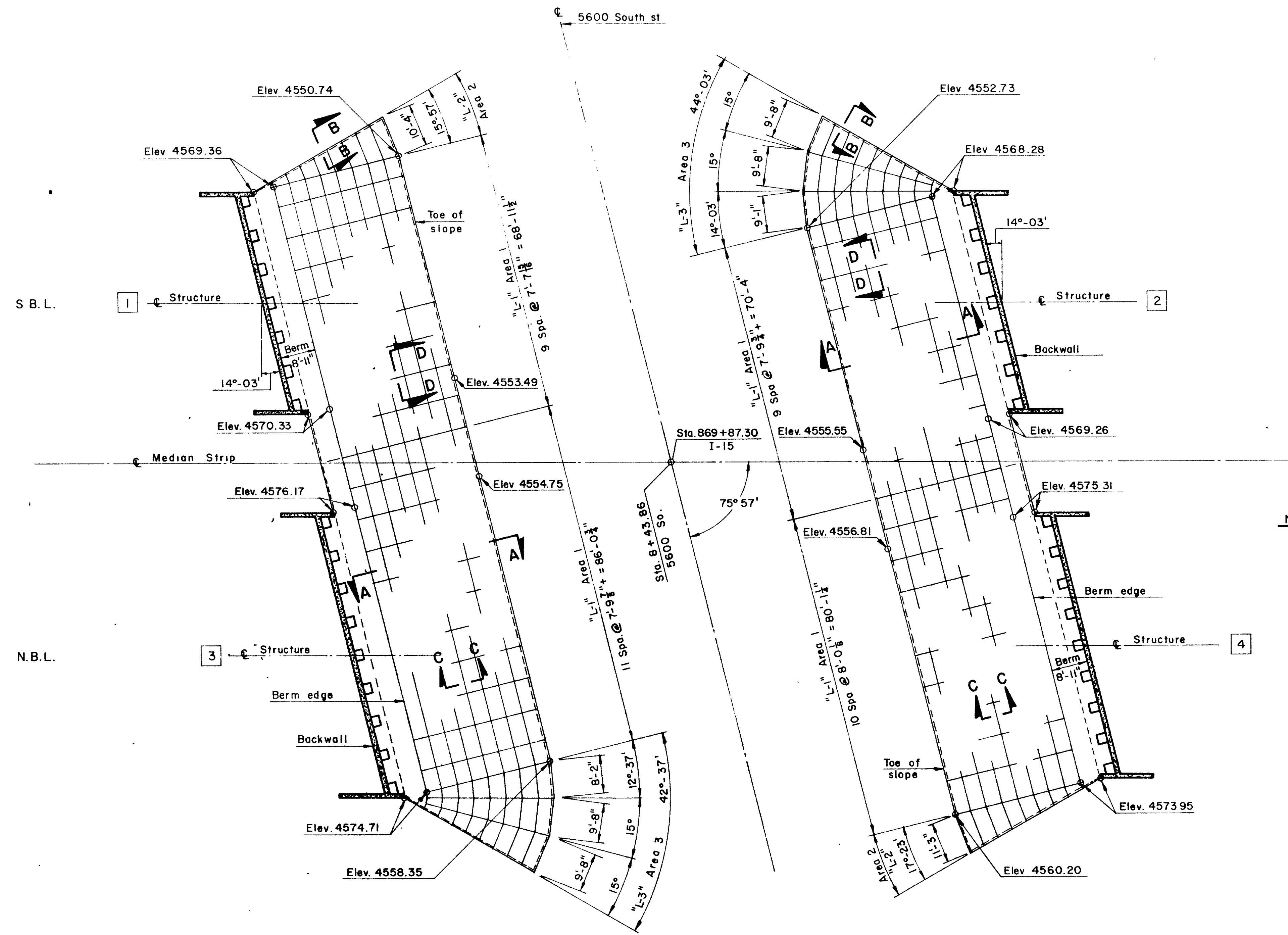
Parapet Section	Total No. of Sections	Dimensions								P1		P2a		P5	
		a	b	c	d	e	m	n	L	No	Spa	No	Spa	No	Spa
A	1	-	-	-	-	-	-	-	9'-7"	-	10	12"	6	12"	
A'	1	-	-	-	-	-	-	-	9'-7"	-	10	12"	6	12"	
B	1	6	7'-10 7/8"	47'-5 1/4"	-	-	-	1'-11 7/8"	47'-4 1/2"	52	11"	52	11"	30	8"
C	1	11	7'-9 1/4"	85'-6"	42'-8 3/8"	42'-8 3/8"	3'-10 3/8"	1'-10 3/8"	85'-5"	94	11"	94	11"	55	8"
D	1	6	7'-8 3/8"	46'-3 1/2"	-	-	-	1'-10 1/4"	47'-4 1/2"	48	12"	48	12"	31	8"

NOTE:
 1. In reference to the P3, P7 (P11, P13, P14), P8 and P10 bars, P3A bars go in parapet A, P8B in parapet B etc. When there are joints over the openings, the P8 bars may be further divided into P8Ad, P8Be, etc., and are placed as shown in the elevation.
 2. See sheet MR-1 for Details of Rail Installation.

UTAH STATE DEPARTMENT OF HIGHWAYS
 SALT LAKE CITY, UTAH
 STRUCTURES DIVISION
 DAVIS-WEBER Co. LINE To 4400 So.
 5600 SOUTH INTERCHANGE
 PARAPET & RAIL DETAIL

DESIGNED BY MW	CHECKED BY mjc	1-15-8(20)334
DRAWN BY AEF	CHECKED BY HJS	PROJECT NUMBER
QUANTITIES BY HJS	CHECKED BY HGE	869+87 30
APPROVAL	GRD. P. LEADER	STATION
APPROVED	DATE	WEBER COUNTY

No.	BY	DATE	REMARKS
REVISIONS			

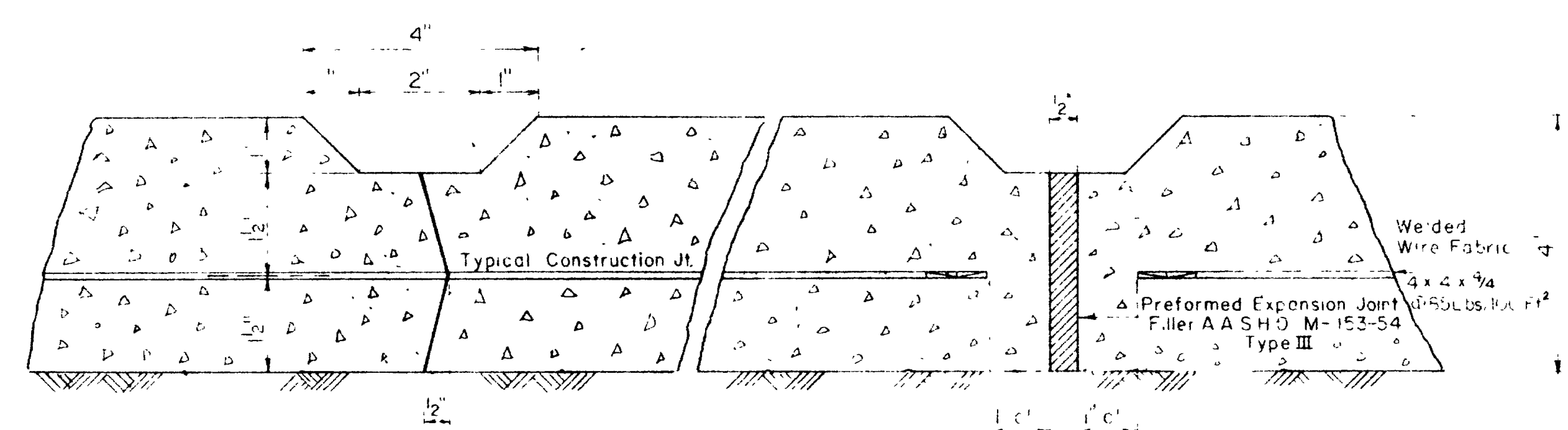


NOTE:

Elevations shown @ front edge of berm, toe of slope and front of bridge pedestals are given to top of concrete slope protection.

1	37'-3"	3088 ^{sq}	or	218 ^{sq}	367
2	35'-9"	3041 ^{sq}		578 ^{sq}	402
3	36'-8"	3,811 ^{sq}		564 ^{sq}	486
4	35'-5"	3,445 ^{sq}		223 ^{sq}	408
					1,663

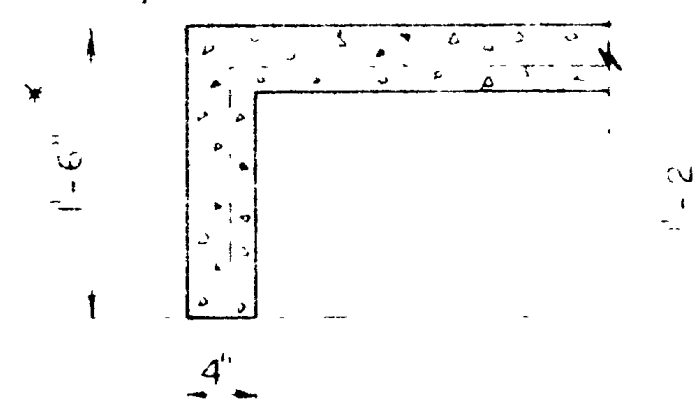
$$= (\text{Berm Width} + \text{Slope Length}) \times \text{Toe Length} \times 2$$



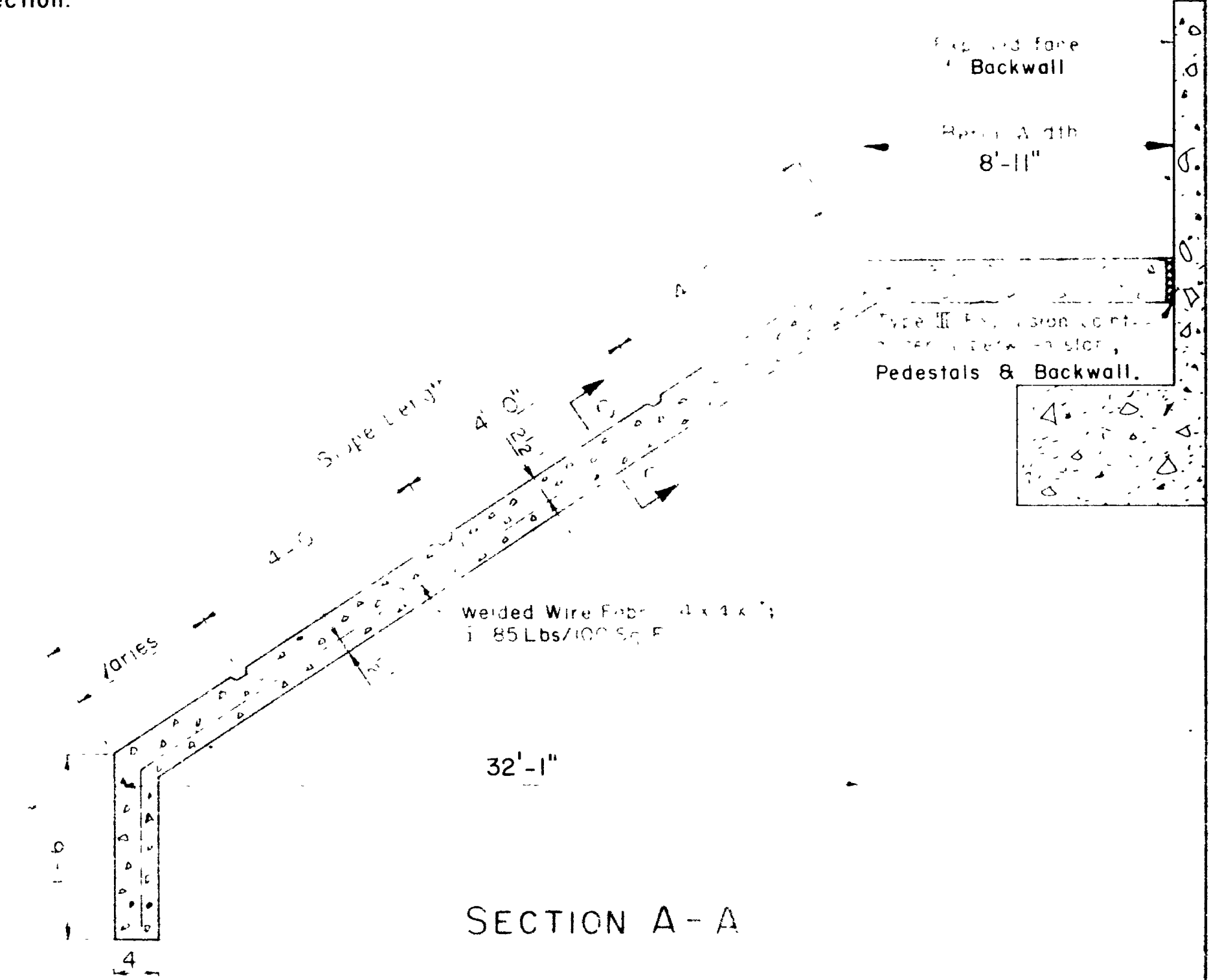
Locate Construction Joints as needed in placing concrete slab. Locate vertical Expansion Joints at 40'-0" max spacing in slab.

SECTION C-C

SECTION D-D



SECTION B-B
(Cut-off wall Detail)



SECTION A-A

NOTE: Cut-off wall is not included in exposed finished surface.

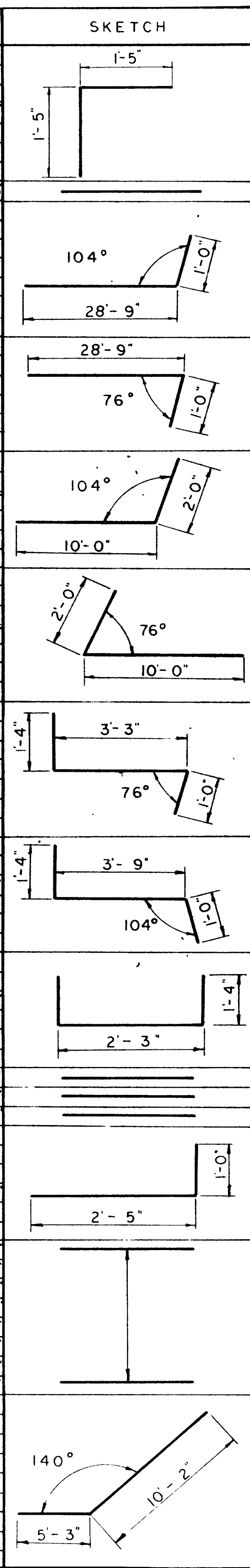
UTAH STATE DEPARTMENT OF HIGHWAYS
STRUCTURES DIVISION
DAVIS-WEBER Co. LINE To 4400 So
5600 SOUTH INTERCHANGE
REINFORCING STEEL FABRICATOR

MW	mge	1-15-8(20)334
AEF	H.G.E.	
HJS	H.G.E.	869 + 87.30

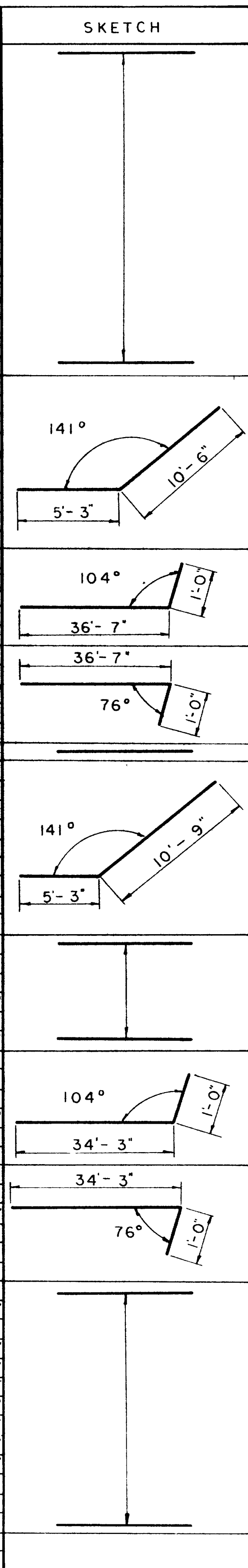
my Chittam
WEBER
F-66 23 26

REVISIONS: DATE, REVISION BY, DATE, REVISION BY, DATE, REVISION BY, DATE, REVISION BY

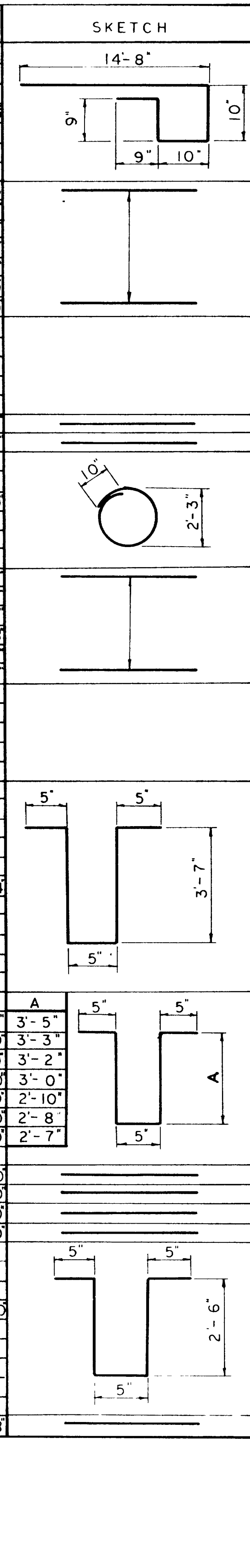
MARK	LOCATION	SIZE NO.	LENGTH	NO BARS	TOTAL LENGTH
A1	ABUTMENTS	4	2'-10"	160	453'-4"
A2		5	8'-1"	502	4057'-0"
A3		4	29'-9"	24	714'-0"
A4		4	29'-9"	24	714'-0"
A5		4	12'-0"	32	384'-0"
A6		4	12'-0"	32	384'-0"
A7		4	5'-7"	14	78'-2"
A8		4	6'-1"	14	85'-2"
A9		4	4'-11"	120	590'-0"
A10		6	4'-4"	32	138'-8"
A11		6	5'-7"	120	670'-0"
A12		8	1'-10"	16	29'-4"
A13		4	3'-5"	268	915'-8"
A14		5	9'-3"	16	148'-0"
A15		7	13'-1"	6	78'-6"
A16		5	12'-2"	120	73'-0"
A17		1	11'-0"	1	66'-0"
A18		1	9'-10"	1	59'-0"
A19		1	8'-8"	1	52'-0"
A20		1	7'-6"	1	45'-0"
A21		5	6'-3"	6	37'-6"
A22	ABUTMENTS	5	15'-5"	8	123'-4"



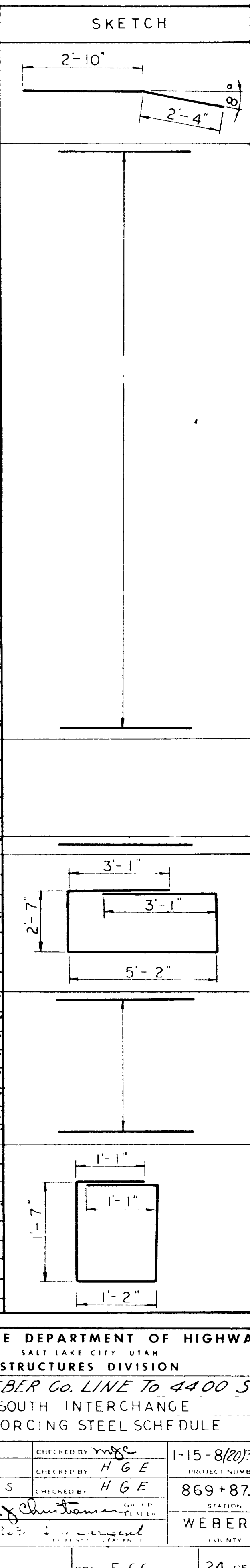
MARK	LOCATION	SIZE NO.	LENGTH	NO BARS	TOTAL LENGTH
A23	ABUTMENTS	4	8'-0"	64	512'-0"
A24		1	10'-2"	32	325'-4"
A25		1	8'-5"	16	134'-8"
A26		1	7'-6"	1	120'-0"
A27		1	6'-8"	1	106'-8"
A28		1	5'-9"	1	92'-0"
A29		1	5'-0"	1	80'-0"
A30		1	4'-1"	1	65'-4"
A31		1	3'-3"	1	52'-0"
A32		4	2'-6"	16	40'-0"
A33		7	13'-5"	6	80'-6"
A34		5	12'-6"	4	50'-0"
A35		1	11'-3"	1	45'-0"
A36		1	10'-0"	1	40'-0"
A37		1	8'-9"	1	35'-0"
A38		1	7'-6"	1	30'-0"
A39		5	6'-3"	4	25'-0"
A40		5	15'-9"	6	94'-6"
A41		4	37'-7"	12	451'-0"
A42		4	37'-7"	12	451'-0"
A43		7	13'-8"	2	27'-4"
A44		5	16'-0"	2	32'-0"
A45		5	12'-8"	2	25'-4"
A46		1	11'-5"	1	22'-10"
A47		1	10'-2"	1	20'-4"
A48		1	8'-11"	1	17'-10"
A49		1	7'-8"	1	15'-4"
A50		5	6'-5"	2	12'-10"
A51		4	35'-3"	12	423'-0"
A52		4	35'-3"	12	423'-0"
A53		5	12'-10"	2	25'-8"
A54		1	11'-11"	1	23'-10"
A55		1	10'-10"	1	21'-8"
A56		1	9'-9"	1	19'-6"
A57		1	8'-8"	1	17'-4"
A58		1	7'-7"	1	15'-2"
A59		1	6'-6"	1	13'-0"
A60		1	12'-5"	1	24'-10"
A61		1	11'-2"	1	22'-4"
A62		1	9'-11"	1	19'-10"
A63		1	8'-8"	1	17'-4"
A64		1	7'-5"	1	14'-10"
A65	ABUTMENTS	5	6'-2"	2	12'-4"



MARK	LOCATION	SIZE NO.	LENGTH	NO BARS	TOTAL LENGTH
AS1	APPROACH-SLAB	4	17'-10"	237	4226'-6"
AS2		7	14'-8"	313	4590'-8"
AS3		4	27'-7"	120	3310'-0"
AS4		7	27'-10"	12	334'-0"
AS5		7	31'-6"	6	189'-0"
AS6		7	35'-3"	6	211'-6"
AS7		4	31'-3"	60	1875'-0"
AS8	APPROACH SLAB	4	35'-0"	60	2100'-0"
C1	COLUMNS	11	19'-1"	42	801'-6"
C2		11	17'-7"	42	738'-6"
C3		4	7'-11"	242	1915'-10"
C4		9	22'-4"	8	178'-8"
C5		1	20'-10"	16	333'-4"
C6		1	19'-4"	16	309'-4"
C7		1	17'-10"	14	249'-8"
C8		1	18'-2"	8	145'-4"
C9	COLUMNS	9	16'-10"	14	235'-8"
D1	DIAPHRAGMS	4	8'-5"	392	3299'-4"
D2		4	8'-1"	24	194'-0"
D3		1	7'-9"	1	186'-0"
D4		1	7'-7"	1	182'-0"
D5		1	7'-3"	1	174'-0"
D6		1	6'-11"	1	166'-0"
D7		1	6'-7"	1	158'-0"
D8		4	6'-5"	24	154'-0"
D9		6	7'-0"	72	504'-0"
D10		4	7'-0"	72	504'-0"
D11		6	6'-7"	48	316'-0"
D12		4	6'-7"	48	316'-0"
D13		4	6'-3"	480	3000'-0"
D14	DIAPHRAGMS	9	5'-10"	44	256'-8"



MARK	LOCATION	SIZE NO.	LENGTH	NO BARS	TOTAL LENGTH
D15	DIAPHRAGMS	9	5'-2"	24	124'-0"
D16		9	5'-2"	48	248'-0"
D17		4	7'-0"	2	14'-0"
D18		1	6'-6"	1	13'-0"
D19		1	5'-10"	1	11'-8"
D20		1	8'-2"	1	16'-4"
D21		1	7'-7"	1	15'-2"
D22		1	6'-8"	1	13'-4"
D23		1	6'-0"	1	12'-0"
D24		1	6'-8"	1	13'-4"
D25		1	6'-1"	1	12'-2"
D26		4	5'-6"	1	11'-0"
D27		6	7'-0"	1	14'-0"
D28		1	6'-6"	1	13'-0"
D29		1	5'-10"	1	11'-8"
D30		1	8'-2"	1	16'-4"
D31		1	7'-7"	1	15'-2"
D32		1	6'-8"	1	13'-4"
D33		1	6'-0"	1	12'-0"
D34		1	6'-8"	1	13'-4"
D35		1	6'-1"	1	12'-2"
D36		6	5'-6"	2	11'-0"
D37		4	6'-7"	6	39'-6"
D38		4	6'-11"	36	249'-0"
D39		6	6'-7"	6	39'-6"
D40		6	6'-11"	36	249'-0"
D41		4	7'-1"	48	340'-0"
D42		6	7'-1"	48	340'-0"
D43		4	6'-2"	6	37'-0"
D44		6	6'-2"	6	37'-0"
D45		4	6'-5"	36	231'-0"
D46	DIAPHRAGMS	6	6'-5"	36	231'-0"
F1	ABUT FOOTINGS	8	29'-6"	32	944'-0"
F2		4	16'-6"	176	2904'-0"
F3		4	28'-8"	8	229'-4"
F4		8	37'-4"	16	597'-4"
F5		4	36'-6"	4	146'-0"
F6		8	35'-1"	16	561'-4"
F7	ABUT FOOTINGS	4	34'-3"	4	137'-0"
F8	BENT FOOTINGS	6	6'-0"	312	1872'-0"
F9		6	6'-9"	60	405'-0"
F10		5	18'-4"	24	440'-0"
F11	BENT FOOTINGS	4	6'-6"	125	812'-6"



UTAH STATE DEPARTMENT OF HIGHWAYS
SALT LAKE CITY, UTAH
STRUCTURES DIVISION
DAVIS-WEBER CO. LINE TO 4400 SO.
5600 SOUTH INTERCHANGE
REINFORCING STEEL SCHEDULE

DESIGNED BY MW CHECKED BY mge
DRAWN BY U.M.S CHECKED BY HGE
QUANTITIES BY HJS CHECKED BY HGE
APPROVAL RECOMMENDED BY [Signature]
APPROVED [Signature]

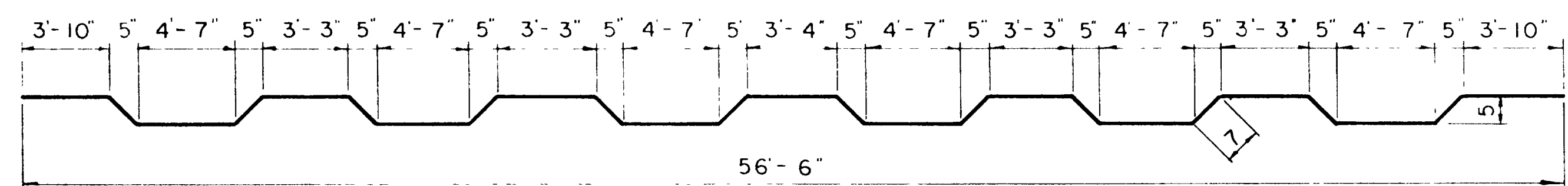
1-15-8(20)334
PROJECT NUMBER
869+8730
STATION
WEBER COUNTY

BR NO. [] DRG. F-66 24 OF 26

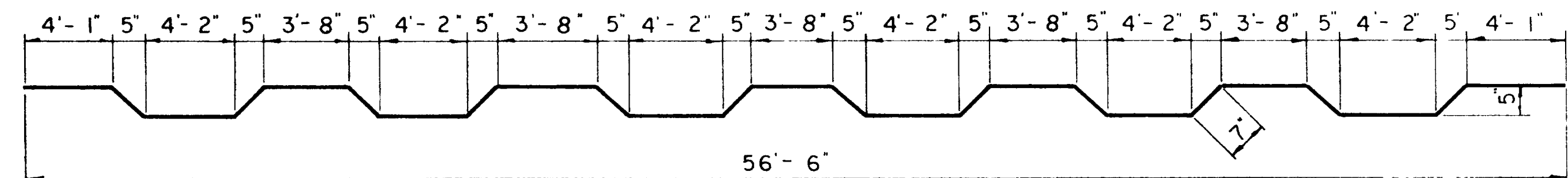
REVISOR BY DATE

MARK	LOCATION	SIZE NO	LENGTH	NO BARS	TOTAL LENGTH	SKETCH
F12	BENT FOOTINGS	11	7'-0"	84	588'-0"	
F13		5	16'-0"	18	288'-0"	
F14		9	6'-4"	76	481'-4"	
F15	BENT FOOTINGS	5	15'-4"	18	276'-0"	
H1	BENTS	8	1'-10"	14	25'-8"	
H2		5	12'-1"	448	5413'-4"	
H3		5	7'-2"	32	229'-4"	
H4		5	3'-3"	40	130'-0"	
H5		4	6'-9"	10	67'-6"	
H6		4	7'-2"	10	71'-8"	
H7		4	7'-6"	10	75'-0"	
H8		11	54'-6"	8	436'-0"	
H9			57'-6"	8	460'-0"	
H10			15'-0"	8	120'-0"	
H11			10'-0"	18	180'-0"	
H12			8'-0"	12	96'-0"	
H13		11	16'-6"	12	198'-0"	
H14		4	54'-6"	4	218'-0"	
H15		5	3'-0"	20	60'-0"	
H16		5	1'-8"	28	46'-8"	
H17		11	12'-0"	28	336'-0"	
H18		11	33'-0"	16	528'-0"	
H19		11	21'-7"	8	172'-8"	
H20		11	22'-7"	8	180'-8"	
H21		4	35'-3"	4	141'-0"	
H22		4	6'-9"	14	94'-6"	
H23		4	7'-0"	14	98'-0"	
H24		4	7'-4"	14	102'-8"	
H25		4	34'-3"	4	137'-0"	
H26		11	21'-7"	8	172'-8"	
H27	BENTS	11	20'-7"	8	164'-8"	
P1	PARAPETS	4	3'-0"	774	2322'-0"	

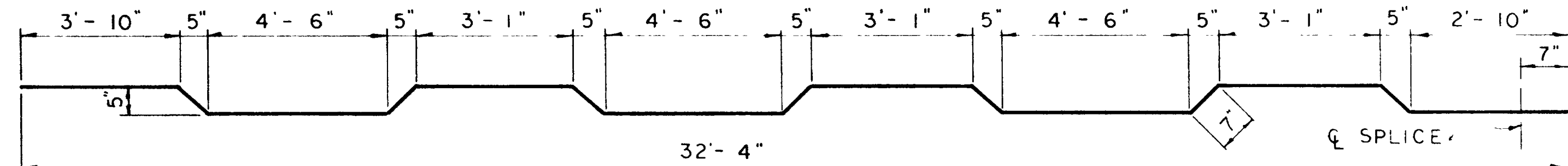
MARK	LOCATION	SIZE NO	LENGTH	NO BARS	TOTAL LENGTH	SKETCH
P2a	PARAPETS	4	2'-7"	214	552'-10"	
P2			1'-4"	640	853'-4"	
P3B			24'-2"	26	628'-4"	
P3C			29'-2"	39	1137'-6"	
P3D			24'-2"	26	628'-4"	
P4			9'-0"	32	288'-0"	
P5			4'-7"	512	2346'-8"	
P5a			4'-7"	8	36'-8"	
P5b			4'-2"	8	33'-4"	
P5c			3'-9"	8	30'-0"	
P5d			3'-4"	8	26'-8"	
P6			2'-6"	80	200'-0"	
P7		4	3'-7"	160	573'-4"	
P8B		6	24'-4"	32	778'-8"	
P8C		6	22'-0"	32	704'-0"	
P8C		6	22'-0"	32	704'-0"	
P8D		6	24'-4"	32	778'-8"	
P9		4	3'-1"	368	1134'-8"	
P10		4	4'-6"	8	36'-0"	
P11			2'-2"	16	34'-8"	
P12			1'-0"	16	16'-0"	
P13			2'-1"	16	33'-4"	
P14	PARAPETS	4	9'-3"	26	240'-6"	
S1	DECKS	5	58'-6"	106	6201'-0"	SEE SKETCH S1
S2			58'-6"	87	5089'-6"	SEE SKETCH S2
S3			56'-6"	380	21470'-0"	
S4			47'-4"	222	10508'-0"	
S5		5	43'-4"	198	8580'-0"	
S6		8	55'-0"	6	330'-0"	
S7		5	33'-6"	104	3484'-0"	SEE SKETCH S7
S8			32'-4"	208	6725'-4"	
S9			11'-7"	68	787'-8"	
S10			11'-0"	72	792'-0"	
S11			10'-5"	68	708'-4"	
S12			24'-3"	524	12707'-0"	
S13			31'-11"	188	6000'-4"	SEE SKETCH S13
S14			30'-11"	376	11624'-8"	
S15			13'-2"	94	1237'-8"	
S16			12'-6"	94	1175'-0"	
S17			11'-9"	94	1104'-6"	
S18			11'-0"	94	1034'-0"	
S19			43'-4"	252	9920'-0"	
S20			56'-0"	4	224'-0"	
S21			32'-0"	96	3072'-0"	SEE SKETCH S21
S22			30'-10"	192	5920'-0"	
S23			11'-3"	64	720'-0"	
S24			10'-8"	64	682'-8"	
S25		5	10'-1"	64	645'-4"	
S26		8	36'-0"	6	216'-0"	
S27		8	35'-4"	6	212'-0"	
S28		4	5'-2"	120	620'-0"	
S29		5	7'-2"	24	172'-0"	
S30			6'-10"	12	82'-0"	
S31			7'-0"	24	168'-0"	
S32			6'-2"	3	18'-6"	
S33			6'-6"	18	117'-0"	
S34	DECKS	5	5'-6"	3	16'-6"	



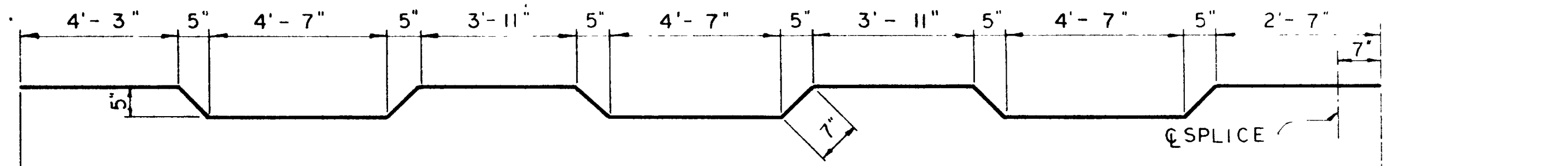
SKETCH S1



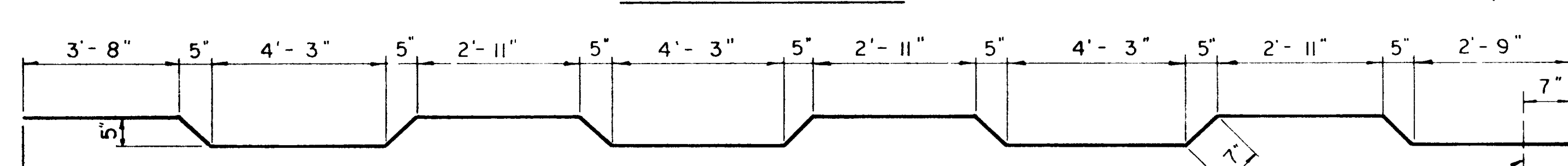
SKETCH S2



SKETCH S7



SKETCH S13



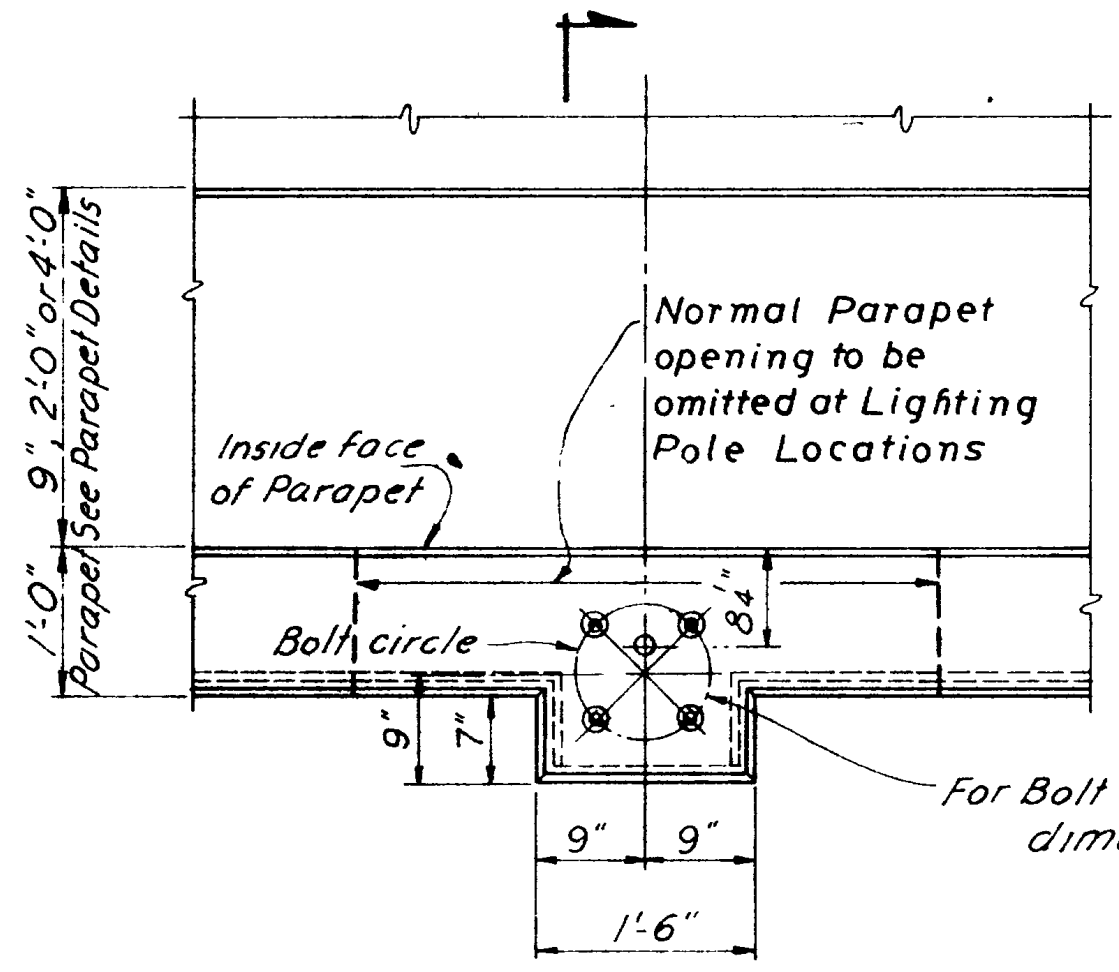
SKETCH S21

47,389'-0" OF #4 BARS @ 0.668# = 31,656 LB
 134,245'-8" OF #5 BARS @ 1.043# = 140,018 LB
 7,899'-6" OF #6 BARS @ 1.502# = 11,865 LB
 5,511'-6" OF #7 BARS @ 2.044# = 11,266 LB
 2,915'-8" OF #8 BARS @ 2.670# = 7,785 LB
 2,562'-0" OF #9 BARS @ 3.400# = 8,711 LB
 5,172'-8" OF #11 BARS @ 5.313# = 27,482 LB
 TOTAL = 238,783 LB

UTAH STATE DEPARTMENT OF HIGHWAYS
 SALT LAKE CITY, UTAH
 STRUCTURES DIVISION
 DAVIS-WEBER Co. LINE TO 4400 So.
 5600 SOUTH INTERCHANGE
 REINFORCING STEEL SCHEDULE

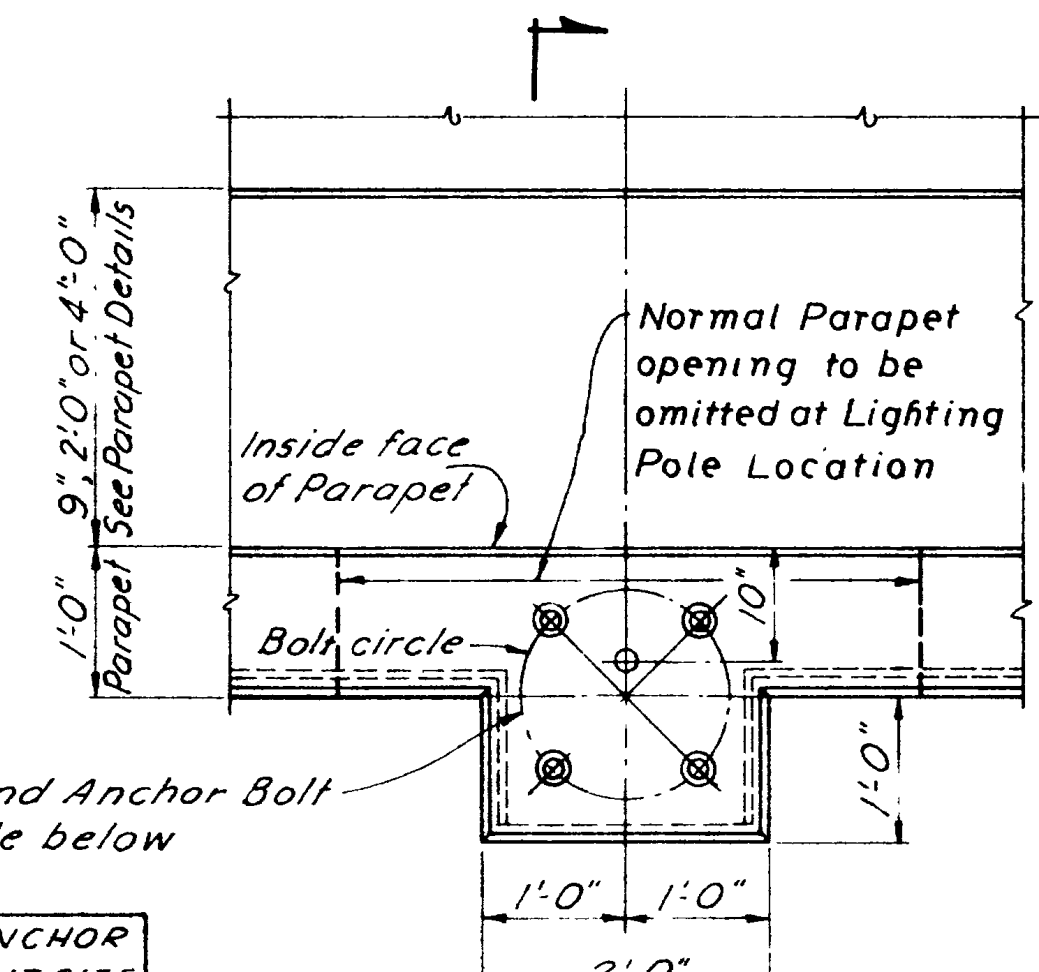
DESIGNED BY MW CHECKED BY MJC 1-15-8/20334
 DRAWN BY UMS CHECKED BY HGE PROJECT NUMBER
 QUANTITIES BY HJS CHECKED BY HGE 869+87.30
 APPROVAL RECOMMENDED BY MJC GROUP LEADER STATION
 APPROVED Sept 4, 1963 DATE D.L. Sargent CHIEF STRUCTURAL ENGINEER WEBER COUNTY

BR NO DRG NO F-66 25 OF 26

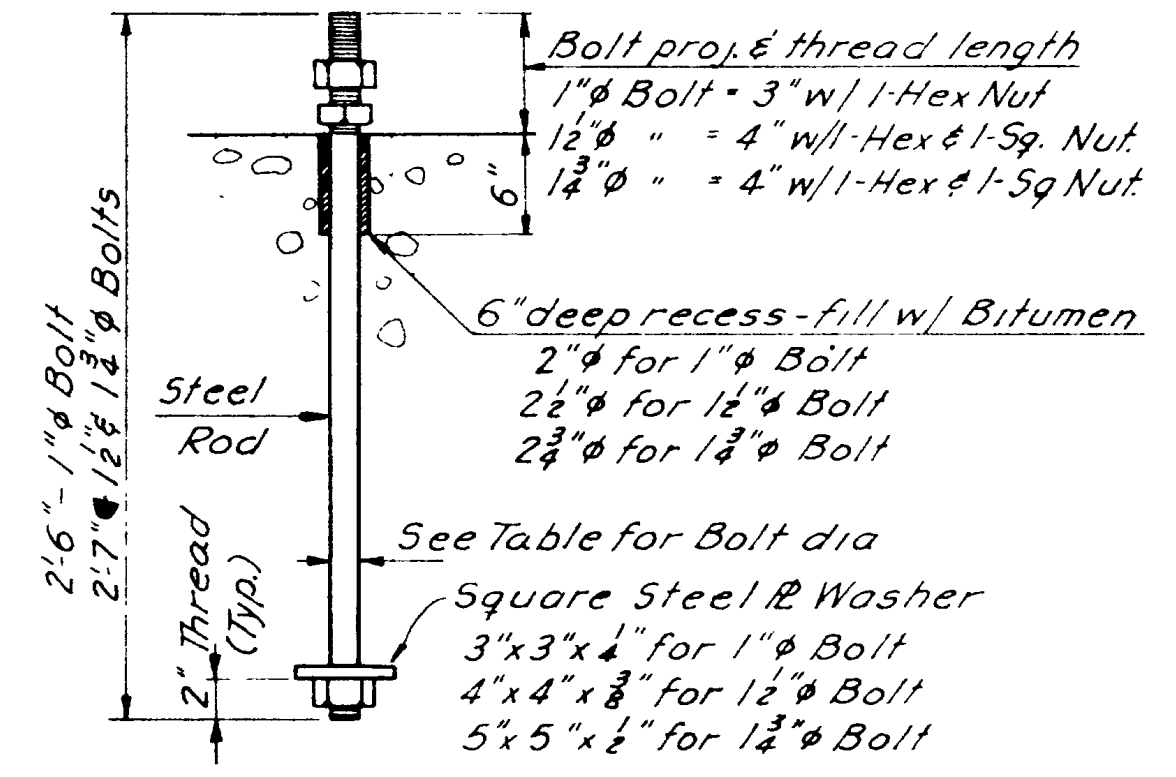


BOLT CIRCLE DIA	ANCHOR BOLT SIZE
11"	1"φ
15"	1 1/2"φ
17 1/2"	1 3/4"φ

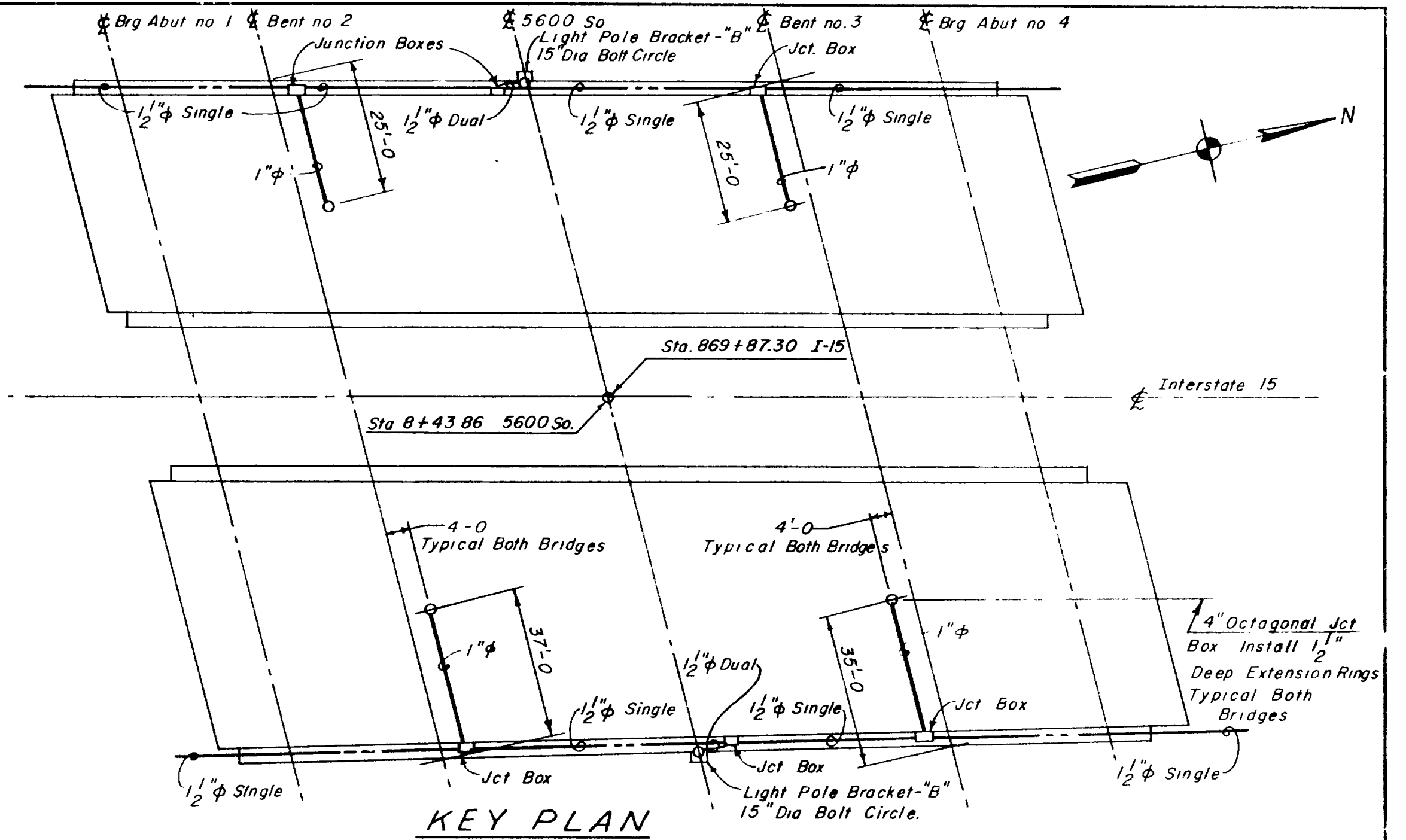
PLAN (BRACKET A)



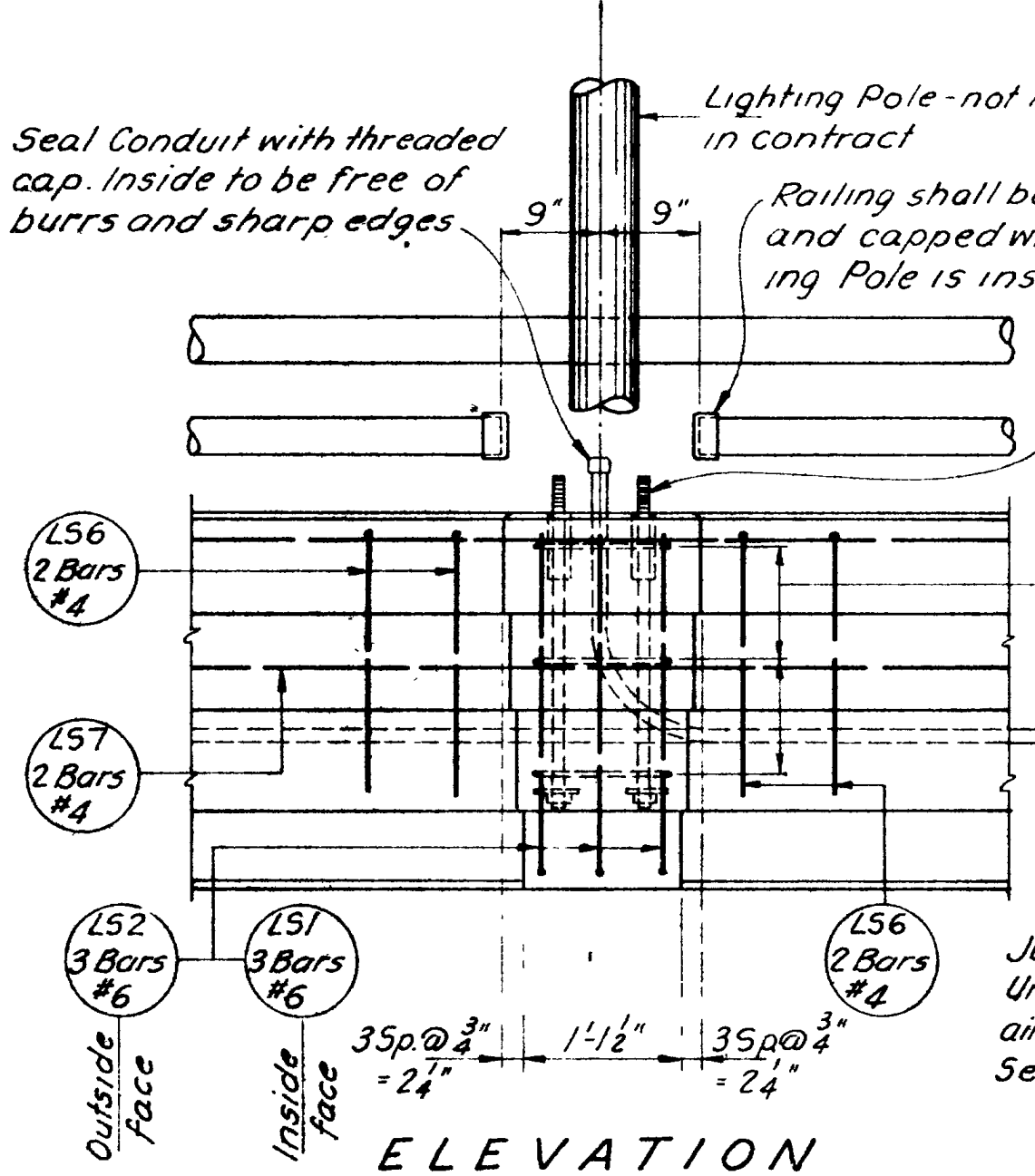
PLAN (BRACKET B)



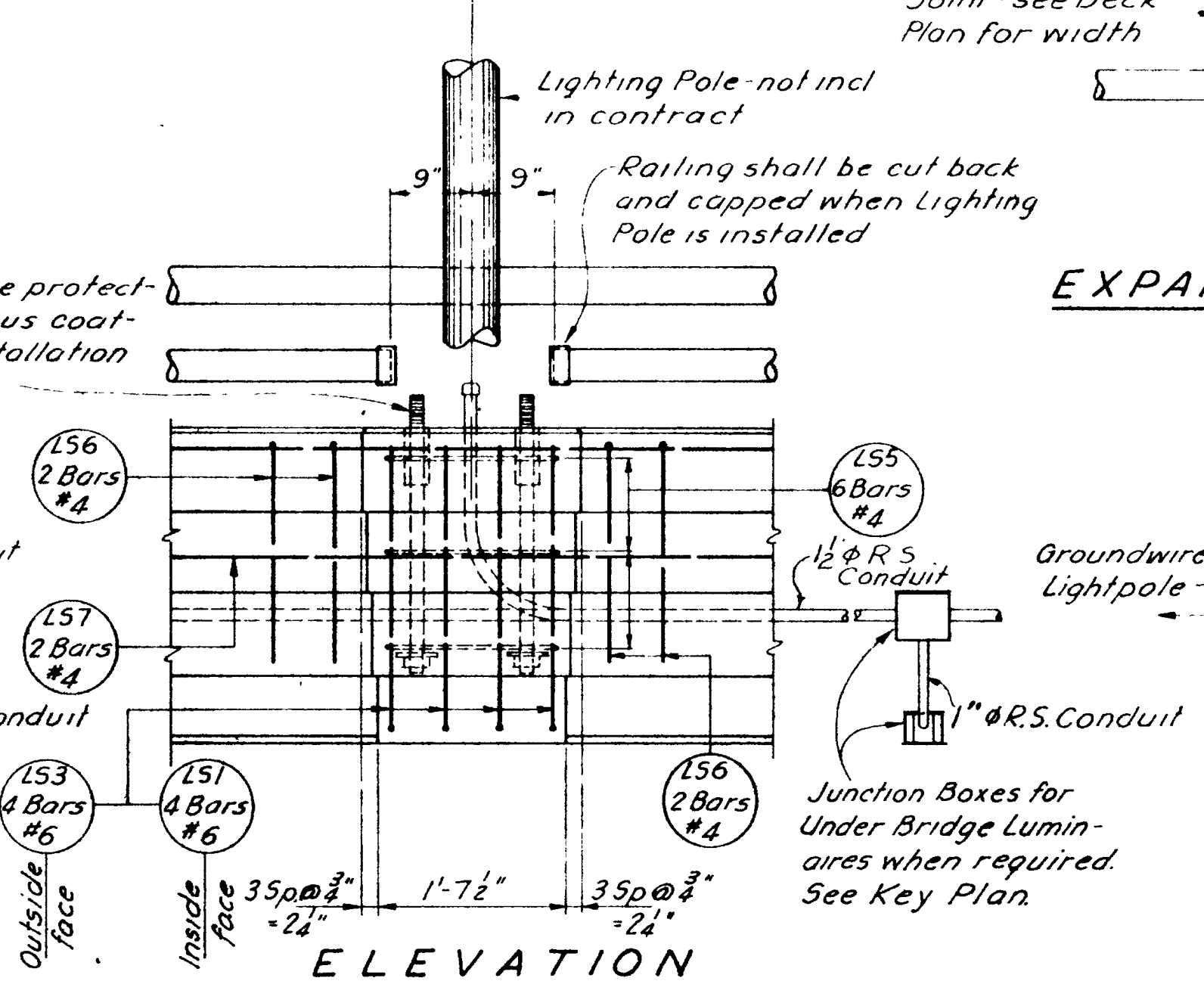
ANCHOR BOLT DETAIL



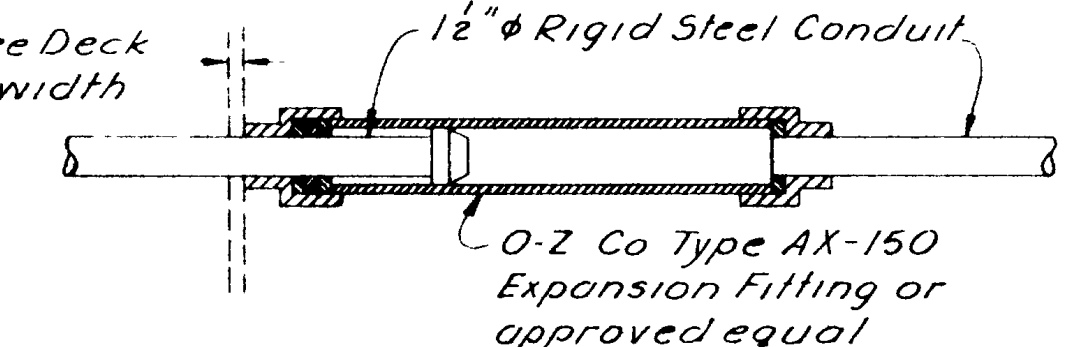
KEY PLAN NOT TO SCALE



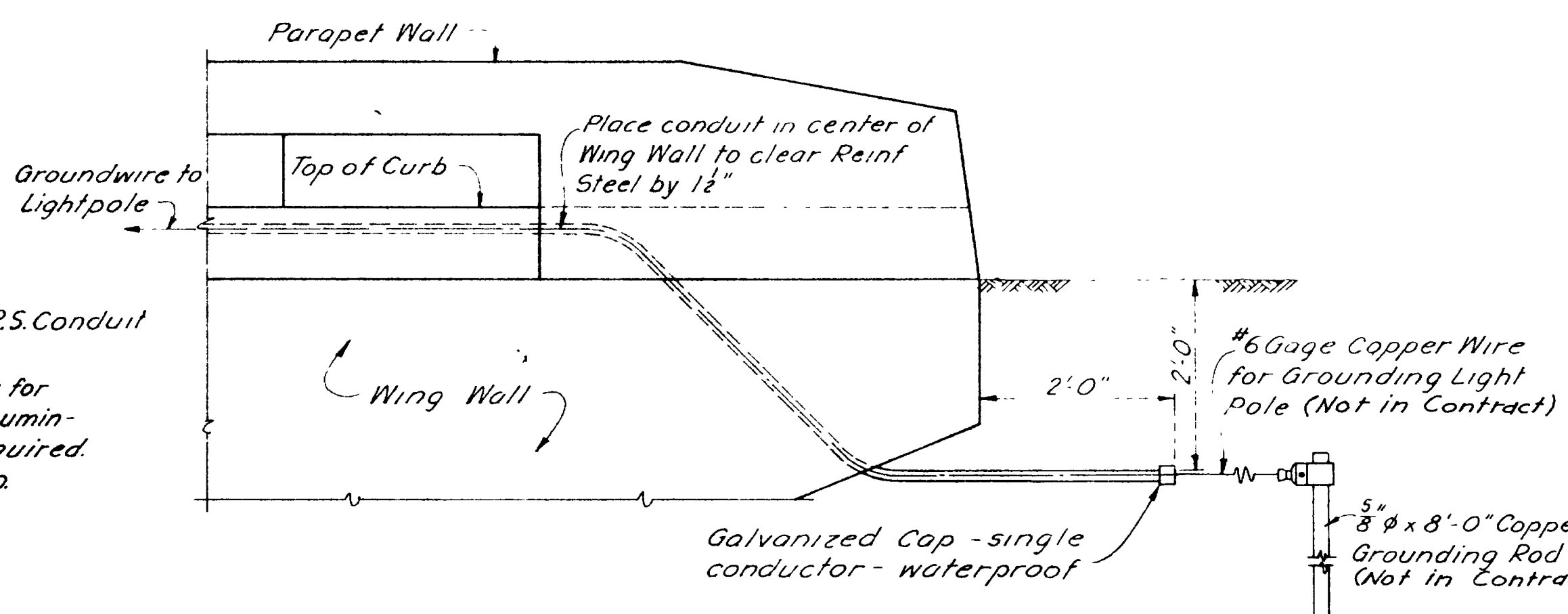
ELEVATION



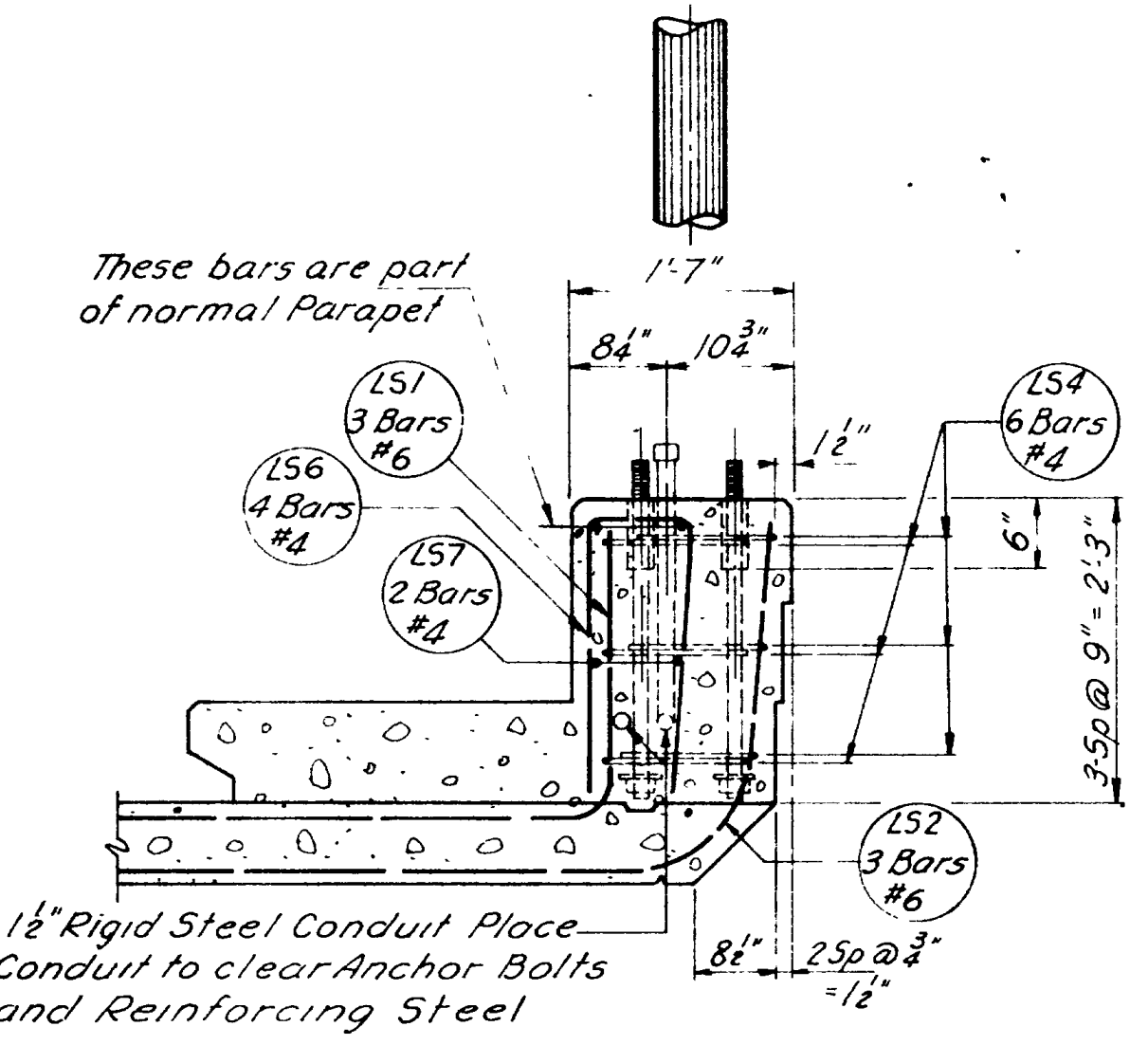
ELEVATION



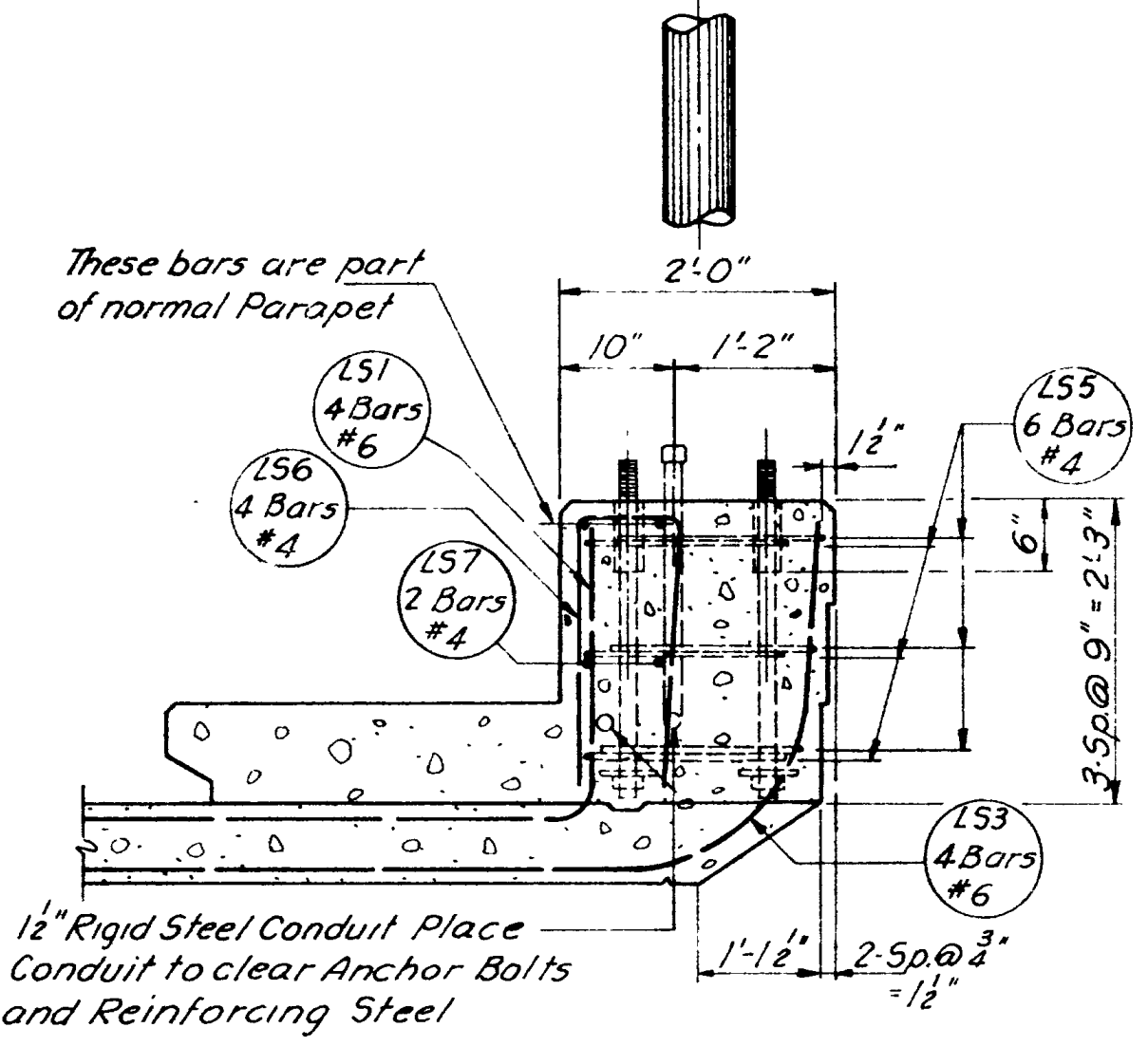
EXPANSION FITTING DETAIL



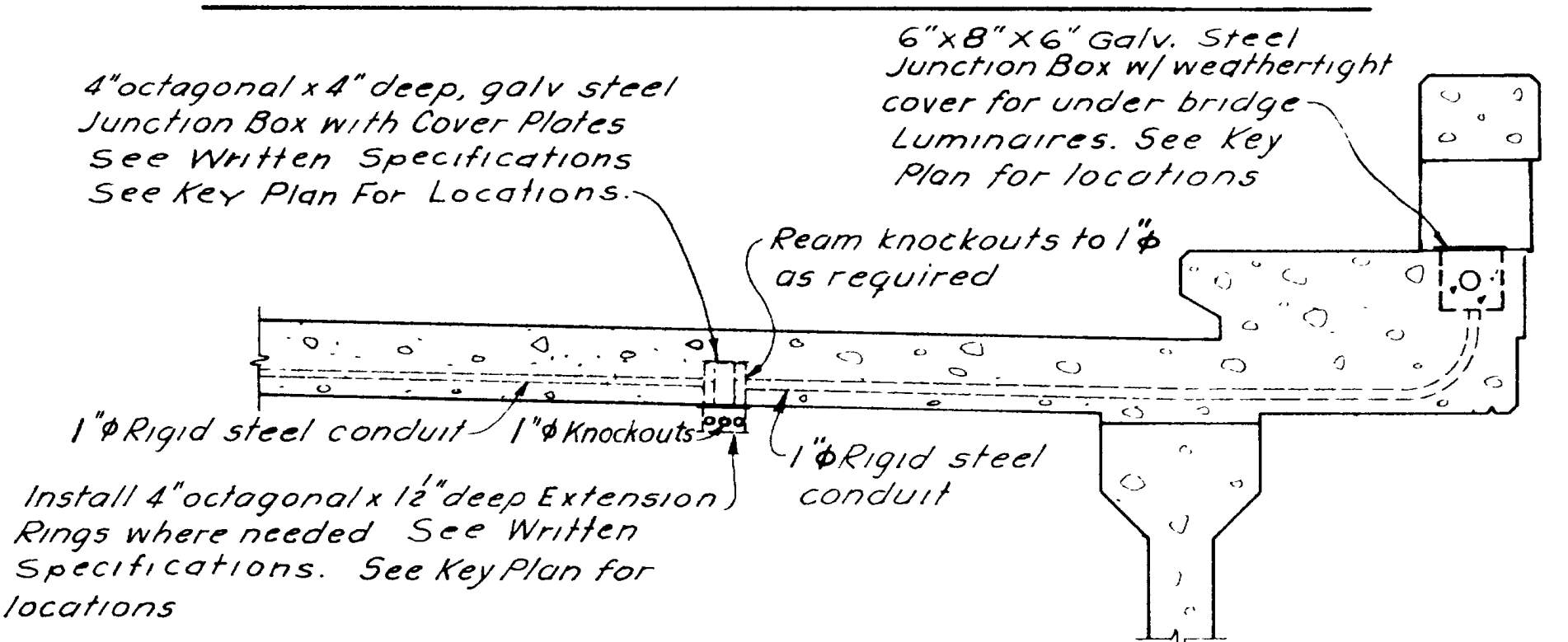
CONDUIT TERMINATION AT WING WALLS



SECTION A-A



SECTION B-B



DETAILS FOR FUTURE SUSPENDED UNDERPASS LUMINAIRES

Note
Light Pole stationing, if shown, is approximate Pole Mounting Bracket shall be located midway between railing posts nearest the station indicated.

STEEL SCHEDULE - POLE SUPPORT				
MARK	SIZE	LENGTH	AMT	SKETCH
L51	6	5'-2"	8	2'-10" 4-R
L52	6	6'-1"	-	3'-4" 11"R 2 1/2"
L53	6	6'-4"	8	3'-4" 15"R 2 1/2"
L54	4	3'-3"	-	L54 = 1'-1" 5'-0" 11" 1"
L55	4	4'-8"	12	L55 = 1'-6" 5'-0" 11" 1"
L56	4	4'-11"	8	2'-1" 12" 1" 1" 1"
L57	4	7'-0"	4	1" 1" 1" 1"

GENERAL NOTES

- 1-Rigid Steel Conduit, threaded and galvanized, shall conform to Federal Specifications WW-C-581c
- 2-Conduit Expansion Fittings shall be installed where ever conduit crosses structure expansion joints
- 3-Conduit shall terminate at junction boxes with double lock nuts and insulated grounding type bushings Bushings shall be 0-2 Co Type BL or approved equal All conduit shall be bonded in boxes with bare #6 AWG Copper wire
- 4-All work shall conform to the latest Edition of the National Electrical Code
- 5-See Special Provisions for Electrical Work - Bridges

UTAH STATE DEPARTMENT OF HIGHWAYS
SALT LAKE CITY, UTAH
STRUCTURES DIVISION

DAVIS-WEBER CO LINE TO 4400 SO.
5600 SOUTH INTERCHANGE
LIGHTING DETAILS FOR STRUCTURE

DESIGNED BY	CHEKED BY	I-15-8(20)334
DRAWN BY J.H.O. VM	CHECKED BY	PROJECT NUMBER
QUANTITIES BY	CHECKED BY	869+87.30
APPROVAL RECOMMENDED BY	GROUP LEADER	STATION
APPROVED	DATE	WEBER COUNTY

BR NO. F-66 26 OF 26

RIVERDALE STRUCTURE

	Length in Place	No Blows last Ft.		Length in Place	No Blows last Ft.
Abut # I					
1	32	66	1	34.0	65
2	31.2	66	2	35.6	48
3	32.4	66	3	34.2	45
4	32.5	56	4	34.3	60
5	34.5	64	5	37.7	45
6	33.5	65	6	34.4	60
7	35.5	63	7	36.4	60
8	34.7	66	8	41.0	50
9	33.4	65			
10	34.0	65			
11	31.0	60			
12	37.0	60			
		Total length of pile Abut # I 407.7 Ft			
Bent # II					
Ftg # 1					
1	30.3	42	1	34.0	44
2	30.2	40	2	32.5	50
3	32.0	41	3	32.5	45
4	30.5	40	4	36.3	46
5	31.8	40	5	35.3	47
6	30.7	40	6	32.2	50
Ftg # 2					
1	30.4	50	7	35.0	48
2	30.4	46	8	35.0	53
3	30.0	49	9	34.2	50
4	29.0	48			
5	30.0	50			
6	29.8	50			
Ftg # 3					
1	31.8	45	1	32.0	48
2	31.7	48	2	34.0	50
3	32.0	47	3	34.5	48
4	30.3	45	4	35.8	45
5	30.8	56	5	37.6	45
6	31.0	50	6	37.7	44
7	30.8	47			
8	32.0	45			
Ftg # 4					
1	30.0	50	1	34.3	45
2	31.3	45	2	35.1	44
3	31.2	50	3	37.0	46
4	31.0	50	4	35.7	45
5	30.3	50	5	37.0	43
6	30.0	51	6	37.2	48
7	29.3	52	7	38.0	45
8	29.0	46	8	39.2	35
			9	37.2	43
		Total length of pile Bent # II 857.6 Ft			
Bent # III					
Ftg # 1					
1	33.9	48	1	27.0	45
2	32.2	50	2	28.8	50
3	30.6	55	3	27.2	50
4	30.4	50	4	28.3	50
5	35.5	48	5	26.6	50
6	32.2	48			
7	33.0	45			
8	29.2	50			
Ftg # 2					
1	26.0	55	1	29.0	45
2	24.0	53	2	28.5	46
3	26.6	50	3	30.0	46
4	28.8	51	4	29.3	48
5	26.3	52	5	29.0	50
Ftg # 3					
1	29.0	45	1	28.0	46
2	28.5	46	2		
3	30.0	46	3		
4	29.3	48	4		
5	29.0	50	5		
6	28.8	46	6		

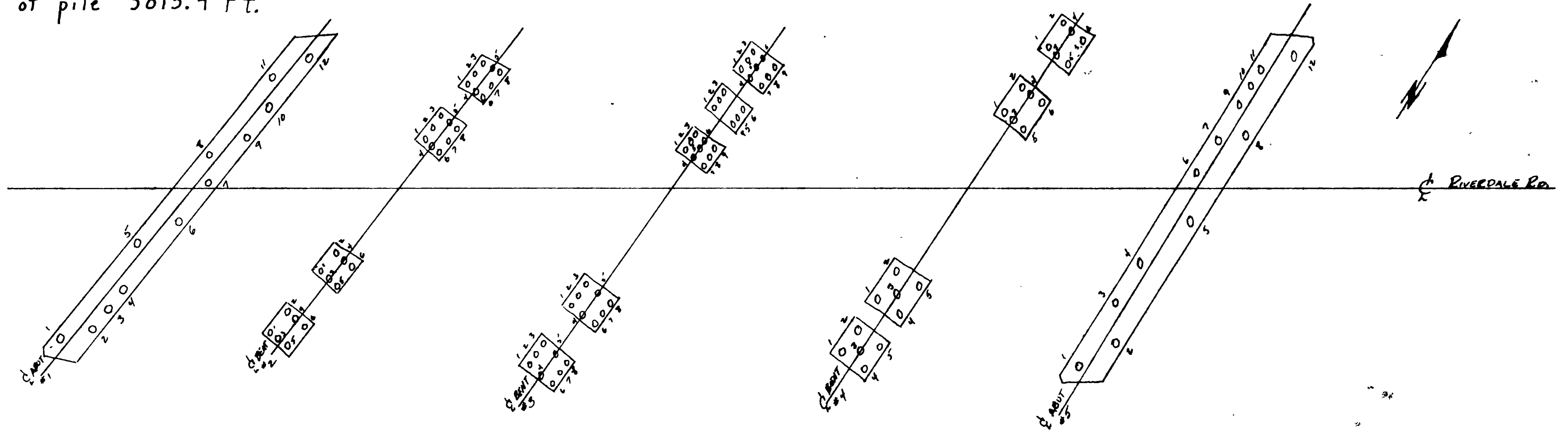
	Length in Place	No Blows last Ft.
Ftg # 4		
1	29.3	49
2	35.5	51
3	34.2	50
4	40.2	40
5	36.5	42
6	24.0	42
Abut # IV		
1	36.5	51
2	40.0	44
3	39.0	45
4	43.1	44
5	41.8	43
6	39.6	45
7	40.0	46
8	46.8	45
9	46.8	40
10	45.2	32
11	47.2	40
12	46.3	32

Total length of pile Bent # IV 643.9 ft.

Total length of pile Abut # V 512.3 ft.

Total length of pile Bent # III 1392.9 ft.

Total length of pile 3815.4 Ft.



FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	1-15-7(58) 332 1-15-8(20) 334	1963	1	

STATE OF UTAH STATE ROAD COMMISSION

PLANS OF PROPOSED STATE ROAD

FEDERAL AID PROJECT

1-15-7 (58) 332

LENGTH = 2.585 MILES

DAVIS COUNTY

1-15-8 (20) 334

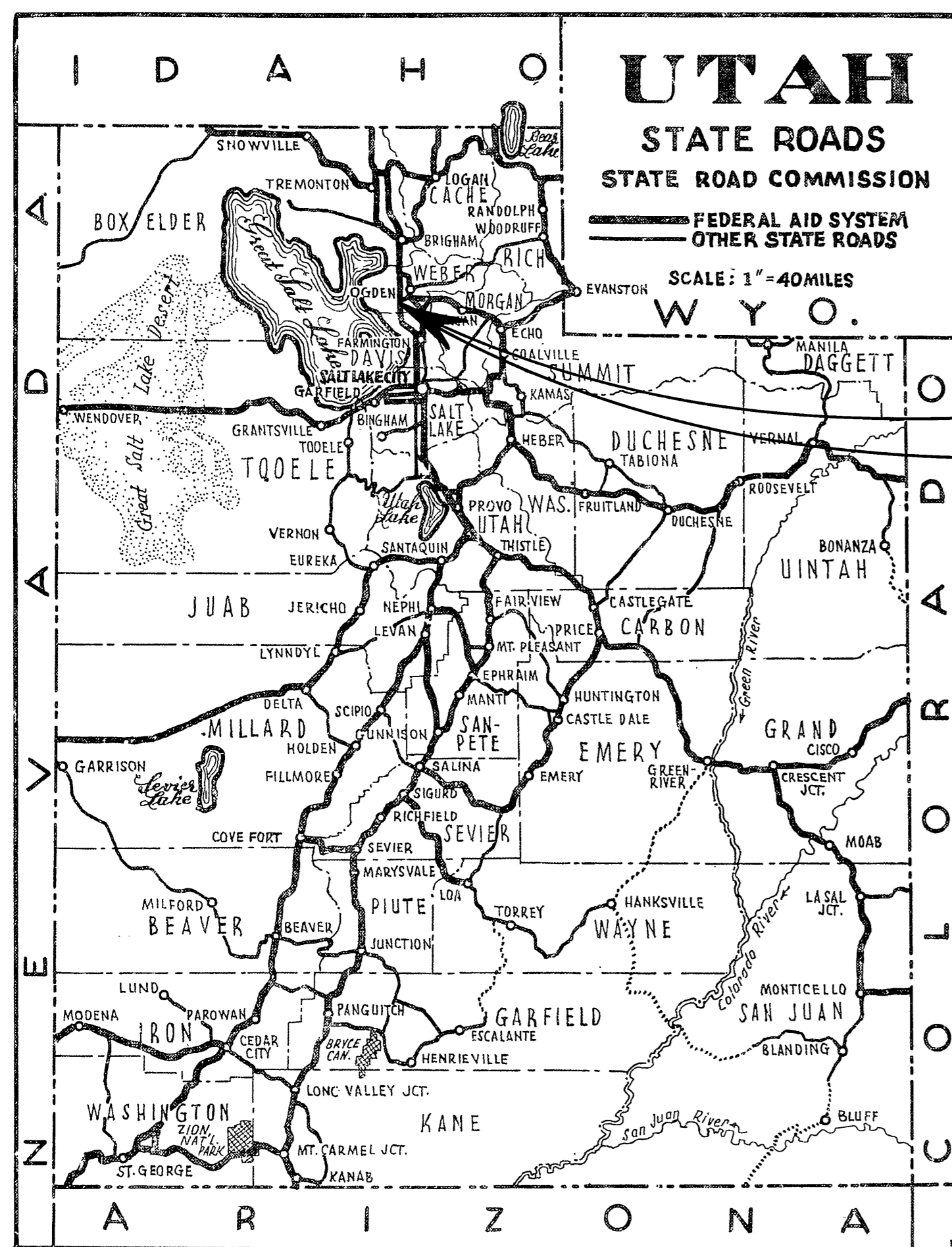
LENGTH = 2.043 MILES

WEBER COUNTY

IG-15-8(18) 334

LENGTH = 0.380 MILES

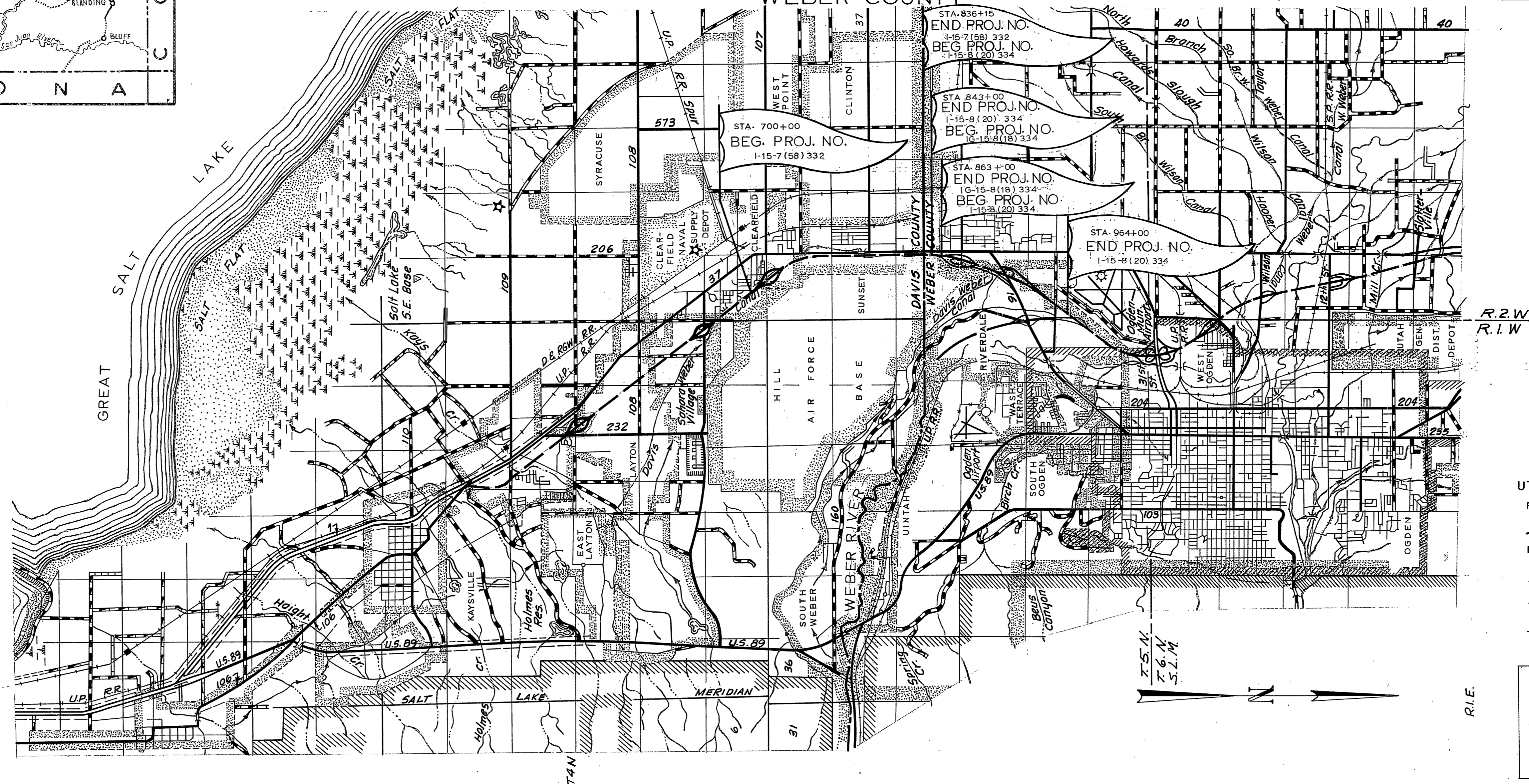
WEBER COUNTY



1-15-7(58) 332
1-15-8(20) 334
IG-15-8(18) 334

INDEX TO SHEETS

SHEET NO.	DESCRIPTION	DRAWING NO.	STATION
1	TITLE SHEET		
2A-C	TYPICAL SECTIONS & MAT'L S. SH		
3A-E	SUMMARY SHEETS		
4-31	PLAN AND PROFILE		
32-44	HILL FIELD OVERPASS	D-719	729+62.78 OFF. REV.
45-69	UPRR. OVERPASS	F-80	854+70
70-95	5600 SOUTH OVERPASS	F-66	869+81.30
96-101	5600 SOUTH CANAL X-ING	D-744	12,701.9600
102-131	RIVERDALE ROAD UNDERPASS	C-453	904+43.80
132-145	4400 SOUTH UNDERPASS	D-735	955+28.84
146	STD. RAILING DETAILS	MR-1	
147	CONCRETE PILES	SP-1	
148	STD. STEEL DETAILS	SS-3	
149	STD. CATCH BASINS	V-752	
150	STD. CATCH BASINS	V-753	
151	STD. CLEANOUT BOXES	V-777	
152-156	STD. ROAD SIGNS	500-1-5	
157	STD. FLARED END SEC. RCP.	B-160-1	
158	RIGHT OF WAY MARKERS	B-164	
159	SUPERELEVATION & WIDENING	202	
160	LEFT TURN MEDIANS	211	
161	CONCRETE CURB & GUTTER	220	
162	DEEP BEAM GUARD RAIL	230	
163	RCP CULVERTS	320	
164	DROP INLETS	346	
165	CONSTRUCTION IDENT. SIGNS	504	
166	CHAIN LINK FENCE	523	



UTAH STATE DEPARTMENT OF HIGHWAYS

RECOMMENDED FOR APPROVAL: AUG., 1963

Ed. Shoup
ENGINEER OF ROADWAY DESIGN

RECOMMENDED FOR APPROVAL: AUG., 1963

Blaine J. Kay
ENGINEER OF PLANS AND CONTRACTS

APPROVED: AUG., 1963

William C. Hill
ACT. DIRECTOR OF HIGHWAYS

DEPARTMENT OF COMMERCE

BUREAU OF PUBLIC ROADS

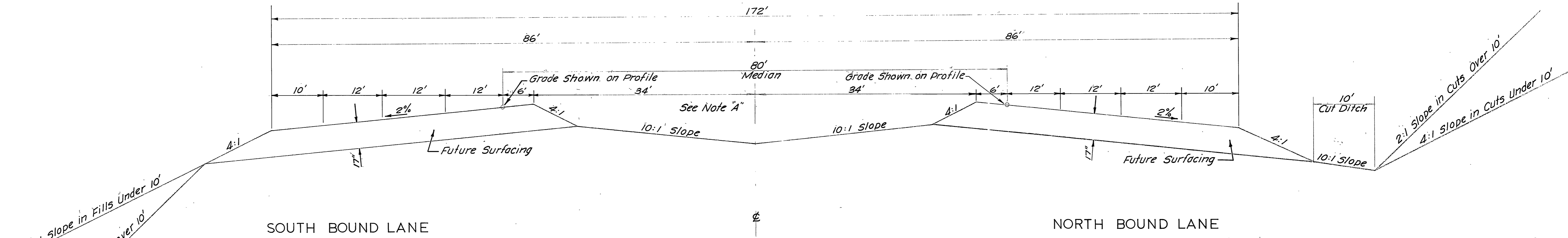
APPROVED:

DIVISION ENGINEER DATE

TYPICAL SECTIONS

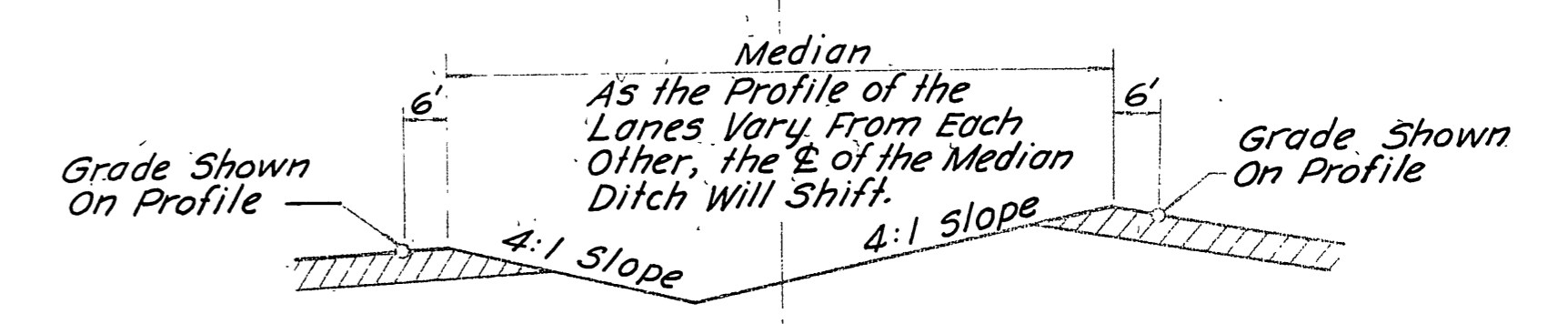
FED. ROAD DIST. NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	1-15-7(68) 332		2	
		1-15-8(20) 334			
16-15-8(18)334					

NO. 1



STA. 700+00 TO STA. 829 + 79.30 S.B. LANE
 STA. 700+00 TO STA. 830 + 86.27 NB LANE

NORTH BOUND LANE

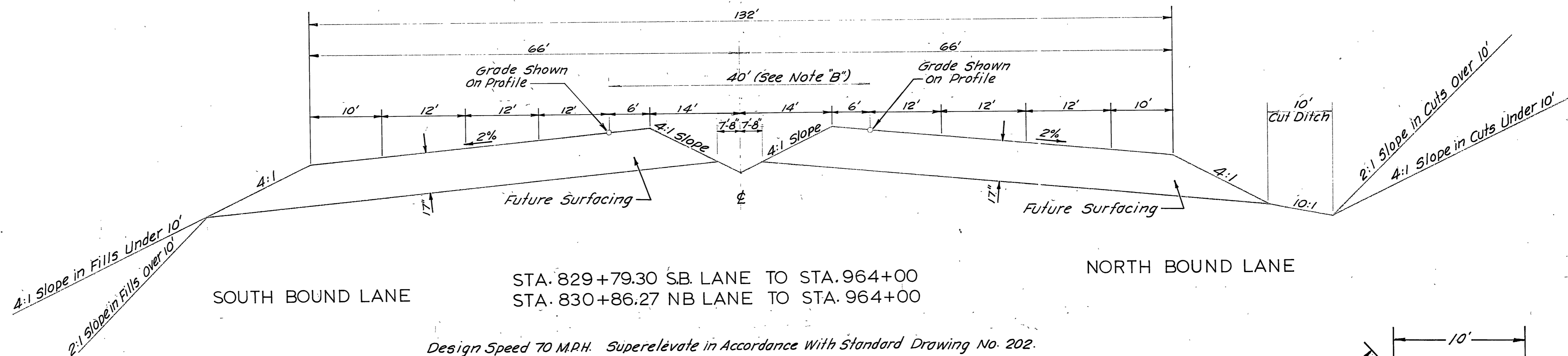


NOTE "A"

STA. 700+00 TO STA. 964+00

NOTE "B"
 Median Width Varies From 80' to 40' in Transition Through Curve. See Sheet Number 13 For Details.

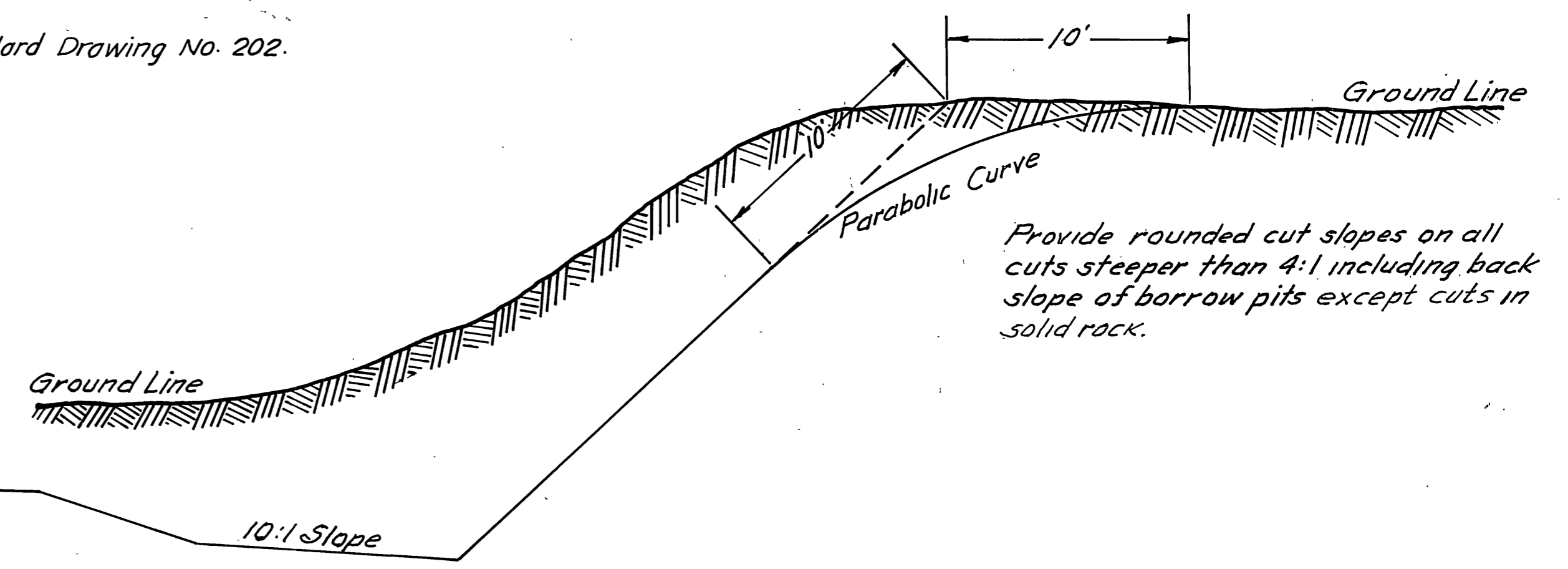
NO. 2



STA. 829+79.30 S.B. LANE TO STA. 964+00
 STA. 830+86.27 NB LANE TO STA. 964+00

Design Speed 70 MPH. Superelevate in Accordance With Standard Drawing No. 202.

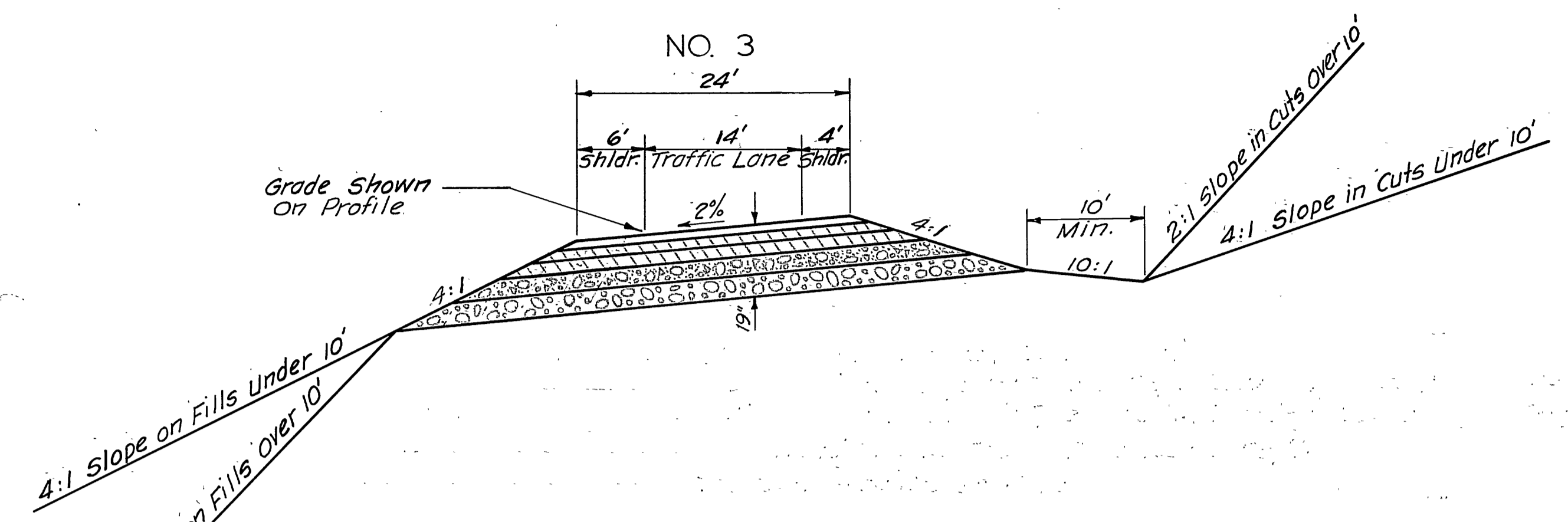
NOTE
 SURFACING OF INTERSTATE NOT INCLUDED IN THIS CONTRACT.



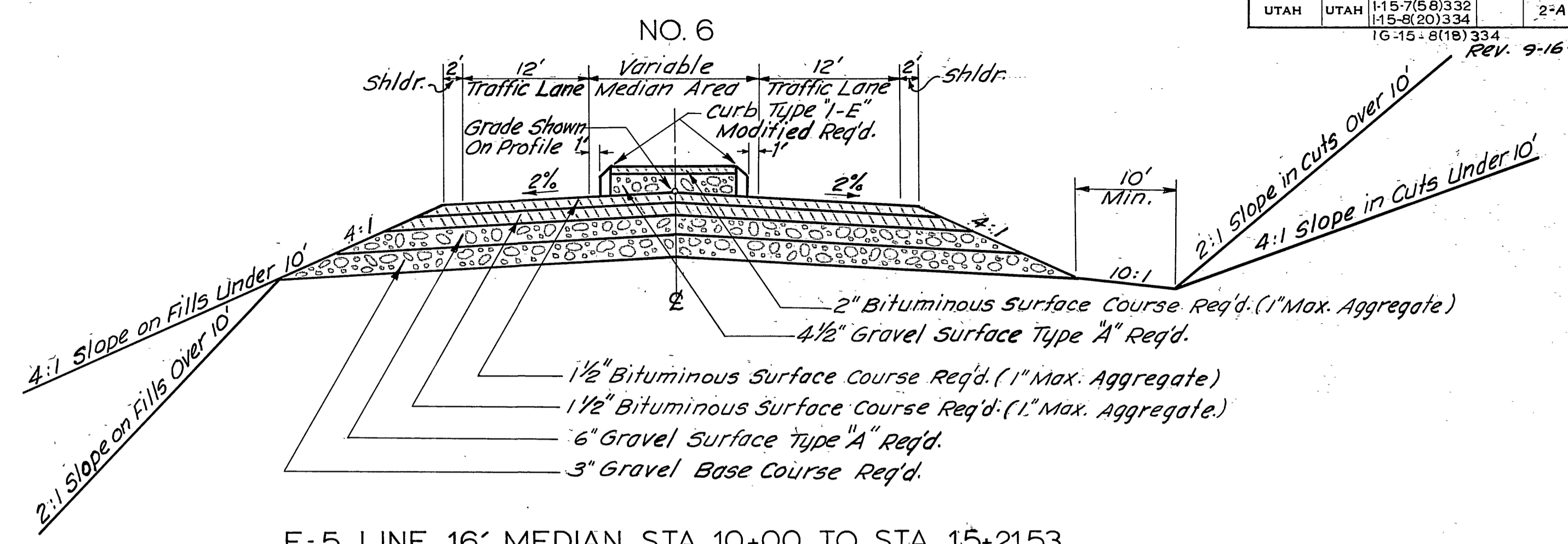
TYPICAL SECTIONS

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	1-15-7(58)332 1-15-9(20)334 16-15-8(18)334		2-A	

REV. 9-16-63

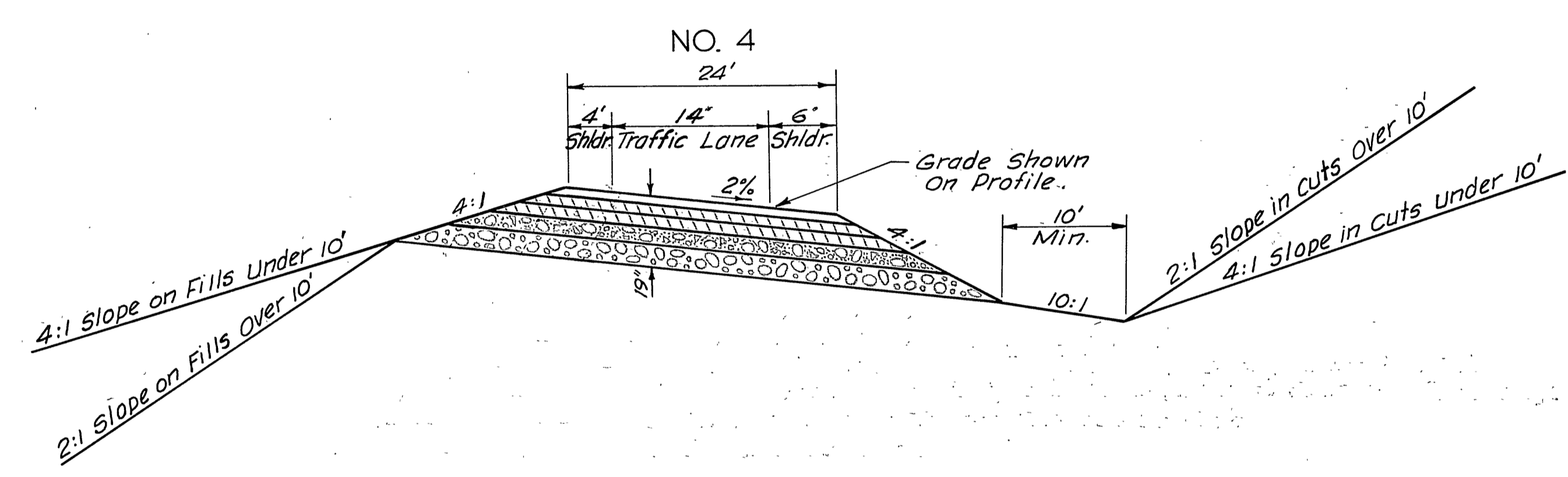


F-1 LINE STA. 10+00 TO STA. 19+51.34 - GRADE AND DRAIN ONLY
 F-2 LINE STA. 10+00 TO STA. 20+75.13 - GRADE AND DRAIN ONLY
 F-4 LINE STA. 10+00 TO STA. 14+42.16 - GRADE AND DRAIN ONLY
 T LINE STA. 10+01.03 TO STA. 41+30.02 - GRADE AND DRAIN ONLY

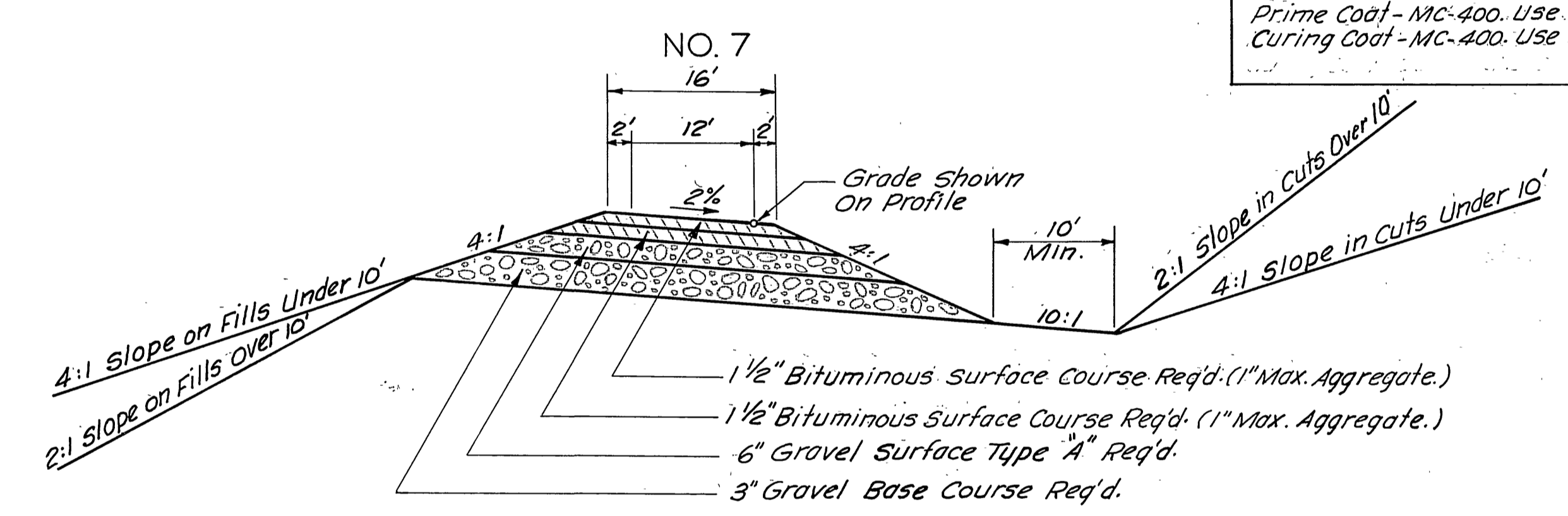


F-5 LINE 16' MEDIAN STA. 10+00 TO STA. 15+21.53
 F-5 LINE VAR. MEDIAN STA. 15+21.53 TO STA. 20+54.78
 F-5 LINE ZERO MEDIAN STA. 20+54.78 TO STA. 57+08.35

BITUMINOUS MATERIAL
 Plant Mix - Penetration 60-70. Use 5.5% Gravel By Weight.
 Seal Coat - RC-800. Use .25 Gal./Sq. Yd. With Type "A" Cover Material.
 Tack Coat - RC-70. Use .15 Gal./Sq. Yd. Between Plant Mix Bituminous Courses.
 Prime Coat - MC-400. Use 0.5 Gal./Sq. Yd.
 Curing Coat - MC-400. Use 0.15 Gal./Sq. Yd.

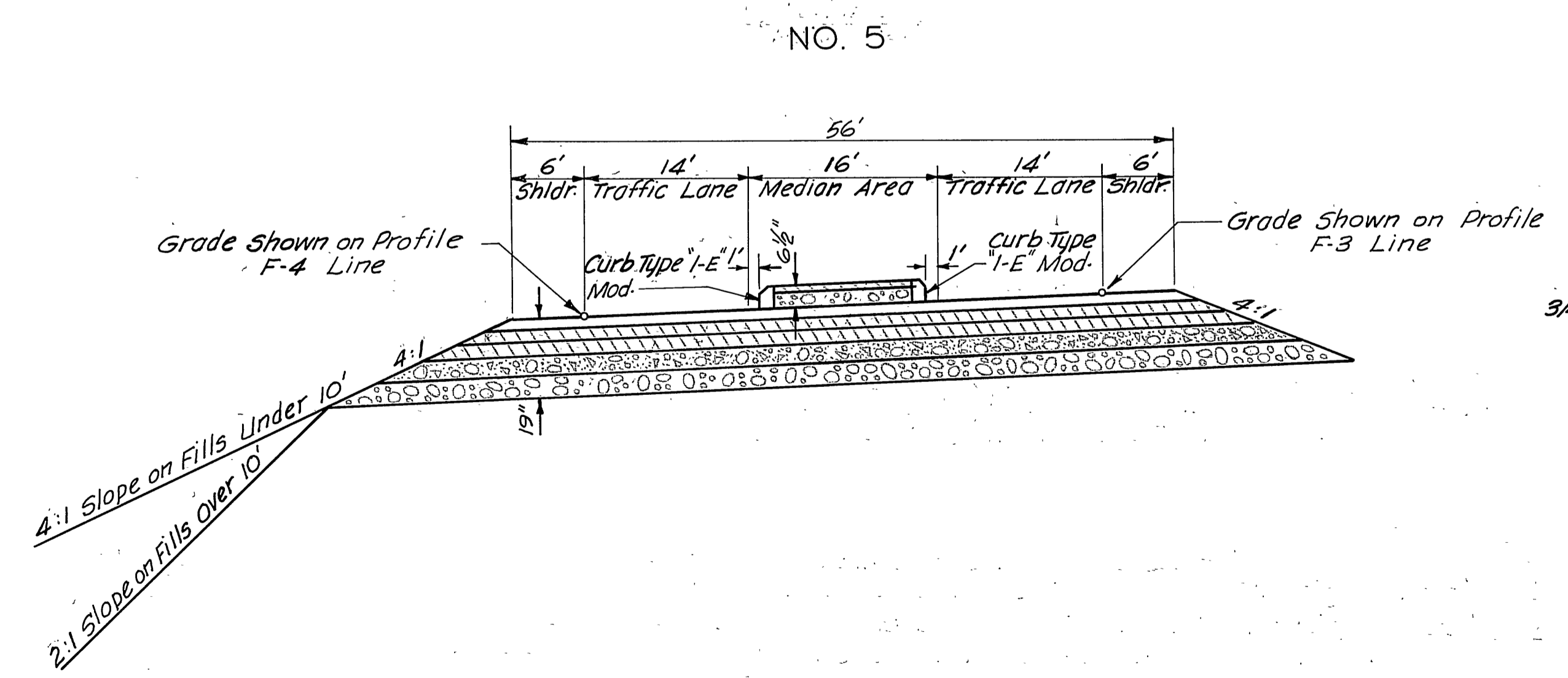


F-3 LINE STA. 10+00 TO STA. 19+00 — GRADE AND DRAIN ONLY
 U LINE STA. 10+00 TO STA. 41+67.29 — GRADE AND DRAIN ONLY
 R-1 LINE STA. 10+00 TO STA. 23+28.66 - GRADE AND DRAIN ONLY
 R-2 LINE STA. 10+00 TO STA. 17+90.07 - GRADE AND DRAIN ONLY

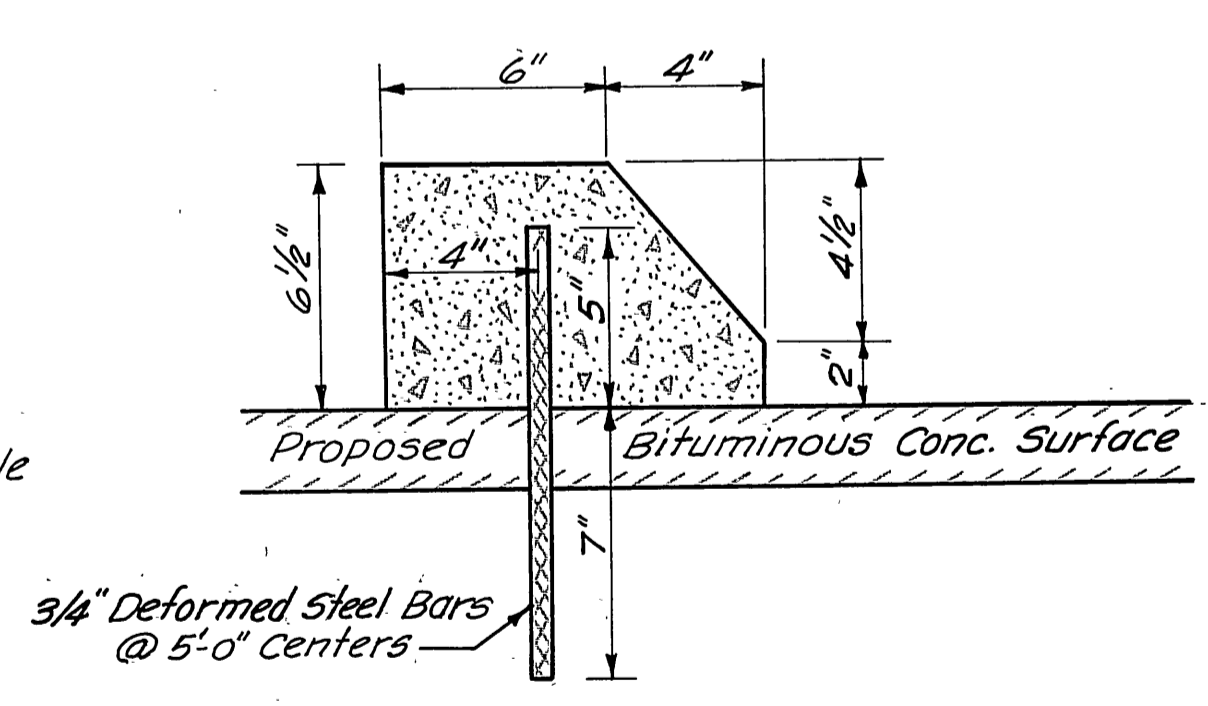


F-5A LINE STA. 10+00 TO STA. 12+87

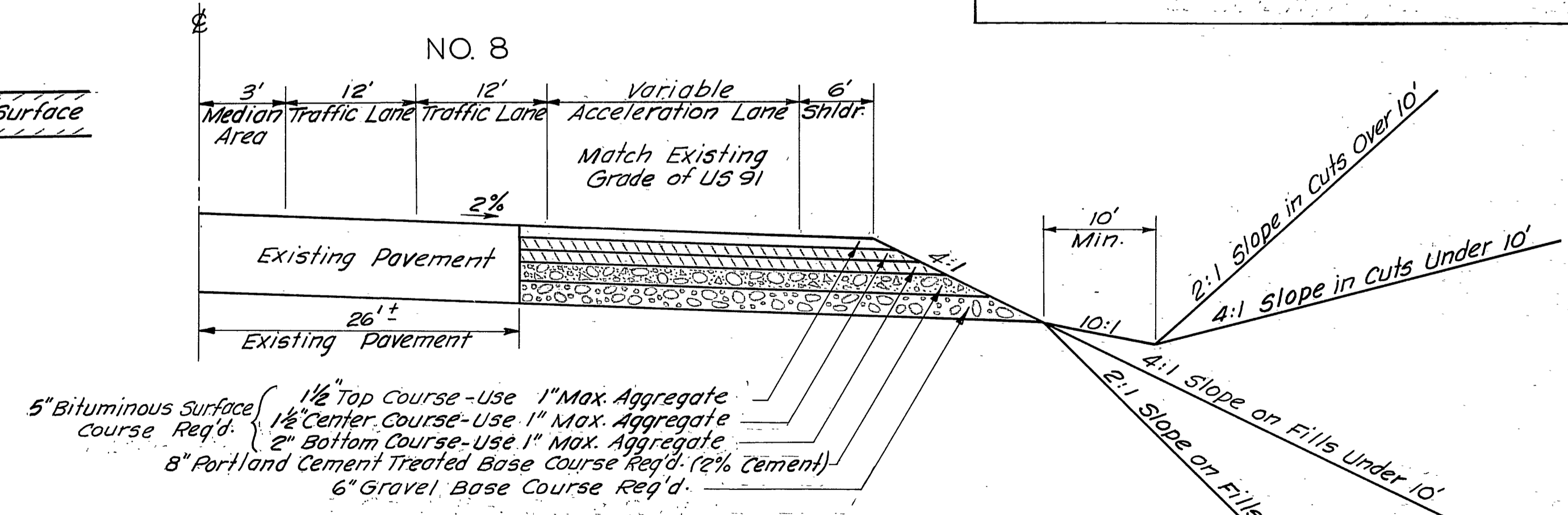
GRAVEL MATERIAL
 Cover Material - Type "A" - 25 #/sq. yd.
 Gravel Surface - Type "A" - 140 #/cu. ft. compacted.
 Portland Cement (2% by weight) Treated Base Course - 140 #/cu. ft. compacted.
 Gravel Base Course - 140 #/cu. ft. compacted.
 Bituminous Surface Course - 148 #/cu. ft.



F-3 LINE STA. 19+00 TO STA. 25+10.15 - GRADE AND DRAIN ONLY
 F-4 LINE STA. 14+42.16 TO STA. 20+57.88 - GRADE AND DRAIN ONLY



CURB TYPE 1-E MODIFIED

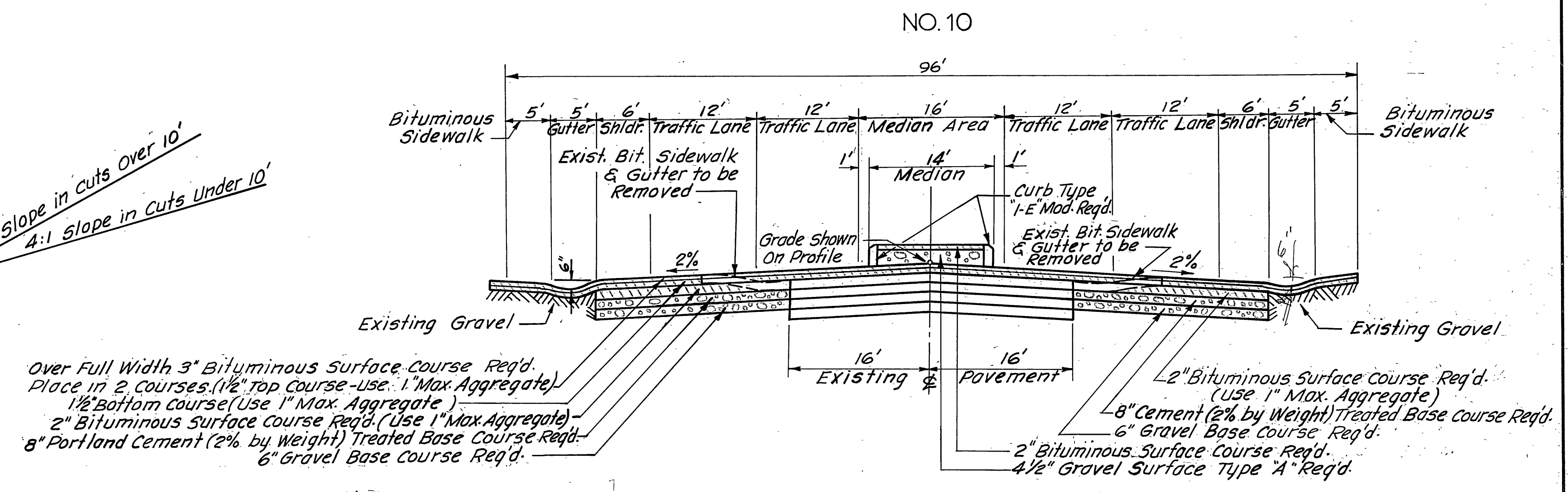
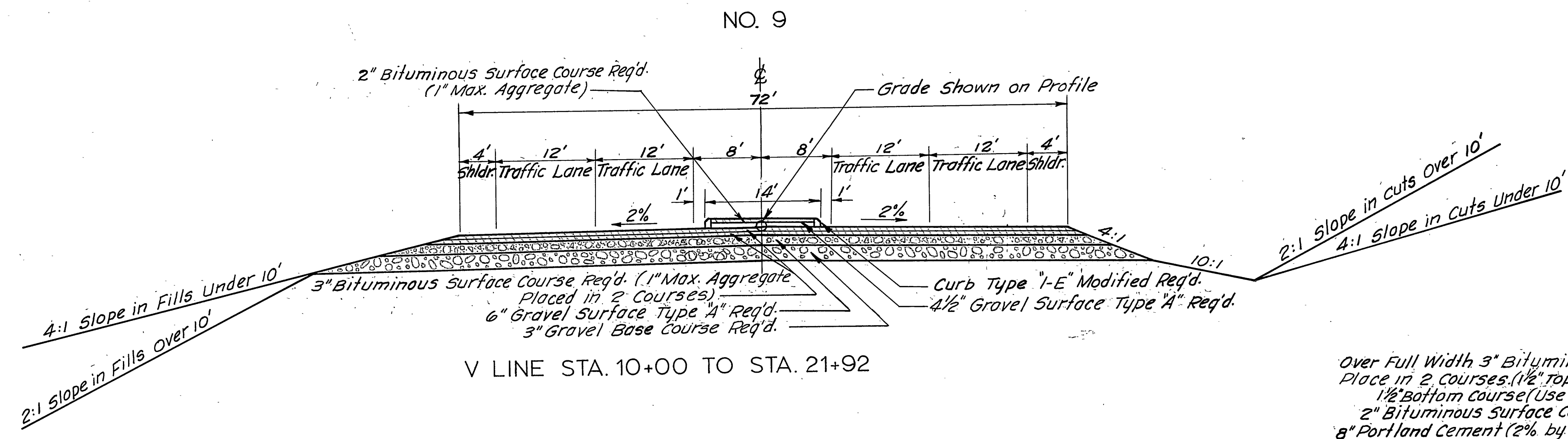


RIVERDALE ROAD AT R-2 AND F-5A LINES

REVISIONS	DATE	BY

TYPICAL SECTIONS

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	15 7(58) 332		2-8	
		15 8(20) 334			
16 15 B(18) 334					
Rev. 9-16-63					
Rev. 9-18-63					



Over Full Width 3" Bituminous Surface Course Req'd. Place in 2 Courses (1/2" top course - use 1" Max. Aggregate) 1/2" bottom course (use 1" Max. Aggregate)

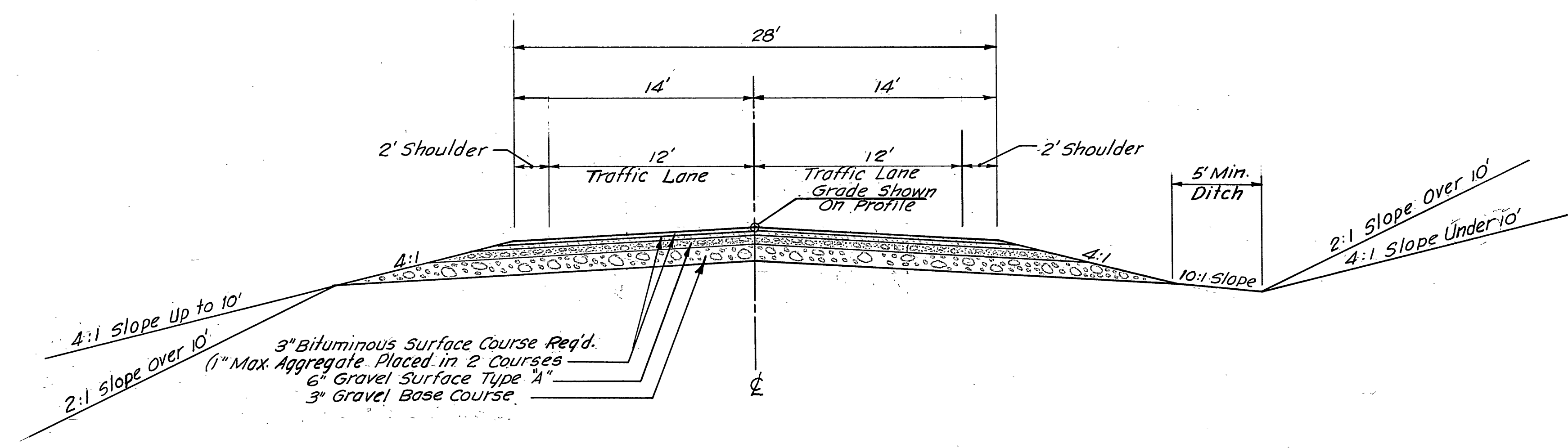
2" Bituminous Surface Course Req'd. (Use 1" Max. Aggregate)

8" Portland Cement (2% by Weight) Treated Base Course Req'd.

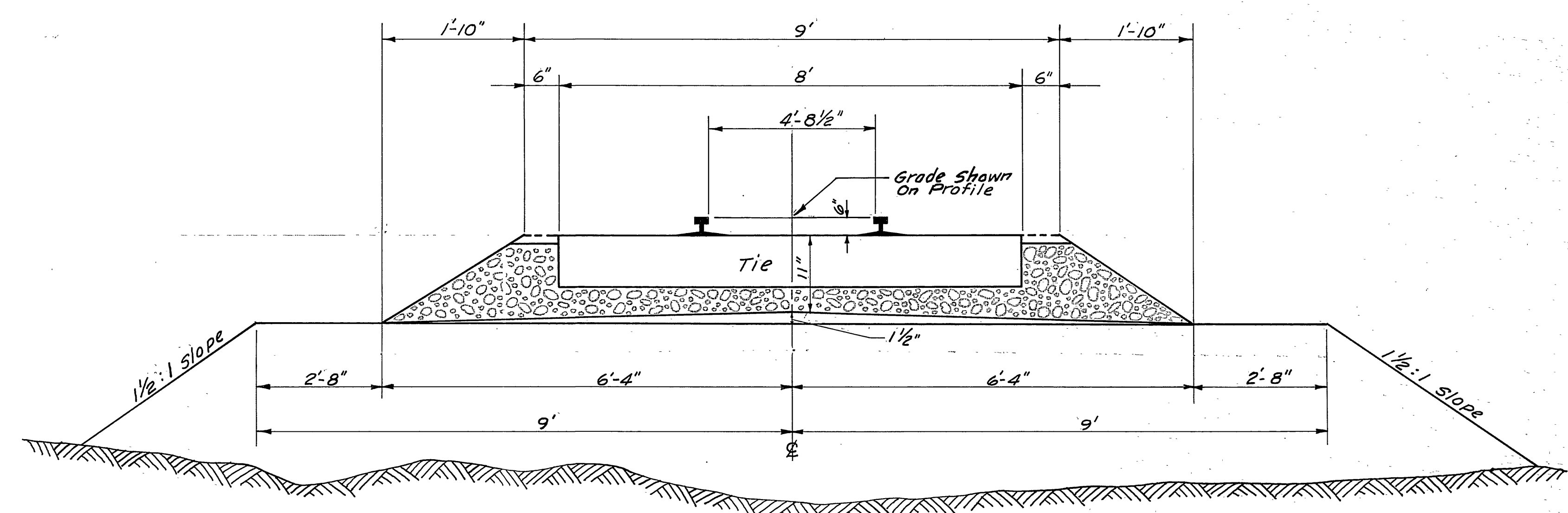
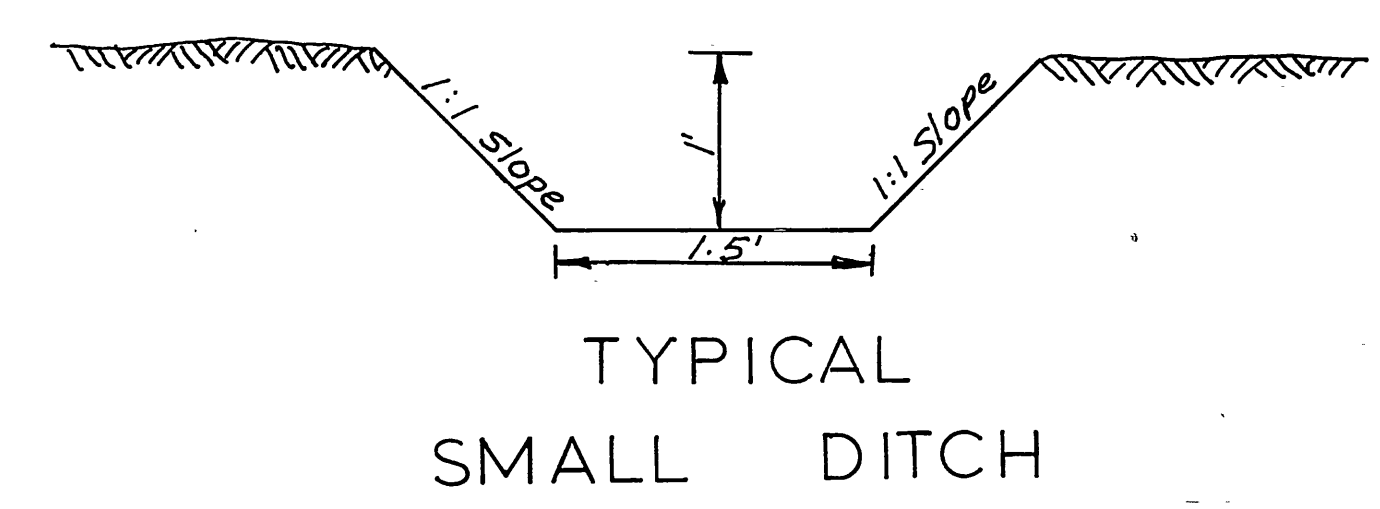
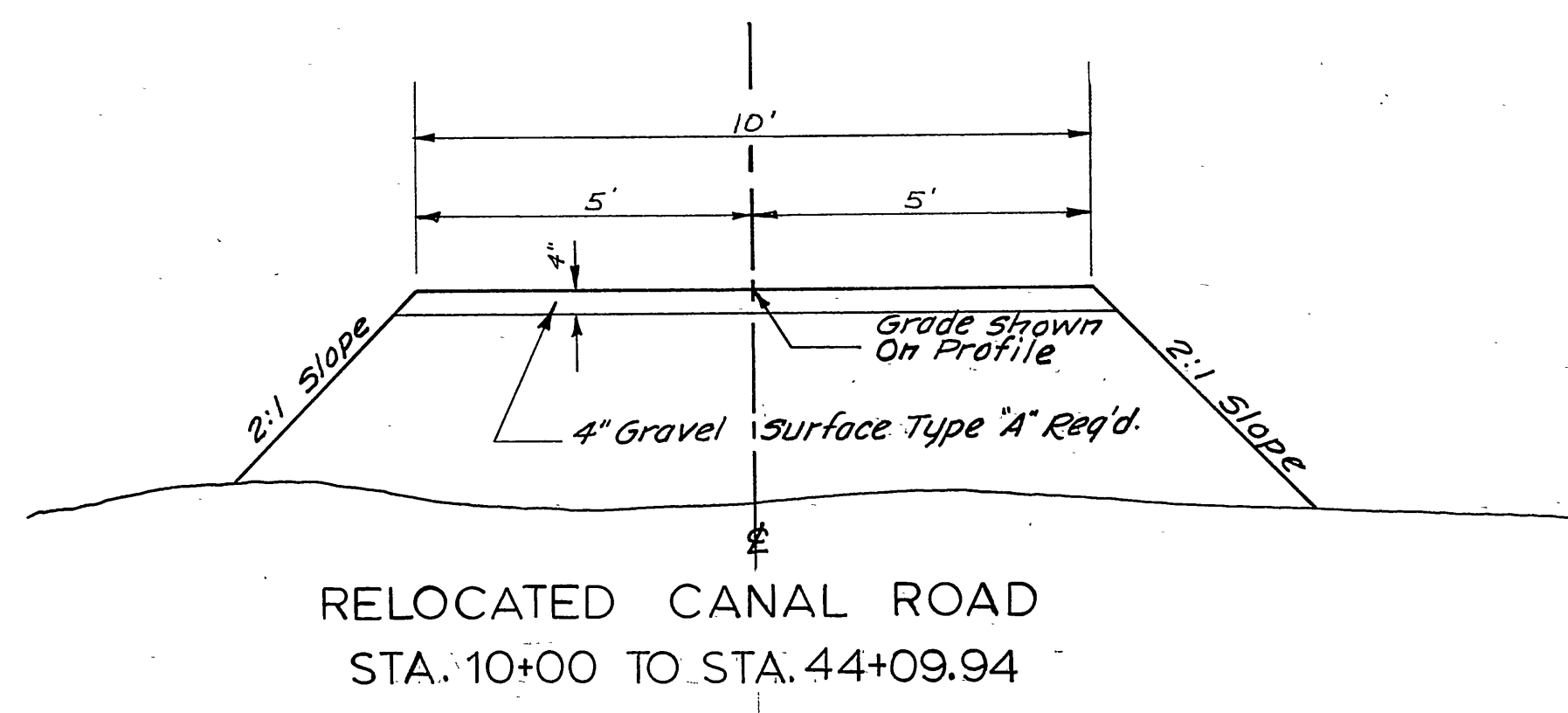
6" Gravel Base course Req'd.

NOTE: Shaping of Rolled Gutter will Not be Paid for Separately But Will be Included in Other Bid Items of Work.

REVISIONS	BY	DATE



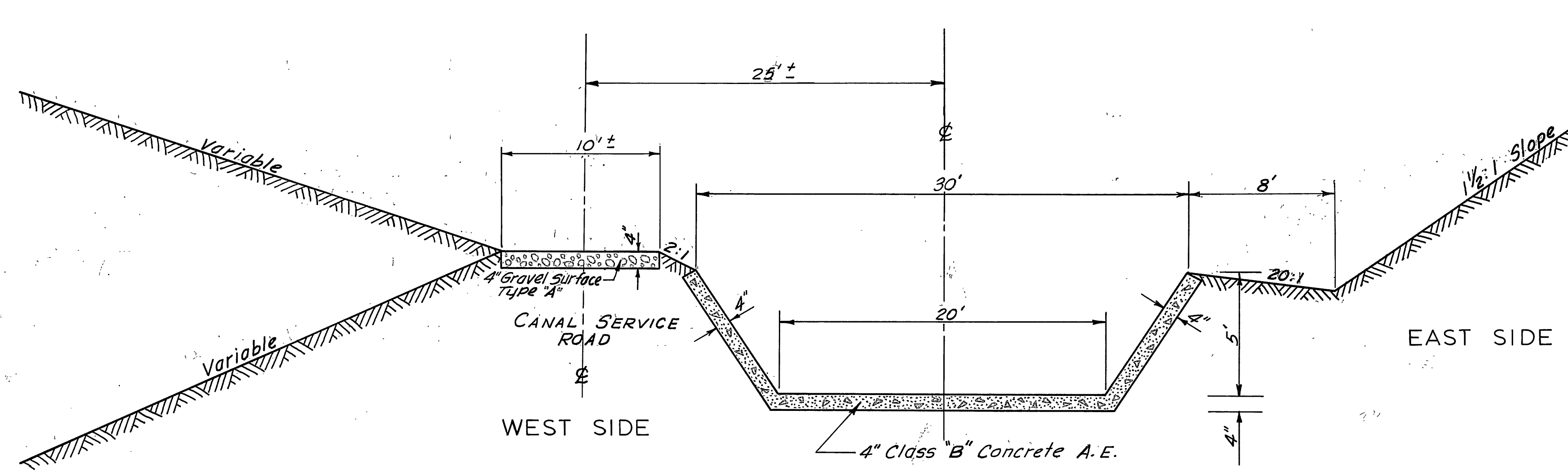
R-7 LINE STA. 10+00 TO STA. 21+41.04
 R-9 LINE STA. 10+00 TO STA. 22+42.63
 R-10 LINE STA. 10+00 TO STA. 19+54.17
 R-8 LINE STA. 10+00 TO STA. 22+94.38



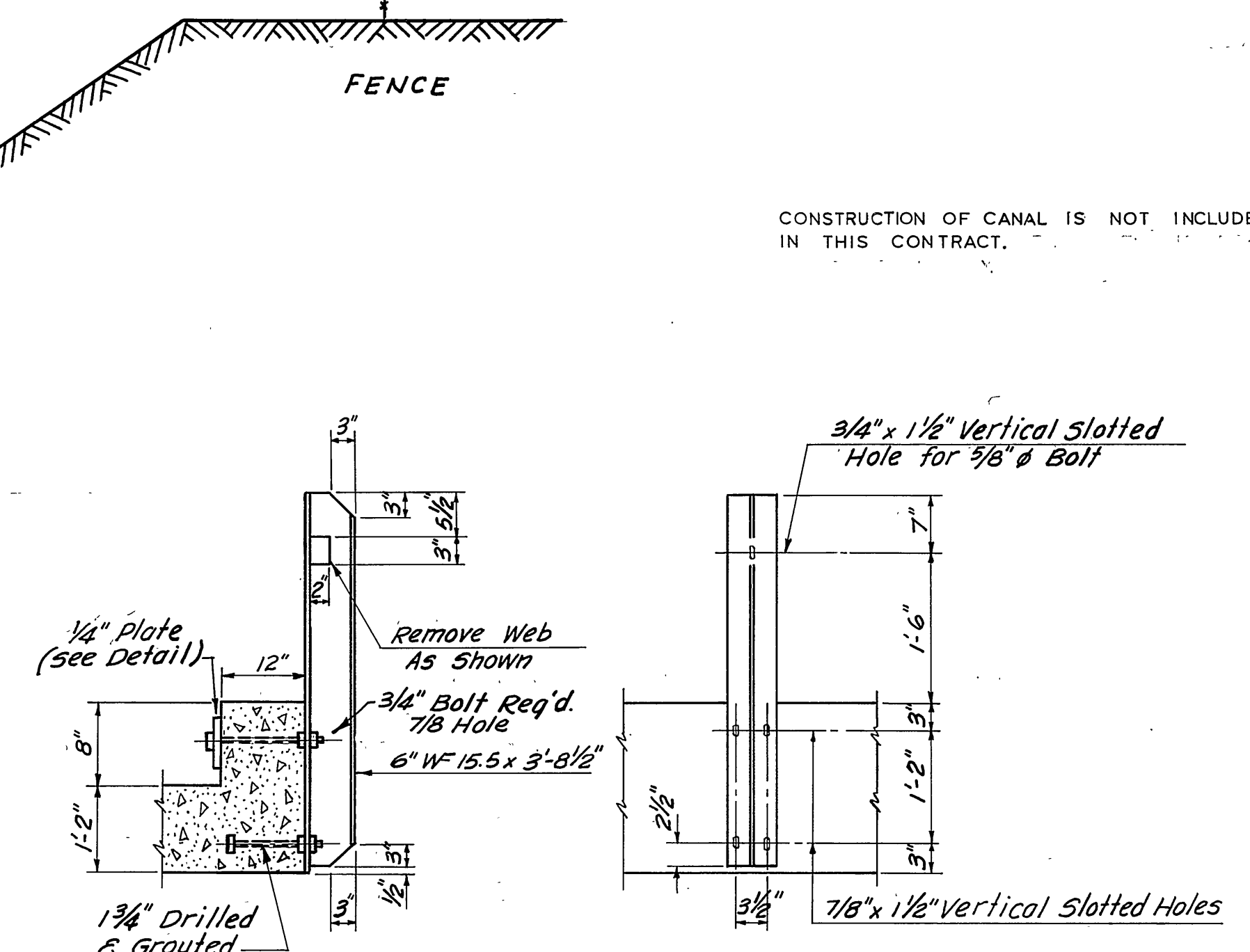
U.P.R.R. RELOCATION
 STA. 10+06.62 TO STA. 28+67.14
 (GRADING ONLY)

NOTE: Ballast Ties and Rails to be Constructed By Others.

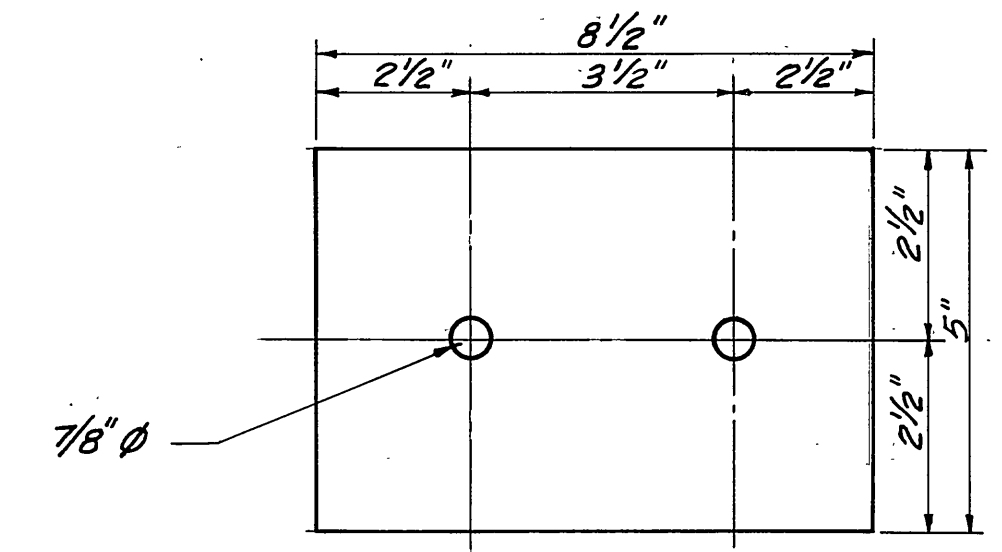
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	115 7 (58) 332		2	5
		115 8 (20) 334			
		1G 15 8 (18) 334			
REV. 9-16-63					



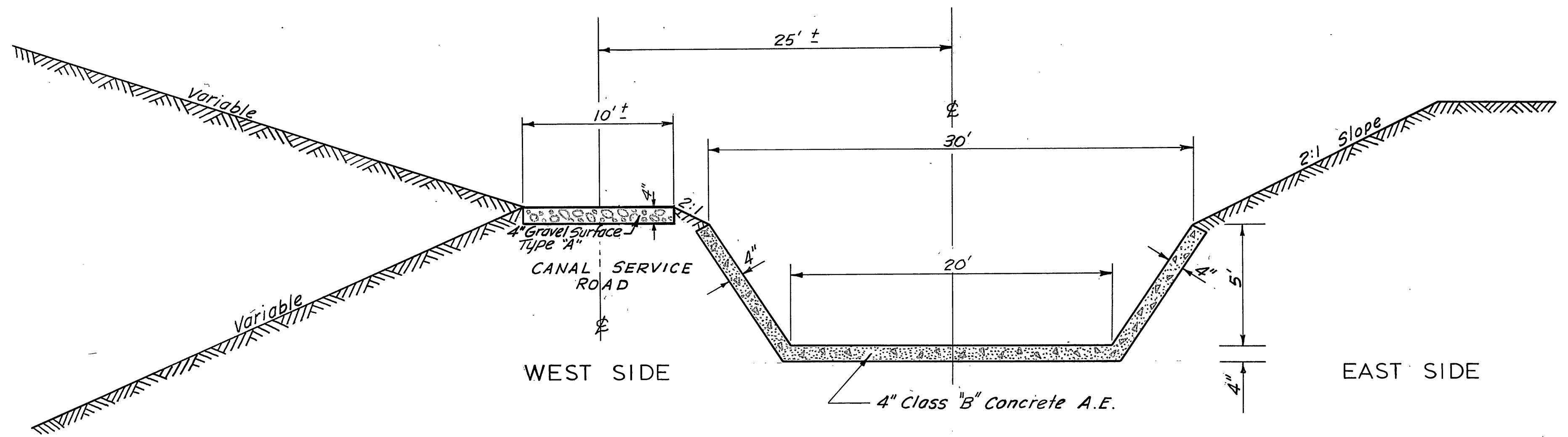
CROSS SECTION — DAVIS WEBER CANAL
 STA. 10+00 TO STA. 35+66.58
 NORTHWEST CORNER OF HILL FIELD
 I-15-8 (12) 334



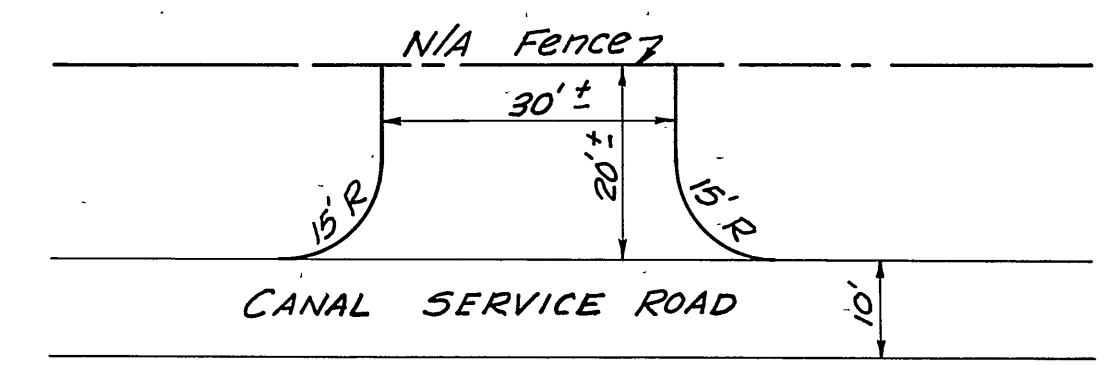
GUARDRAIL POST
 (2 Required)
 "V" LINE



1/4" PLATE



CROSS SECTION — DAVIS WEBER CANAL
 STA. 13+50 TO STA. 66+40.77



RELOCATED CANAL

R.R. Fence
 Provide Maintenance Turn-Arounds
 As Shown or as Directed by the Project
 Engineer.
 Place at Station 785 ± and Station 820 ±.

REVISIONS	BY	DATE

NOTES

The information on these materials prospects shall in no way be construed so as to conflict with "Division I, Section 1-6.2 of Designated Local Materials Sources of the State of Utah Standard Specifications, 1960 Edition and Supplements."

LEGEND

Ground Water Table (G.W.T.) = ▽, ○ = Test Hole
Bottom of Test Hole = * , ○ = Section No.
Section Line (ξ) =
1/4 Section Line =
1/16 Section Line =
Proposed Highway =
Prospect Boundary =
Property Boundary =

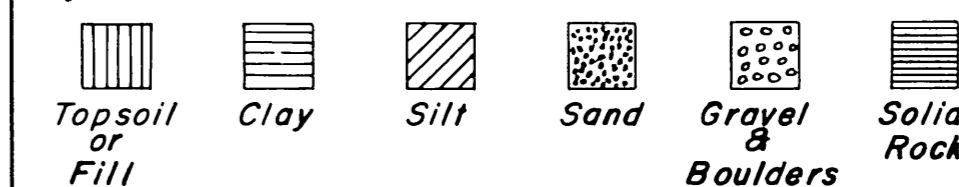
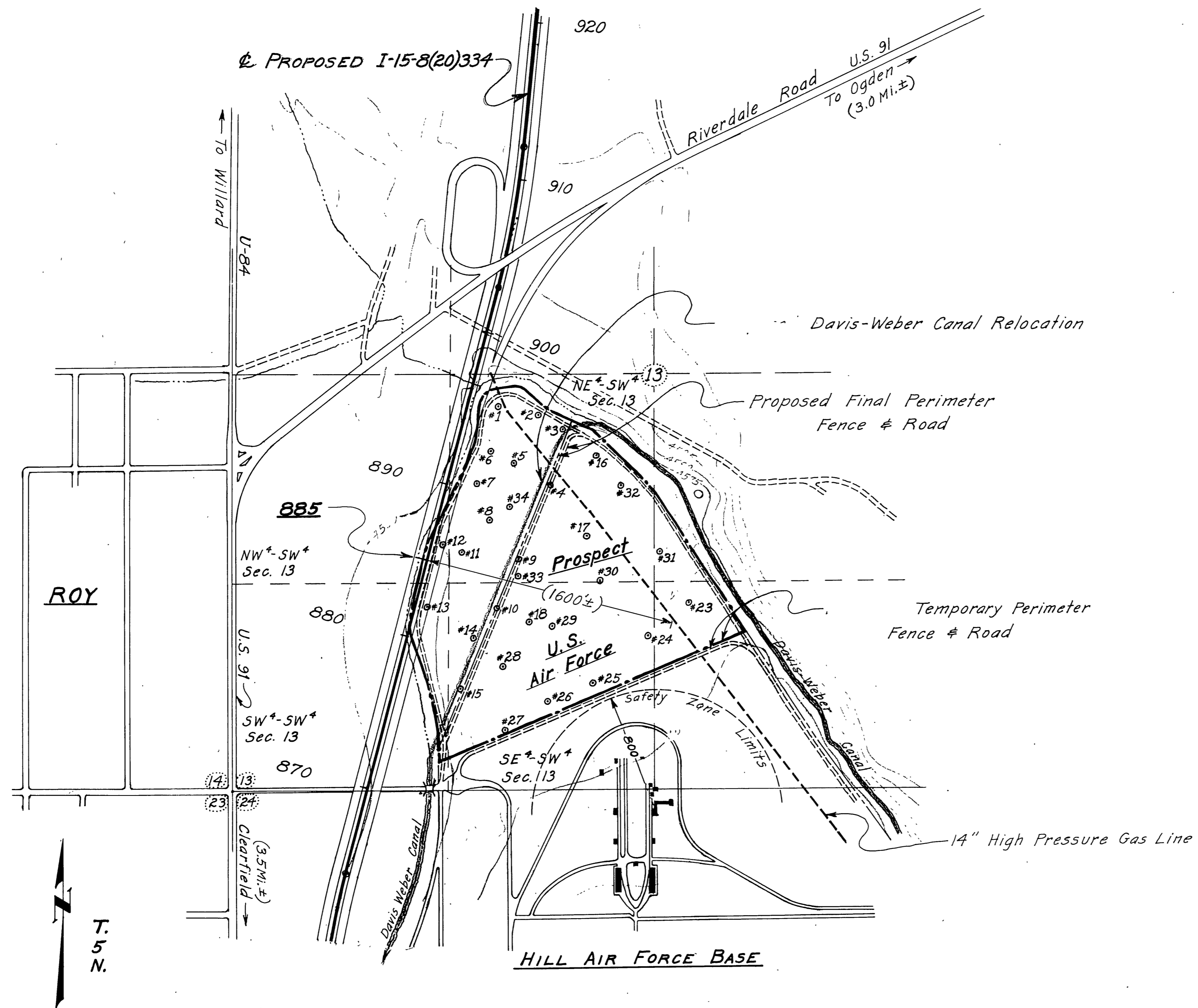


Table with project details: FED. ROAD DIV NO., STATE, FEDERAL AID PROJECT NO., FISCAL YEAR, SHEET NO., TOTAL SHEETS. Includes project name: SOUTH. OF HILL AIRBASE INTERCHANGE TO WEBER-DAVIS COUNTY LINE TO 4400 SOUTH IN RIVERDALE.

REV 8-30-63

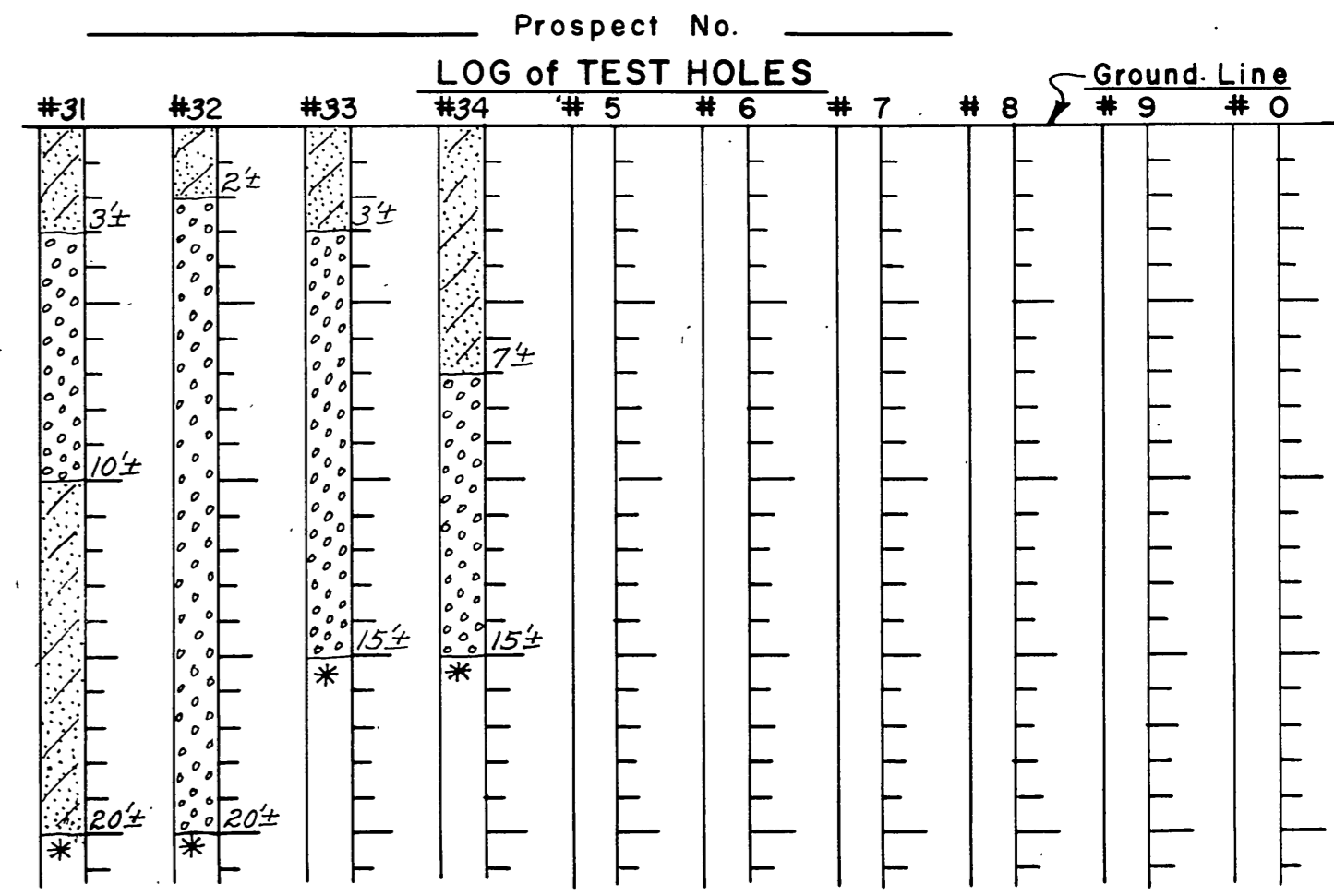
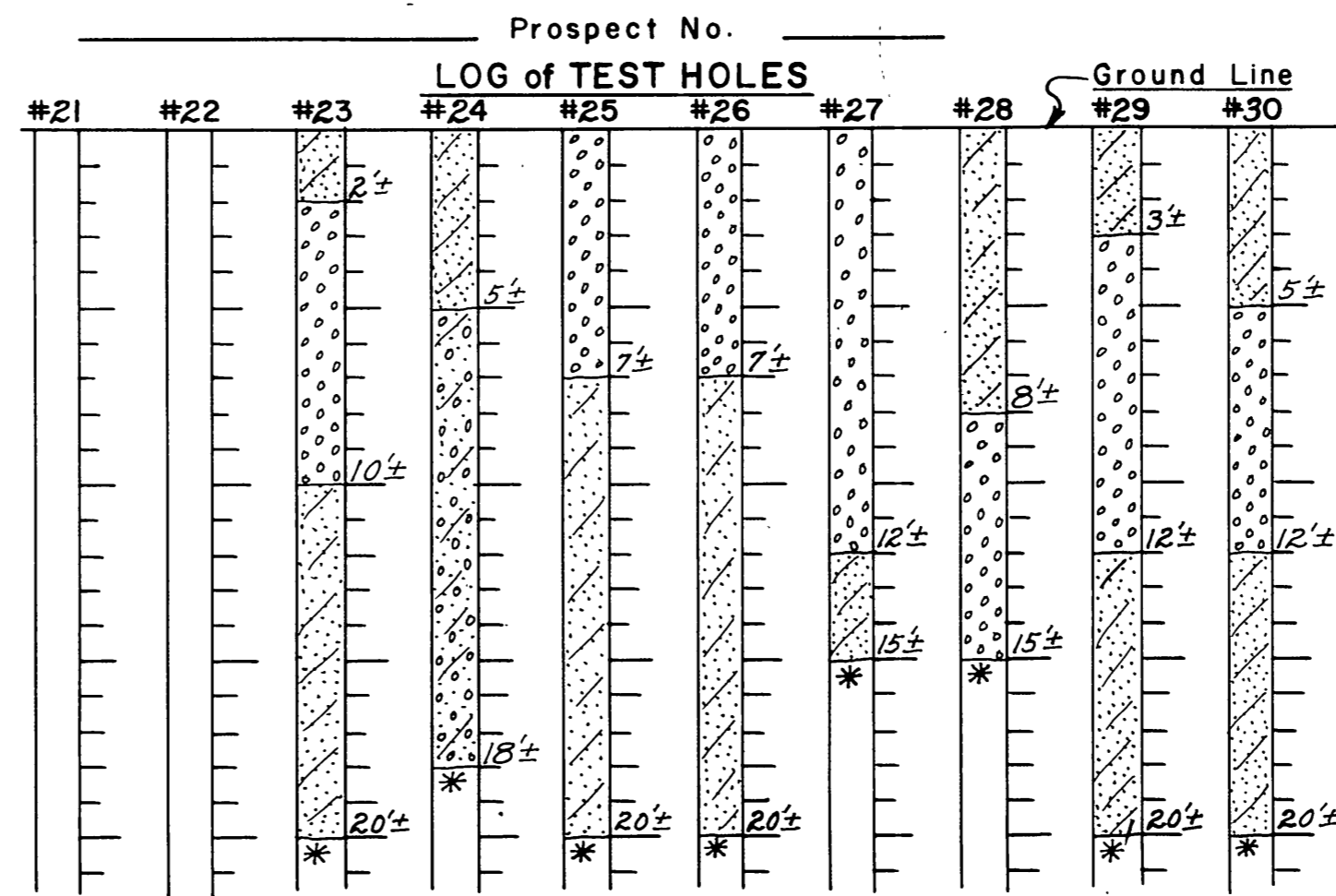
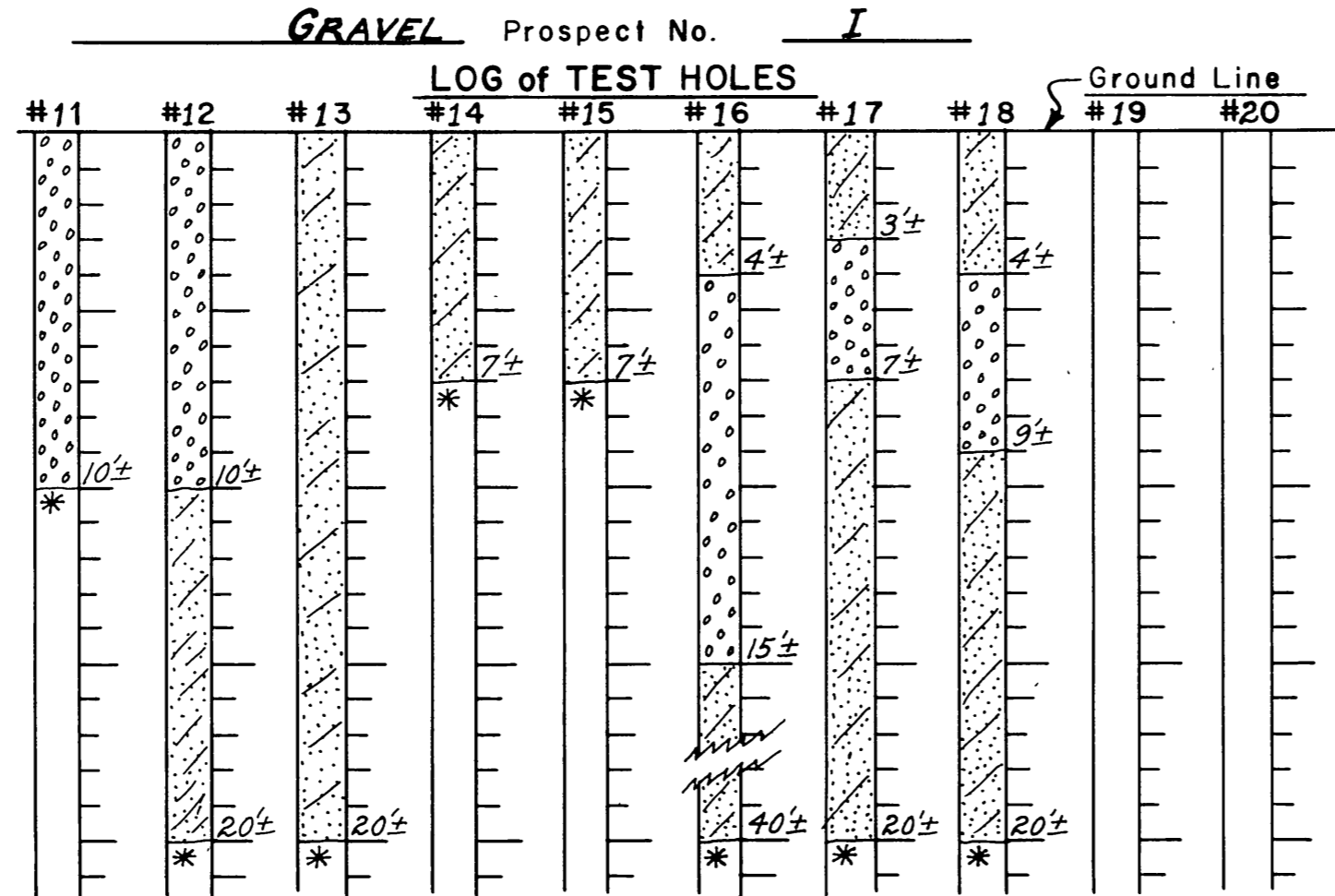
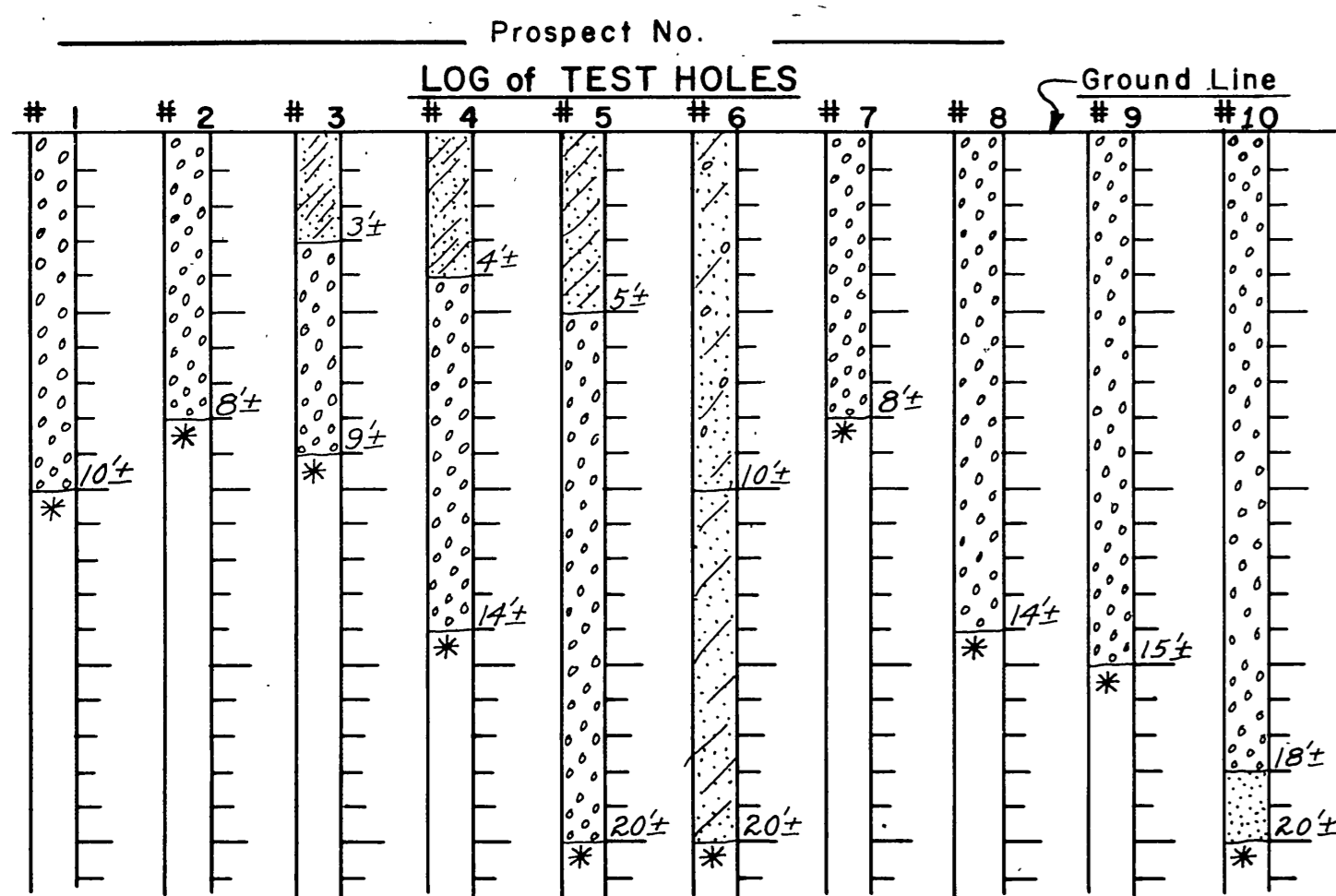


B. & M. Scale: 1" = Contour Interval =

R-2-W. S.L.B. & M. Scale: 1" = 500' Contour Interval =

B. & M. Scale: 1" = Contour Interval =

B. & M. Scale: 1" = Contour Interval =



NOTE: FOR CONSTRUCTION CHANGES SUMMARY SHEET
SEE BASIC CONSTRUCTION PLANS NO D-25

I-15-7(58)332
I-15-8(20)334
I-15-8(18)334
Rev 8-30-63
REV 9-16-63

SUMMARY SHEET

SURFACING table with columns for PROJECT, LOCATION, STATION, LENGTH, GRAVEL MATERIAL, and BITUMINOUS MATERIAL.

LINEAR SUMMARY table with columns for REMARKS, PROJECT NUMBER, LINE, STATION, MAIN LINE FEET, RAMP ROAD, CROSS ROAD, and FRONTAGE ROAD.

DEEP BEAM GUARD RAIL table with columns for LINE, STATION TO STATION, LT., RT., and REMARKS.

* MC 400 @ 0.15 Gal/yd²

6' CHAIN LINK FENCE table with columns for LINE, STATION TO STATION, LINEAR FT., and REMARKS.

1-15-7 (58) 332
1-15-8 (20) 334
16-15-8 (18) 334

Revised Aug. 30, 1963
Revised Oct. 7, 1963

SUMMARY SHEET

WATERING, ROLLING & COMPACTION		WATERING, ROLLING & COMPACTION (CONT.)	
DESCRIPTION	USE	DESCRIPTION	USE
WATERING		ROLLING, PNEUMATIC TIRED OR POWER ROLLER	
<i>All Embankment</i>		<i>Riverdale Road Acceleration Lane</i>	
I-15-7 (58) 332 533, 326 x 40 = 21, 333, 040 Gals.	22, 400 M-Gals.	Portland Cement Treated Base Course	
IG-15-8 (18) 334 578, 456 x 40 = 23, 138, 240 Gals.	24, 300 M-Gals.	(44.17 + 45.5') x 600' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 3 Hrs.	
I-15-8 (20) 334 412, 433 x 40 = 16, 497, 320 Gals.	17, 300 M-Gals.	Gravel Base Course	
		(46.83' + 47.83') x 600' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 3 Hrs.	
<i>All Gravel Material</i>		TOTAL 6 Hrs.	10 Hrs.
24, 337 Tons x 0.53 x 40 = 51, 594 Gals.	600 M-Gals.	"F-5" Acceleration Lane	
GRAND TOTAL	64, 600 M-Gals.		
COMPACTION		MECHANICAL TAMPING	
I-15-7 (58) 332		(25.67' + 27') x 875' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 3 Hrs.	
Total Embankment 533, 326 cu. Yds.	587, 000 cu. Yds.	(28.33' + 29.33') x 875' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 3 Hrs.	
I-15-8 (20) 334 412, 433 cu. Yds.	454, 000 cu. Yds.	TOTAL 6 Hrs.	10 Hrs.
IG-15-8 (18) 334 578, 456 cu. Yds.	636, 000 cu. Yds.	I-15-8 (20) 334 Project Total	740 Hrs.
GRAND TOTAL	1, 677, 000 cu. Yds.	GRAND TOTAL	1 070 Hrs.
ROLLING PNEUMATIC TIRED OR POWER ROLLER		ROLLING PNEUMATIC TIRED OR POWER ROLLER	
"V" Line		I-15-7 (58) 332 700 cu. Yd. ÷ 8 cu. Yd./Hr. = 87.5 Hrs.	88 Hrs.
		I-15-8 (20) 334 350 cu. Yd. ÷ 8 cu. Yd./Hr. = 43.8 Hrs.	44 Hrs.
		IG-15-8 (18) 334 230 cu. Yd. ÷ 8 cu. Yd./Hr. = 28.6 Hrs.	29 Hrs.
		GRAND TOTAL	161 Hrs.
<i>Gravel Surface Type "A"</i>			
76' x 2 x 2192' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 206 Hrs.			
<i>Gravel Base Course</i>			
79' x 2192' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 107 Hrs.			
I-15-7 (58) 332 Project Total = 313 Hrs.	330 Hrs.		
5600 South Street			
<i>Gravel Surface Type "A" & Gravel Base Course</i>			
4' x 44' x 900' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 98 Hrs.			
TOTAL 98 Hrs.	110 Hrs.		
<i>"R-7," "R-8," "R-9" & "R-10" Lines</i>			
<i>Gravel Surface Type "A"</i>			
32' x 2 x 8632.22' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 341 Hrs.			
<i>Gravel Base Course</i>			
35' x 8632.22' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 186 Hrs.			
TOTAL 527 Hrs.	560 Hrs.		
<i>"F-5" Line Sta. 10+00 to 17+88.16 16' Median</i>			
Sta. 17+88.16 to 57+08.35 0' Median			
<i>Gravel Surface Type "A"</i>			
(46' + 48') x 788.16' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 5 Hrs.			
(30' + 32') x 3920.19' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 15 Hrs.			
<i>Gravel Base Course</i>			
50' x 788.16' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. = 2 Hrs.			
34' x 3920.19' ÷ 9 ft. ² ÷ 1800 Yd. ² /Hr. = 8 Hrs.			
TOTAL 30 Hrs.	40 Hrs.		
<i>"F-5A" Line</i>			
<i>Gravel Surface & Gravel Base</i>			
(18' + 20' + 22') x 287' ÷ 9 ft. ² /Yd. ² ÷ 1800 Yd. ² /Hr. 1 Hr.			
TOTAL 1 Hrs.	10 Hrs.		

1-15-7 (58) 332
1-15-8 (20) 334
16-15-8 (18) 334

Revised Aug. 30, 1963
Revised Sept. 16, 1963
Revised Oct. 7, 1963

SUMMARY SHEET

MEDIAN CURB TYPE "E"			
LINE	STATION		LINEAR FEET
	FROM	TO	
"V"	10+60	14+43	667
"V"	15+27	19+18	815
"V"	19+75	21+69	430
TOTAL			1912
USE			1910
5600 SO.	4+20	6+15	397
5600 SO.	6+74	11+33	940
5600 SO.	12+52	13+20	140
"F-5"	10+60	17+02	1380
TOTAL			2857
USE			2860
PROJECT TOTAL			4770

SUMMARY OF ITEMS					
DESCRIPTION PROJECT	UNIT	QUANTITY	QUANTITY	QUANTITY	TOTAL
		I-15-7(58) 332	I-15-8 (20) 334	IG-15-8 (18) 334	
Furnishing Construction Signs	Lump	Lump	Lump	Lump	Lump
Removal of Trees	Each	0	4127	1530	30
Flagging	Man Hr.	600	1000	0	1600
Unclassified Roadway Excavation	Cu. Yd.	303,000	1,248,000	0	1,551,000
Excavation for Structures Unclassified	Cu. Yd.	1100	450	100	1650
Demolition of Buildings, Basements, Foundations and Slabs	Lump	Lump	Lump	Lump	Lump
Mechanical Tamping	Hour	88	44	29	161
Imported Borrow	Cu. Yd.	53,400			53,400
Compaction Method "B"	Cu. Yd.	560,000	433,000	607,000	1,600,000
Rolling Pneumatic Tired or Power Roller	Hour	330	740	0	1070
Class "A" Overhaul	Sq. Yd.	3,278,000	2,920,000	6,073,000	12,271,000
Class "B" Overhaul	Yd. Mile	918,000	107,000	446,000	1,471,000
Furnishing Water Equipment	Lump	Lump	Lump	Lump	Lump
Watering	M-Gal.	22,400	17,900	24,300	64,600
Right of Way Fence 6' Chain Link	Lin. Ft.	24,865	21,069	4438	50,372
12' Gates Chain Link	Each	2			2
12" Reinforced Concrete Pipe	Lin. Ft.		300		300
24" Reinforced Concrete Pipe	Lin. Ft.	1434	5560	508	7502
30" Reinforced Concrete Pipe	Lin. Ft.	198			198
24" Reinforced Concrete End Sections	Each	22	21	4	47
30" Reinforced Concrete End Sections	Each	4			4
Gravel Base Course	Ton	1600		9600	11,200
Portland Cement Treated Base Course	Ton		4000		4000
Portland Cement Type I	Bag		1700		1700
Bituminous Material MC-400	Ton	20	90		110
Bituminous Material MC-800	Ton	10	50		60
Bituminous Material MC-70	Ton	10	40		50
Gravel Surface Type "A"	Ton	2660	14,400		17,060
Bituminous Material 60-70 Penetration	Ton	90	480		570
Bituminous Surface Course 1" Max.	Ton	1590	9265		10,855
Cover Material Type "A"	Ton	110	530		640
Surface Ditches	Lin. Ft.		1700	1900	3600
Small Ditch Excavation	Cu. Yd.		130	100	230
Concrete Curb "E"	Lin. Ft.	1910	2860		4770
Deep Beam Highway Guard Rail	Lin. Ft.	187	249		436
Metal Bin Type "A" Wall	Sq. Ft.			1000	1000
Metal Bin Type "B" Wall	Sq. Ft.			762	762
Right of Way Markers	Each	30	30	4	64
Obliteration of Old Road	Mile	0.5			0.5
Drop Inlet Type "C"	Each	2	9	3	14
Excavation and Stockpiling Topsoil	Cu. Yd.	0	20,000	0	20,000
Concrete Class "A" Air Entaining	Cu. Yd.		30		30
Reinforcing Steel	Lbs.		3300		3300
Structural Steel	Lbs.		4800		4800
Hydrated Lime	Ton	16	90		106

MISCELLANEOUS				
DESCRIPTION OF ITEM	I-15-7 (58) 332	I-15-8 (20) 334	IG-15-8 (18) 334	TOTAL
Furnishing Construction Signs	Lump	Lump	Lump	Lump
Flagging	600 Mn Hr.	1000 Mn Hr.	0	1600 Mn Hr.
Furnishing Water Equipment	Lump	Lump	Lump	Lump
12' Gates Chain Link	2 EA	0		2 EA
Metal Bin Type Retaining Wall Type "A"	0	0	1000 SF	1000 SF
Metal Bin Type Retaining Wall Type "B"	0	0	762 SF	762 SF
Right of Way Markers	30 EA	30 EA	4 EA	64 EA
Obliteration of Old Road	0.5 Mi.	0	0	0.5 Mi.
Excavation and Stockpiling Topsoil	0	20,000 cY	0	20,000 cY
Demolition of Buildings, Basements, Foundations and Slabs.	Lump	Lump	0	Lump
Removal of Trees	0	1530 EA	30 EA	1560 EA

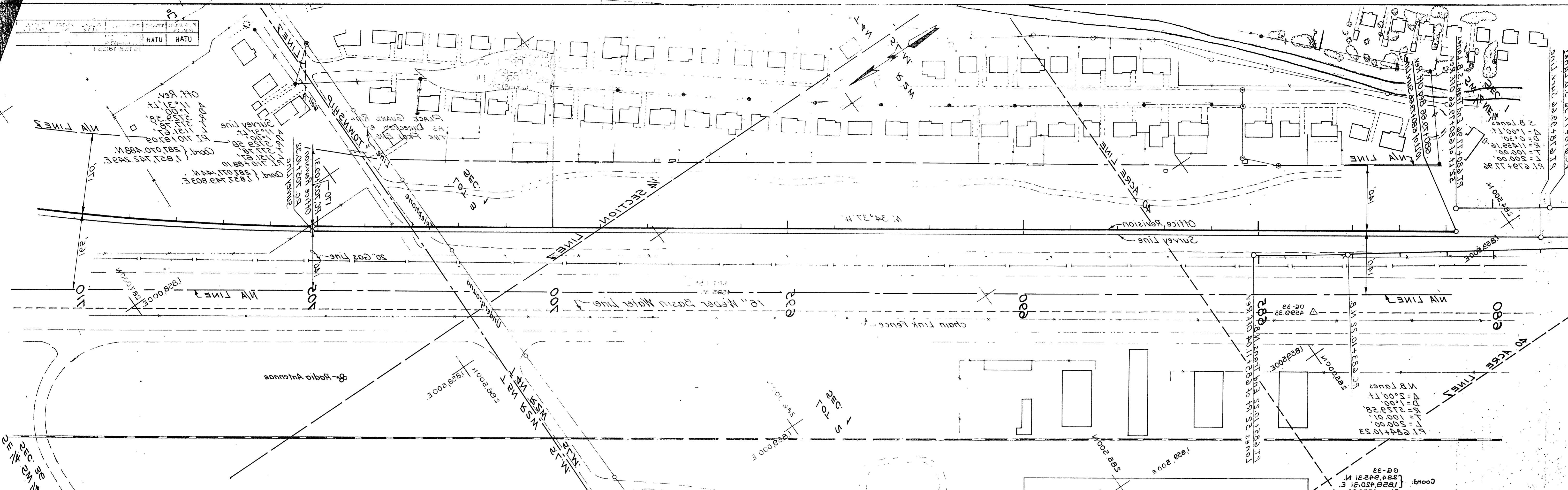
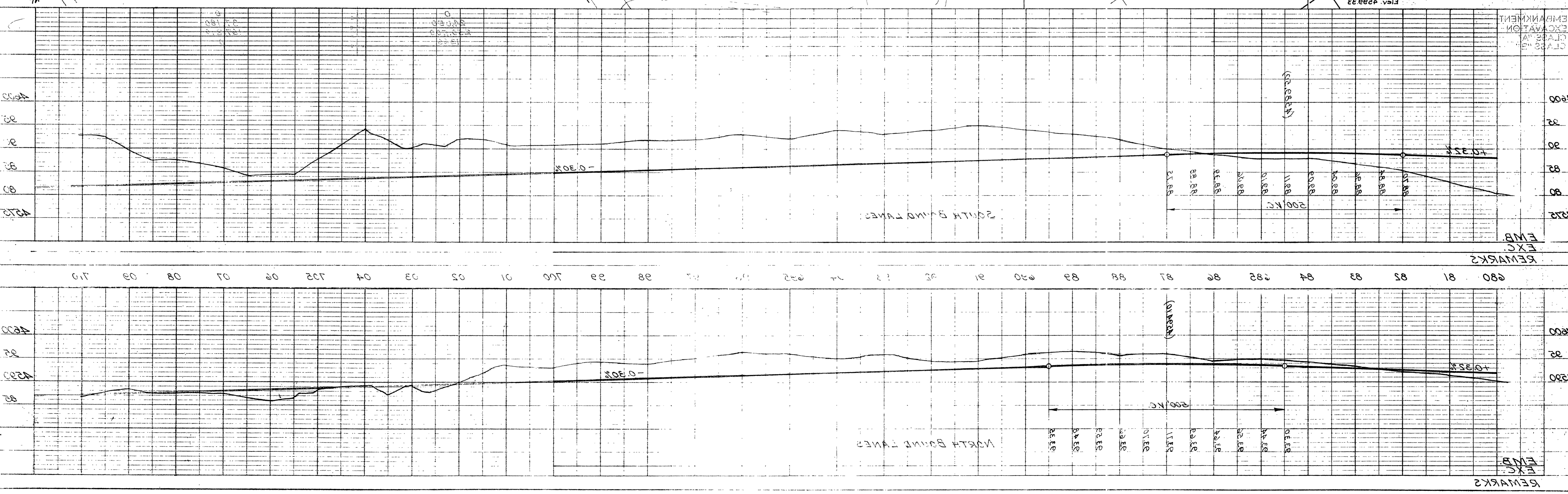
SUMMARY SHEET

FED. ROAD DIST. NO.	PROJECT NO.	SHEET NO.	TOTAL SHEETS
UTAH		3 E	

1-15-7(58)332
1-15-8(20)334
1-15-8(18)334
Rev. AUG. 30, 1963

DRAINAGE

LINE	STATION	DESCRIPTION	MEDIAN DROP INLET REQ'D EACH	R.C.P. REQ'D. LIN. FT.			R.C.P. END SEC. REQ'D. LF.			SURFACE DITCH REQ'D. - LIN. FT.				STRUCT. EXC. CU. YDS.	MECH. TAMPING CU. YD.	SMALL DITCH EXC. CU. YD.	CLASS "A" CONCRETE CU. YD.	REIN. STEEL LBS.	STRUCT. STEEL LBS.	STANDARD DRAWING AND LINE NUMBER	REMARKS
				24"	30"	48"	24"	30"	12"	STA.	STA.	LT.	RT.								
Main	721 + 80	Drop Inlet - Cross Drainage	1	250			2						93	55						346 C	
"U"	17 + 35	Surface Drainage		78			2							4							
"T"	15 + 85	Surface Drainage		68			2							4							
Main	763 + 65	Drop Inlet	1	300									82	96						346 C	Drop Inlets Req'd on E. Connect Into New 24" Storm Sewer.
	784 + 00	Cross Drainage		192			4						185	80							
	795 + 00	Cross Drainage			198		4						180	185							
	806 + 00	Cross Drainage		174			4						142	72							
	817 + 00	Cross Drainage		178			4						145	72							
	827 + 00	Cross Drainage		194			4						187	80							
	TOTAL	I-15-7 (58) 332	2	1434	198		22	4					1014	648							
	USE		2	1434	198		22	4					1100	700							
Main	849 + 00	Drop Inlet - Cross Drainage	1	234			2			849+00	859+00	Lt.	100'	75	96					346 C	Drop Inlet Req'd in Median
	860 + 00	Cross Drainage	1	274			2			860+00	869+00	Lt.	90'	112	100					346 C	Drop Inlet Req'd in Median
	TOTAL	IG-15-8 (18) 334	3	508			4						1900	75	208	100					
	USE		3	508			4						1900	100	230	100					
	839 + 50	Drop Inlet - Cross Drainage	1	192			2			836+15	839+50	Lt.	335							346 C	Drop Inlet Req'd in Median
	880 + 00	Median Drain S.B. Lane	1	124			1			871+00	879+00	Lt.	800								
	890 + 00	Drop Inlet - Cross Drainage	1	156			2							72	64					346 C	Drop Inlet Req'd in Median
"F-3"	13 + 85	Cross Drainage		124										51							
										10+60	15+50	Lt.	490								
"F-5"	29 + 50	Cross Drainage at 890+00 ± I-15		76			2						56	31							
Main	900 + 00	Ditch & Median Drainage	1	178			2						17	10		3.4	420	620	V-753 346	C	C.B. 8' Lt. Line 2 C.B. 8' Rt. Line 1
	900+00 to 909+50	Storm Sewer Along Median E.		950																	
	909 + 50	Drop Inlet Cross Drainage	1	750			2						17	10		3.4	420	620	V-753 346	C	C.B. 8' Lt. Line 2 C.B. 8' Rt. Line 1
	919 + 00	Cut Ditch & Median Drainage	1	144			2						18	10		3.8	457	620	V-753 346	C	C.B. 8' Lt. Line 3 C.B. 8' Rt. Line 2
	919+00 to 927+50	Storm Sewer Along Median E.		850																	
	927 + 50	Drop Inlet - Cross Drainage	1	144			2						18	10		3.6	432	620	V-753 346	C	C.B. 8' Lt. Line 2 C.B. 8' Rt. Line 2
	938 + 00	Drop Inlet - Cross Drainage	1	144			2						17	10	18	3.4	420	620	V-753 346	C	C.B. 8' Lt. Line 2 C.B. 8' Rt. Line 1
	950 + 00	Cut Ditch & Median Drainage	1	144			2						17	10		3.4	420	620	V-753 346	C	C.B. 8' Lt. Line 2 C.B. 8' Rt. Line 1
	950+00 to 963+00	Storm Sewer Along Median E.		1300																	
	963 + 00	Drop Inlet - Cross Drainage	1	144			2						17	10	12	3.4	420	620	V-753 346	C	C.B. 8' Lt. Line 2 C.B. 8' Rt. Line 1 Outlet Ditch Req'd.
"R-10"	14 + 20	Cross Drainage at 919+00		40			2														
Main	881+00 to 908+00	Pave Median & Cut Ditches																			
Main	932+00 to 964+00	Pave Median & Cut Ditches																			
"R-10"	10 + 30	Irrigation		100									52	34							
Main	912+40 to 915+00	Irrigation													93						Small Ditch Req'd. Outside N/A Line
Main	945 to 948 Rt.	Irrigation											83								
	TOTAL	I-15-8 (20) 334	9	5560	198		21	300					1625	384	302	123	24.4	2989	4340		
	USE		9	5560	198		21	300					1700	450	350	130	30	3300	4800		
	GRAND TOTAL		14	7502	198		47	4	300				3600	1650	1280	230	30	3300	4800		



REMARKS
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UTAH
 STATE ENGINEER
 LICENSE NO. 12345
 DATE

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
15	UTAH	1-15-75(8)334	196	6	
* REV 7-12-66 B					
15 818 334					
Rev 8-30-63					
Rev 9-16-63					
Rev 1-27-64					
Rev 3-24-64					

HILL FIELD INTERCHANGE

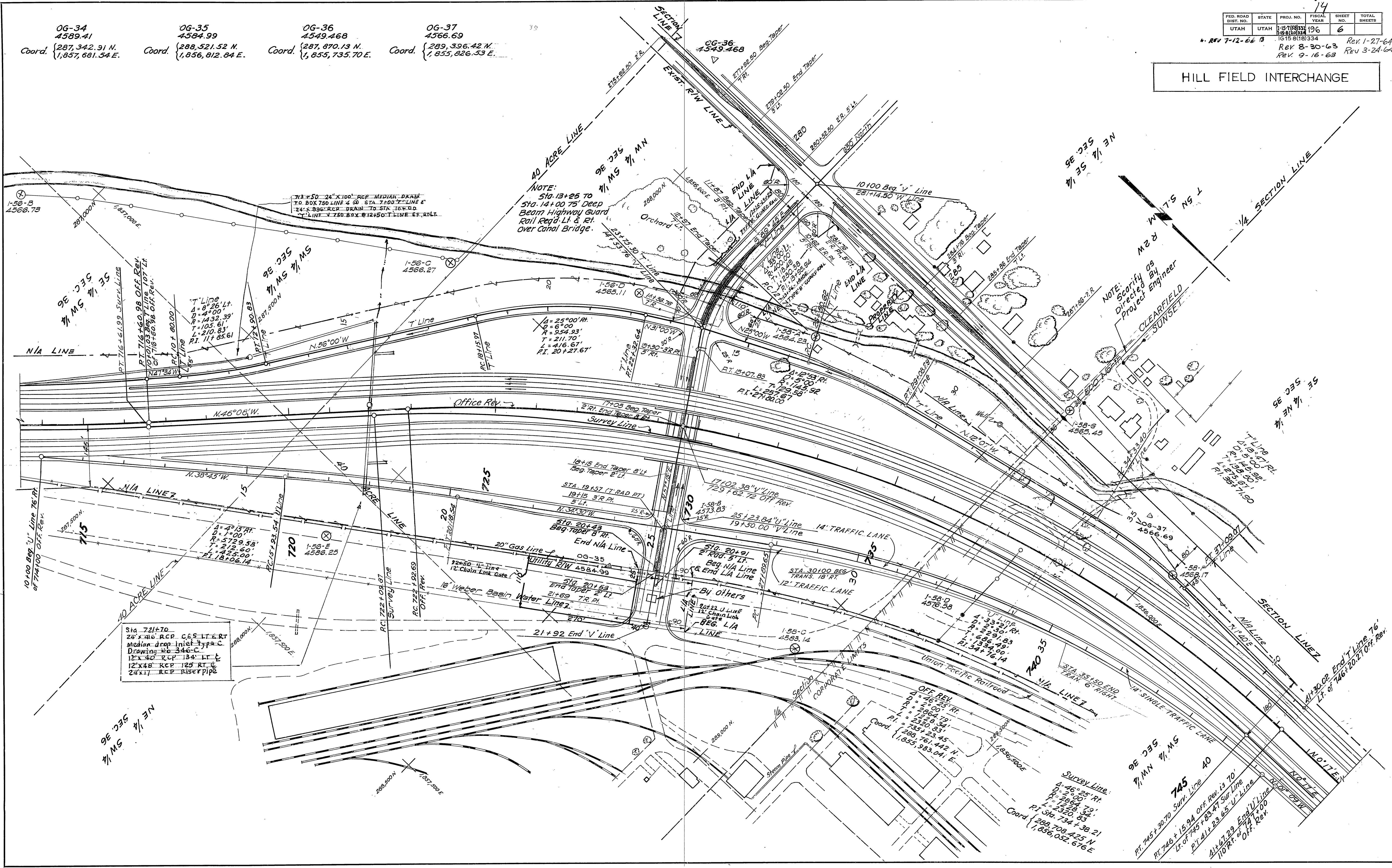
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1,857,681.54 E.

06-35
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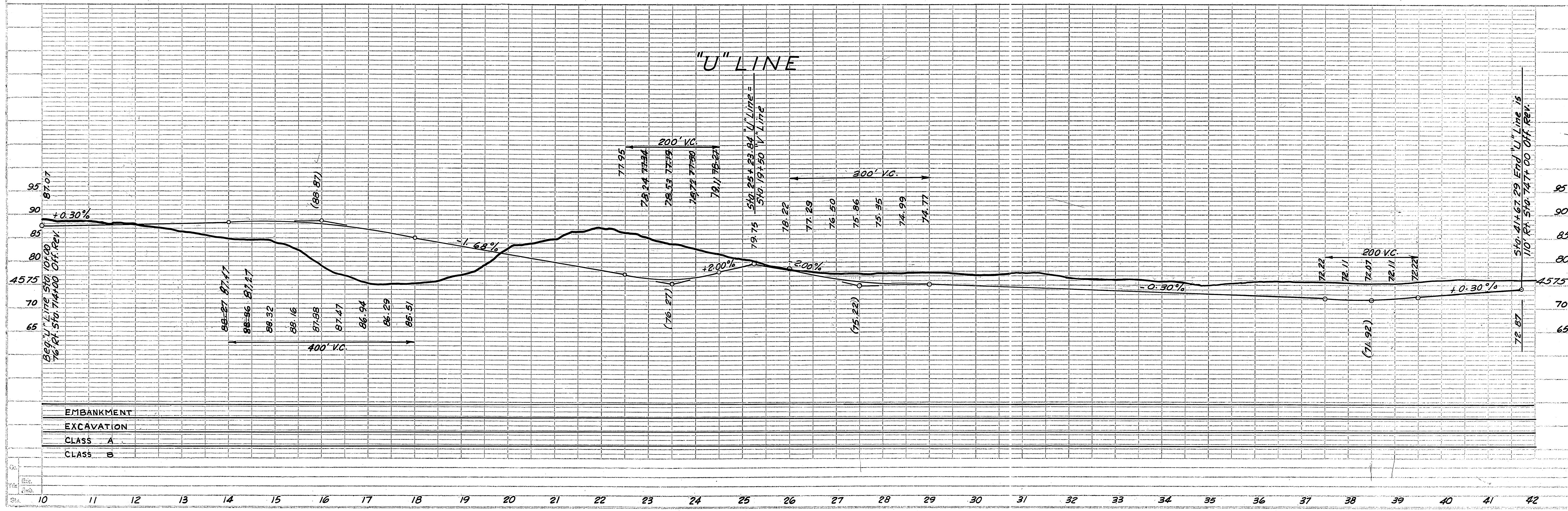
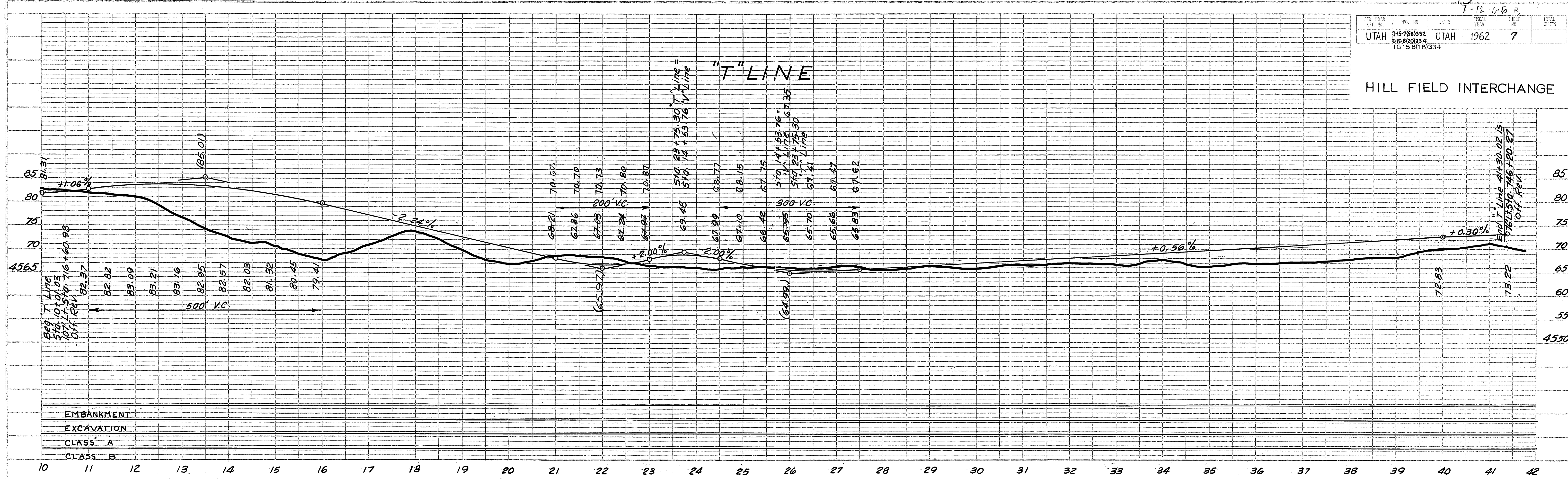
06-36
4549.468
Coord. {287,870.13 N.
1,855,735.70 E.

06-37
4566.69
Coord. {289,396.42 N.
1,855,826.53 E.

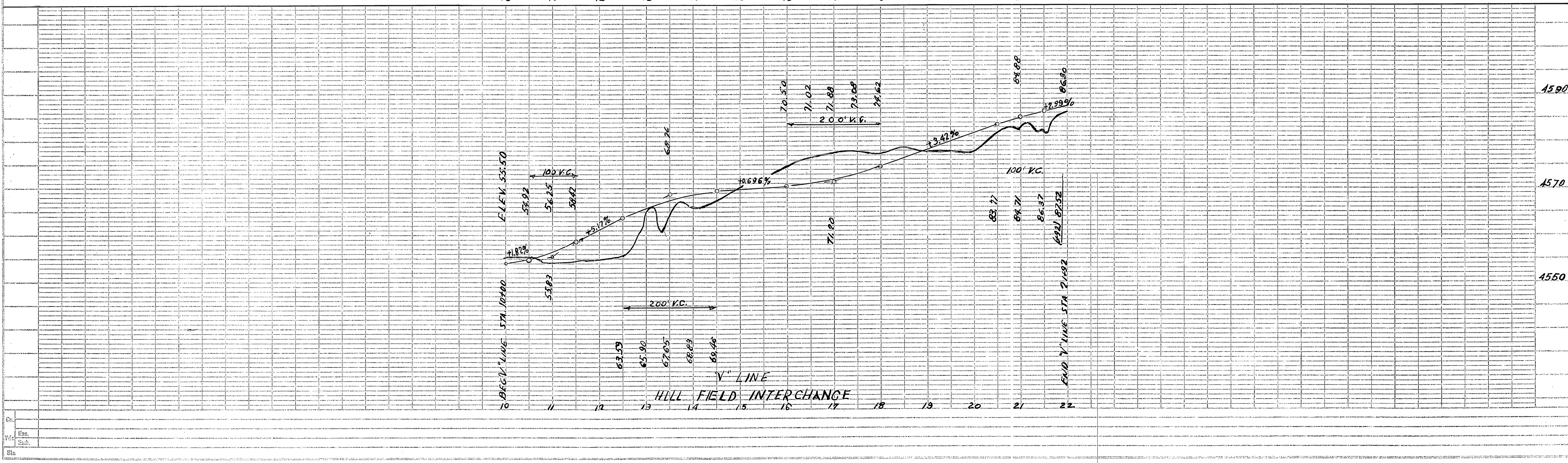
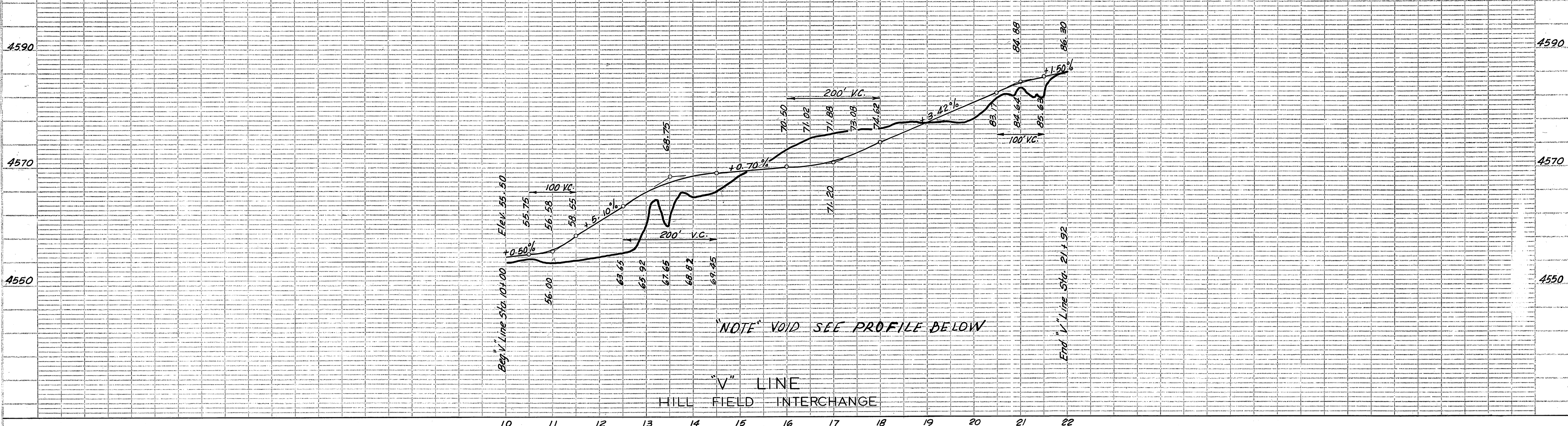
REVISIONS	BY	DATE
1	Y.F. Stevens	11-67



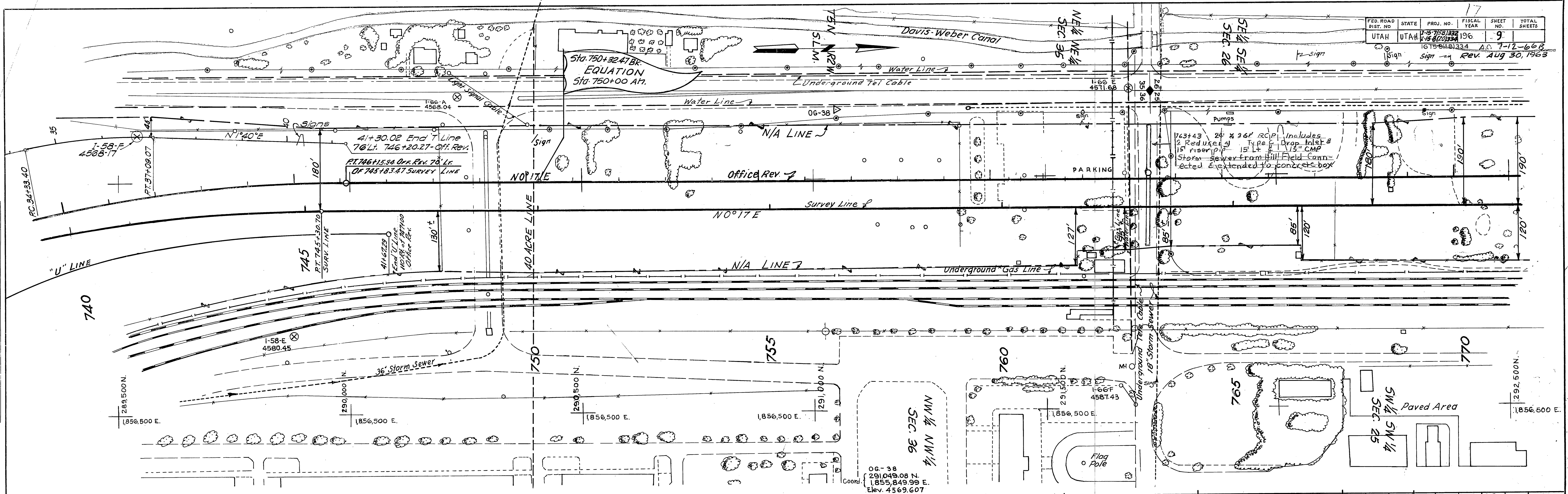
HILL FIELD INTERCHANGE



FED. ROAD DIST. NO.	PROJ. NO.	STATE	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	1-15-76(8)334			8	
A.C. REV. 7-18-66 B.					



FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	107-12-1334	196	9	
107-12-1334 AC 7-12-66B Rev. Aug 30, 1963					



EMBAKMENT	90.37	4653	4732	382	33.53	988	1707	1608	7438
EXCAVATION	9.637	1548	4732	167	33.53	21.69	1707	668	355
CLASS "A"		3120		2158		11810		9408	70830
CLASS "B"		3495		212		793		668	376

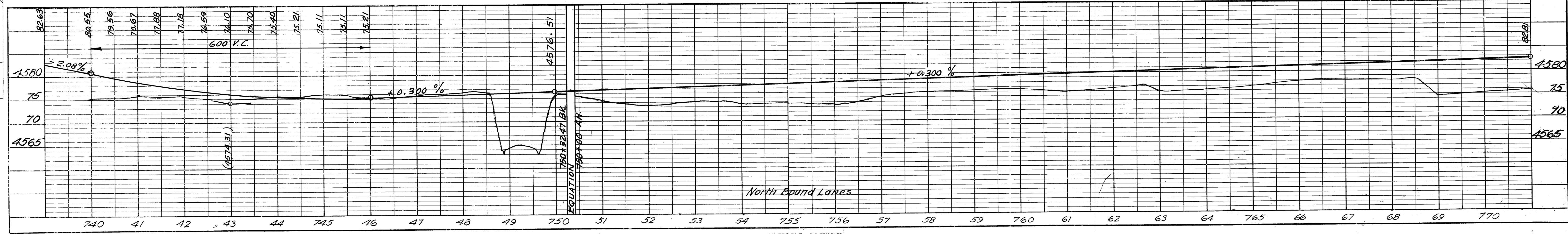
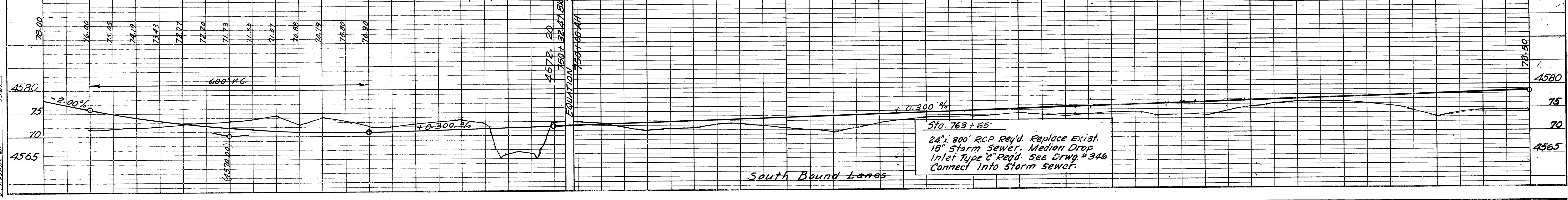
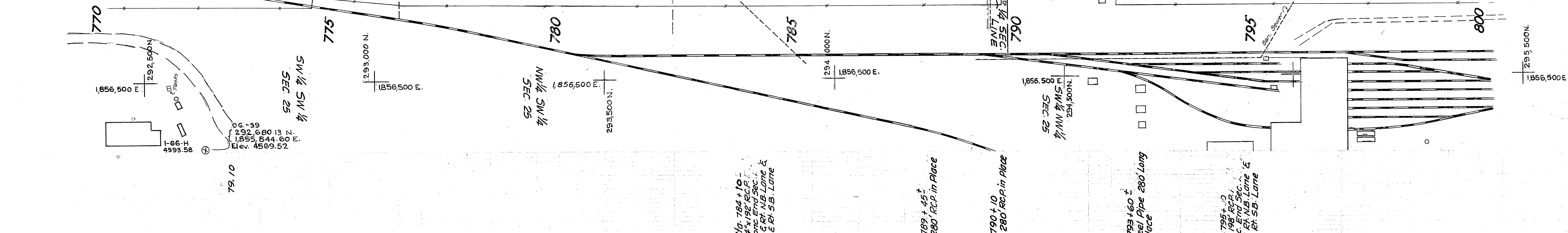
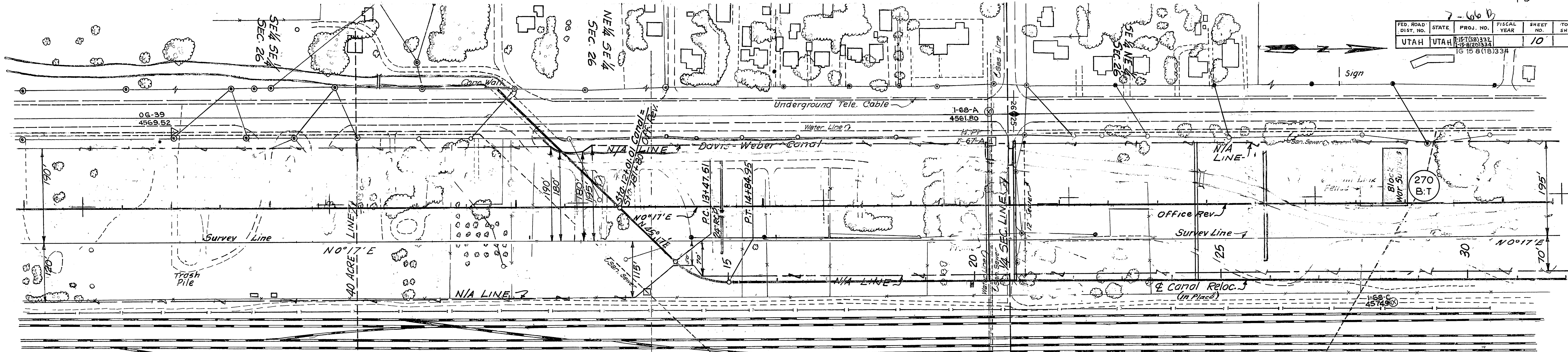


PLATE I - PLAN-PROFILE B. P. R. STANDARD
THE FREDERICK POST CO., CHICAGO

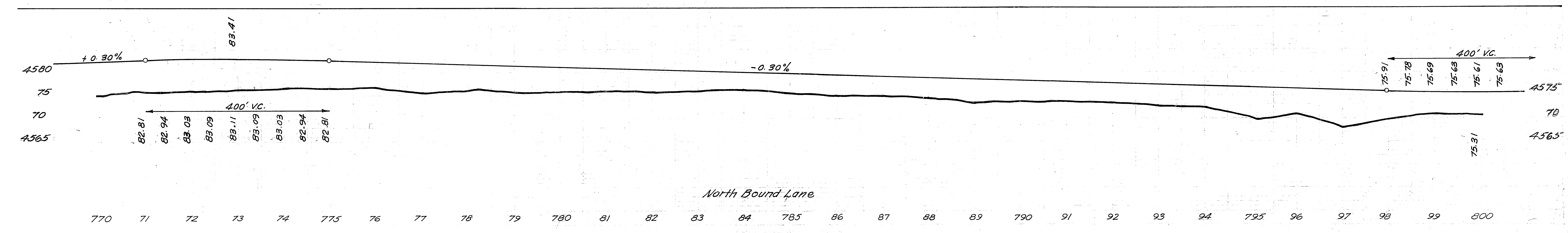
PLAN
SURVEYED
NOTE BOOK
ALIGNED CHECKED
BY
DATE
1961

PROFILE
SURVEYED
NOTE BOOK
CHECKED
BY
DATE
1961

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	158(18)334	1961	10	10



Station	78.50	78.63	78.72	78.78	78.80	78.78	78.72	78.63	78.50	75.91	75.78	75.69	75.63	75.61	75.63
55 EMBANKMENT															
EXCAVATION CLASS "A"															
CLASS "B"															
Area	13,281	256	50,798	0	19,767	19,767	10,304	23,329	0	0	0	8,055	15,138	0	0
Volume															
Station	78.50	78.63	78.72	78.78	78.80	78.78	78.72	78.63	78.50	75.91	75.78	75.69	75.63	75.61	75.63



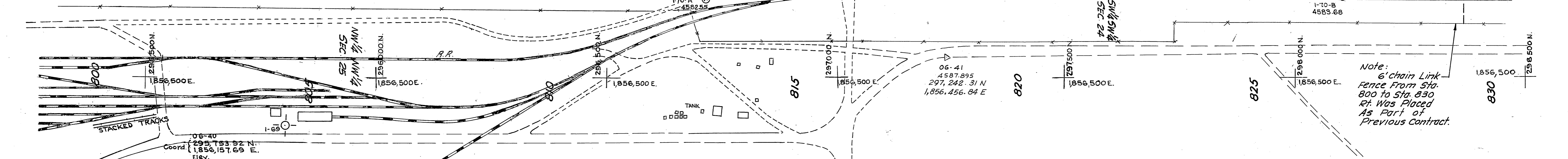
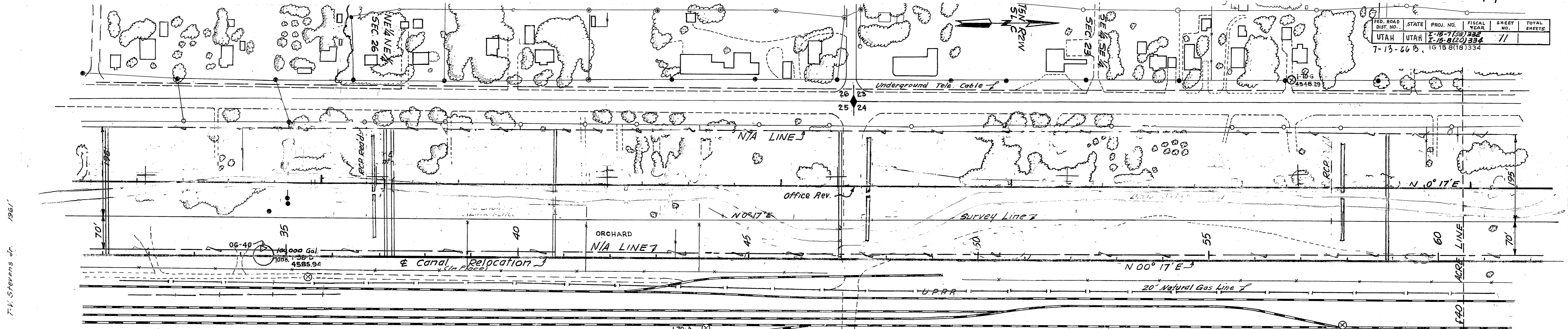
T.V. Stevens Jr. 1961

T.V. Stevens Jr. 1961

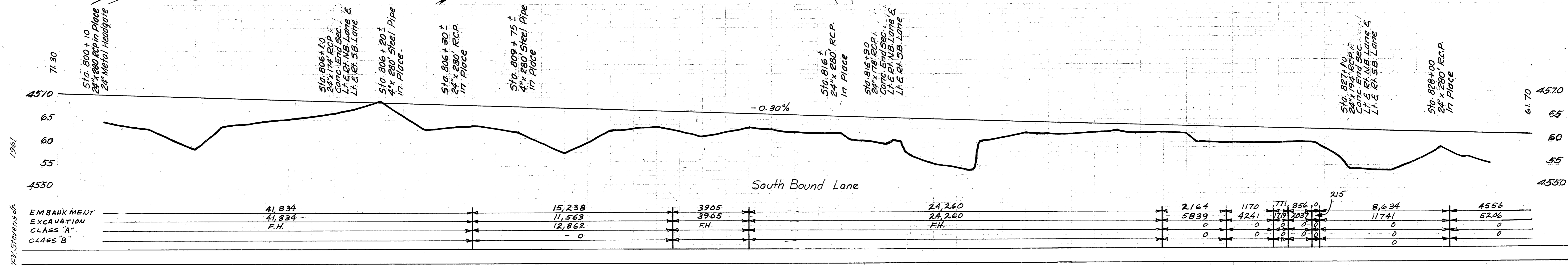
North Bound Lane

1-15 7(25) 24

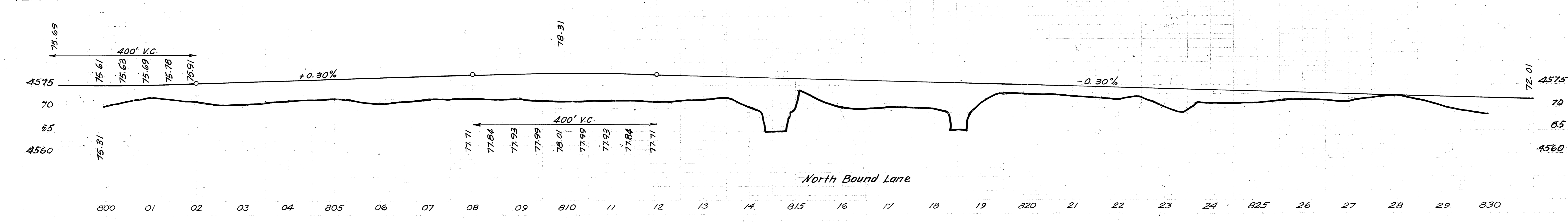
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	7-15-153	1966	71	111
		7-15-8(20)334			
		7-13-66 B.	IG 15 8(18)334		



Note:
6' chain Link Fence From Sta. 800 to Sta. 830 Rt. Was Placed As Part of Previous Contract.

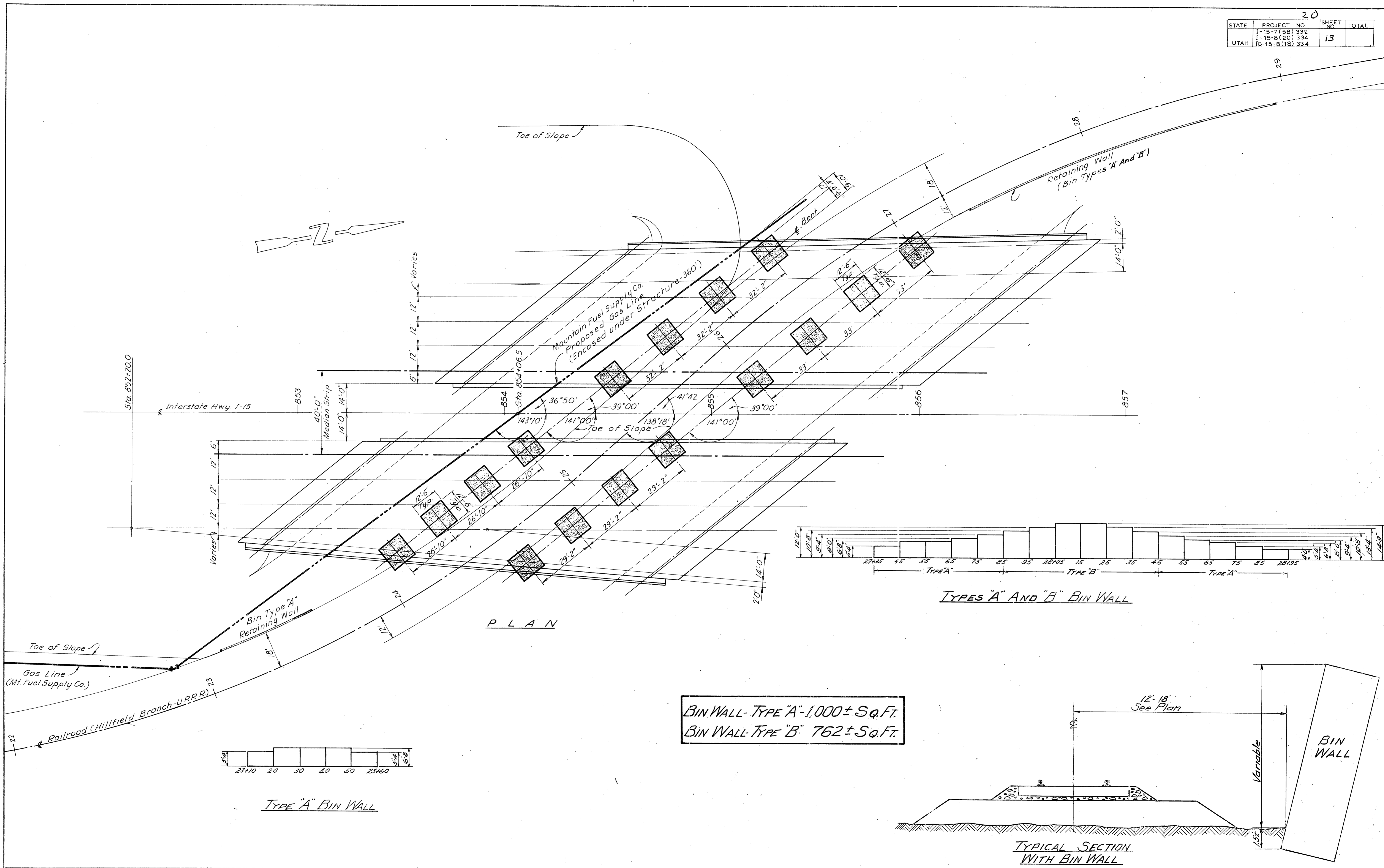


EMBAKMENT	41,834	15,238	3905	24,260	2164	1170	771	856	0	215	8,634	4556
EXCAVATION	41,834	11,563	3905	24,260	5839	4241	1719	2037	0	0	11741	5206
CLASS "A"	F.H.	12,862	F.H.	F.H.	0	0	0	0	0	0	0	0
CLASS "B"		0			0	0	0	0	0	0	0	0

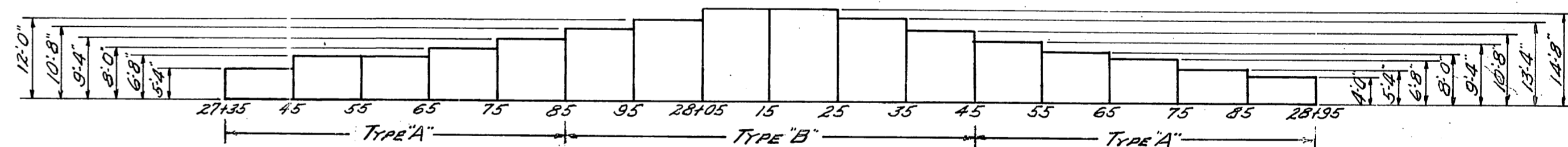


T.V. Stevens Jr. 1961

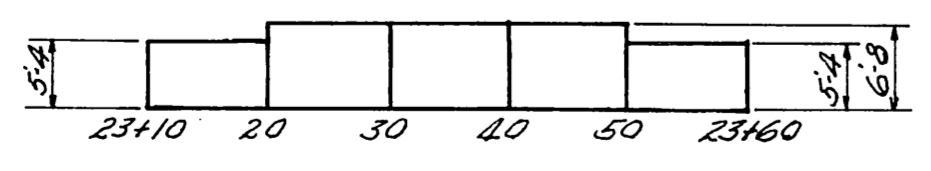
T.V. Stevens Jr. 1961



PLAN

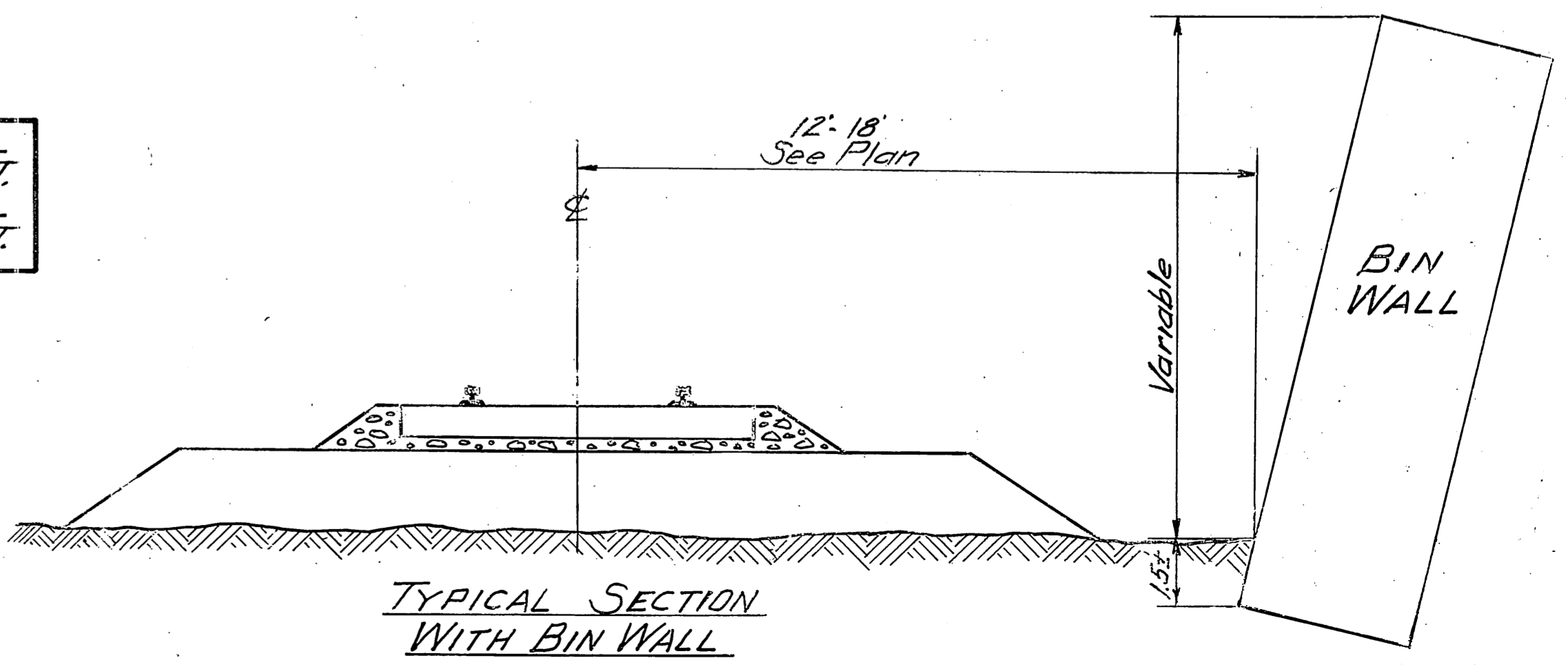


TYPES "A" AND "B" BIN WALL



TYPE "A" BIN WALL

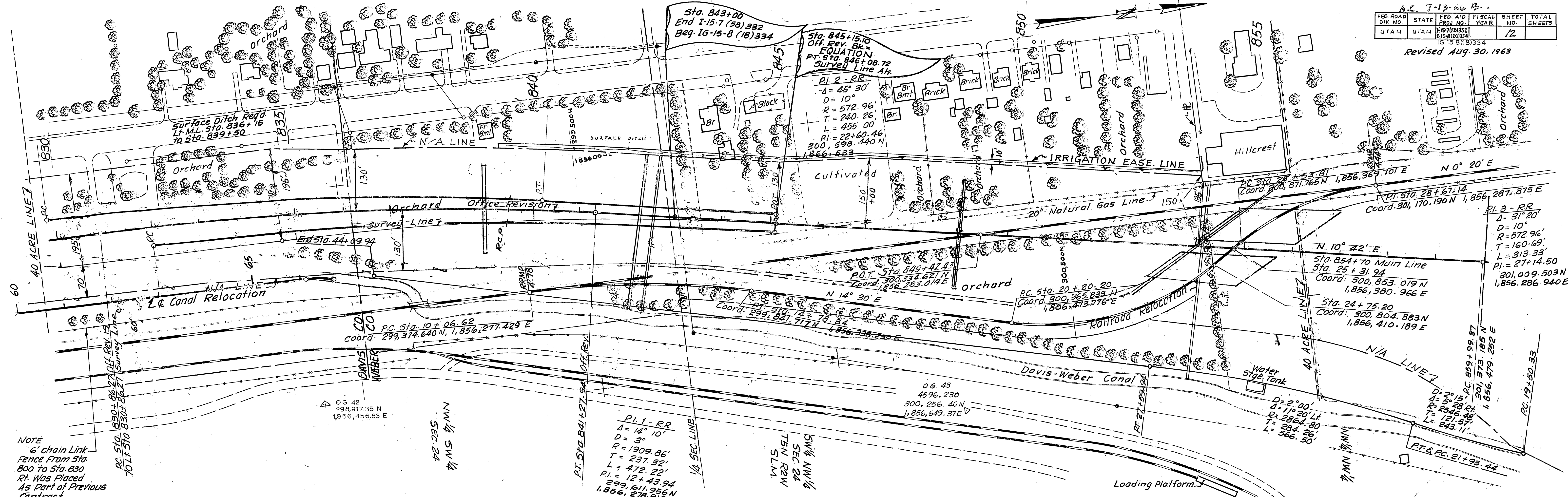
**BIN WALL- TYPE "A"-1,000± Sq.Ft.
BIN WALL- TYPE "B"- 762± Sq.Ft.**



TYPICAL SECTION WITH BIN WALL

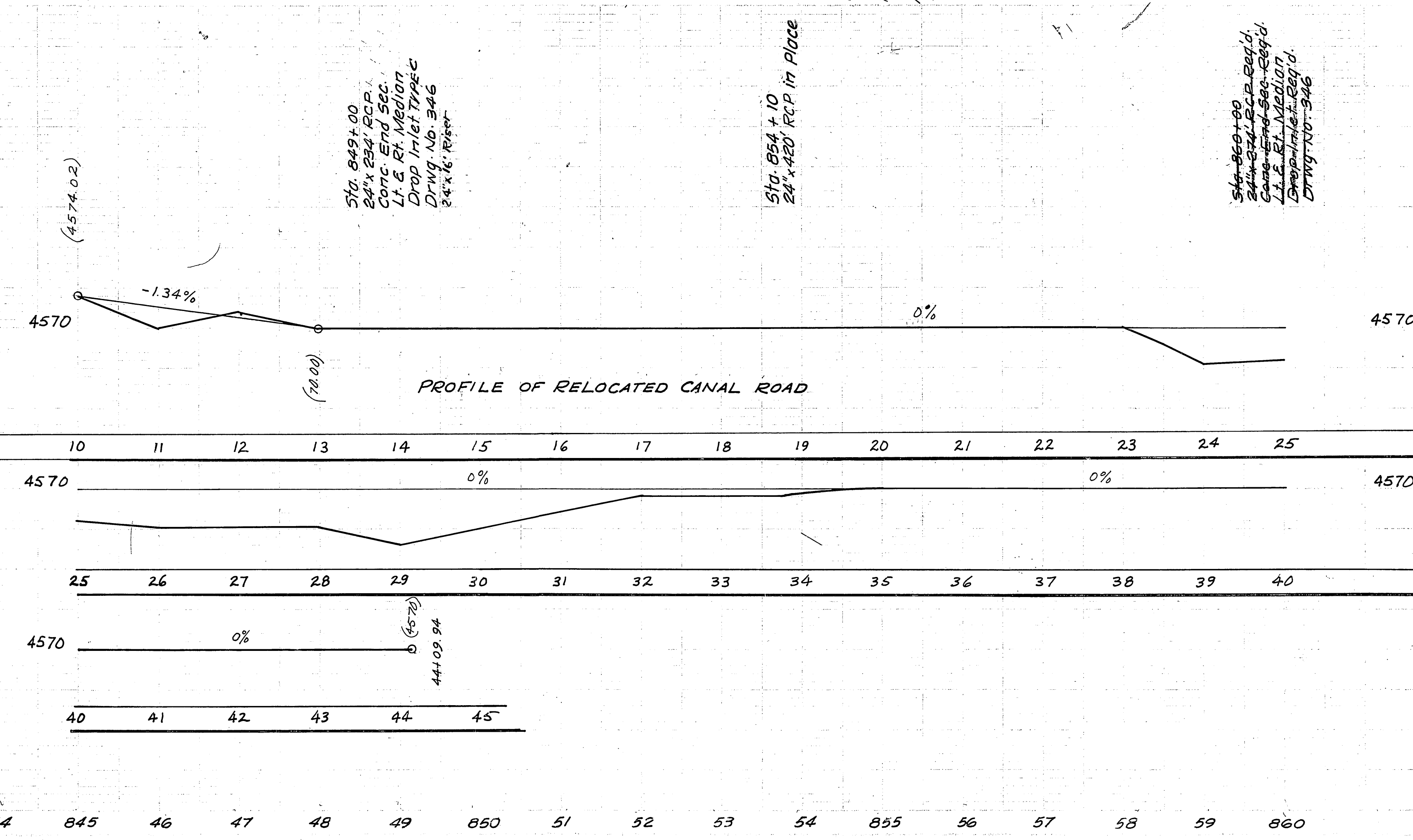
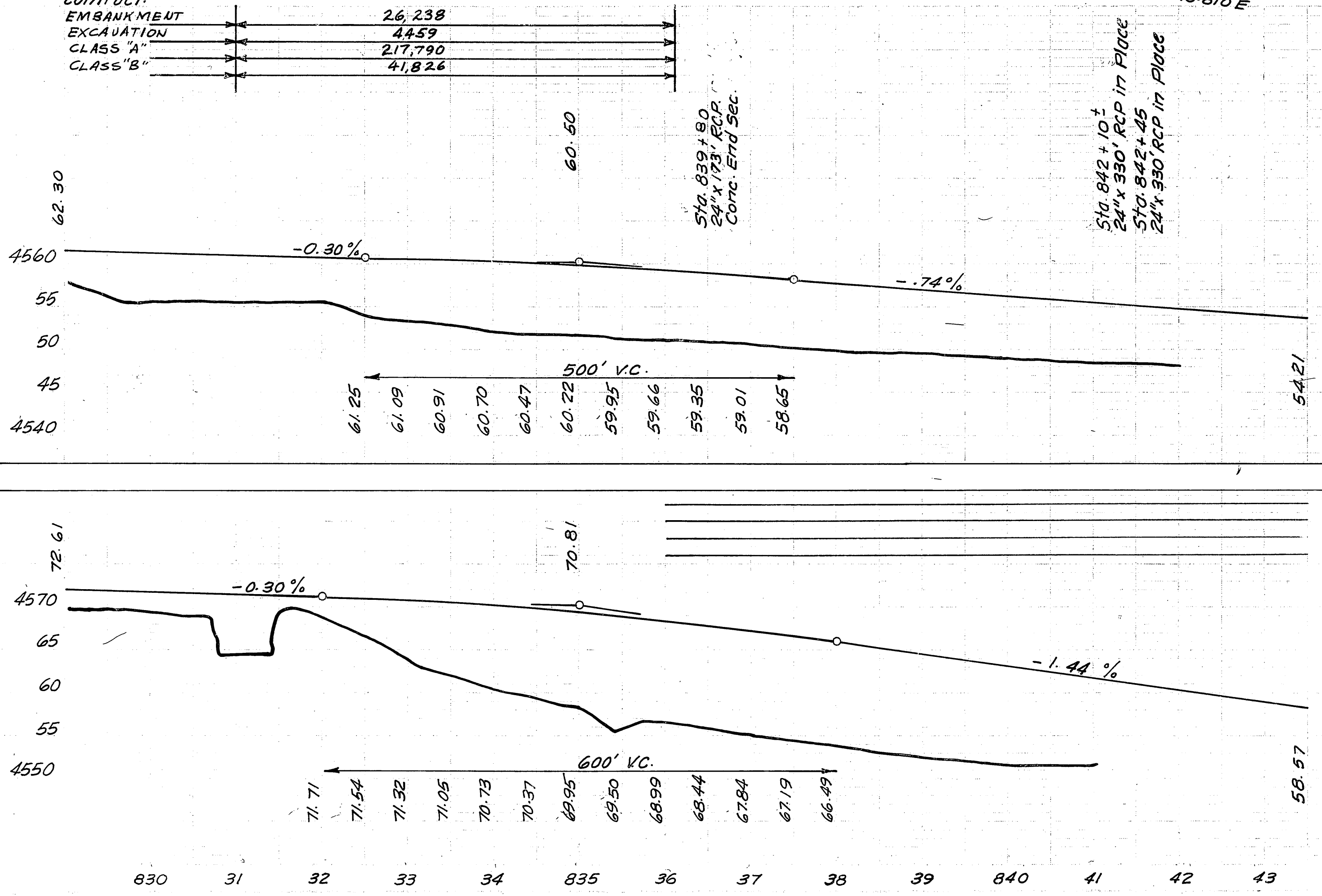
FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	45-100333-1	1963	72	

Revised Aug. 30, 1963



NOTE
6' chain Link Fence From Sta. 800 to Sta. 830 Pt. Was Placed As Part of Previous Contract.

EMBANKMENT	26,238
EXCAVATION	4459
CLASS "A"	217,790
CLASS "B"	41,826

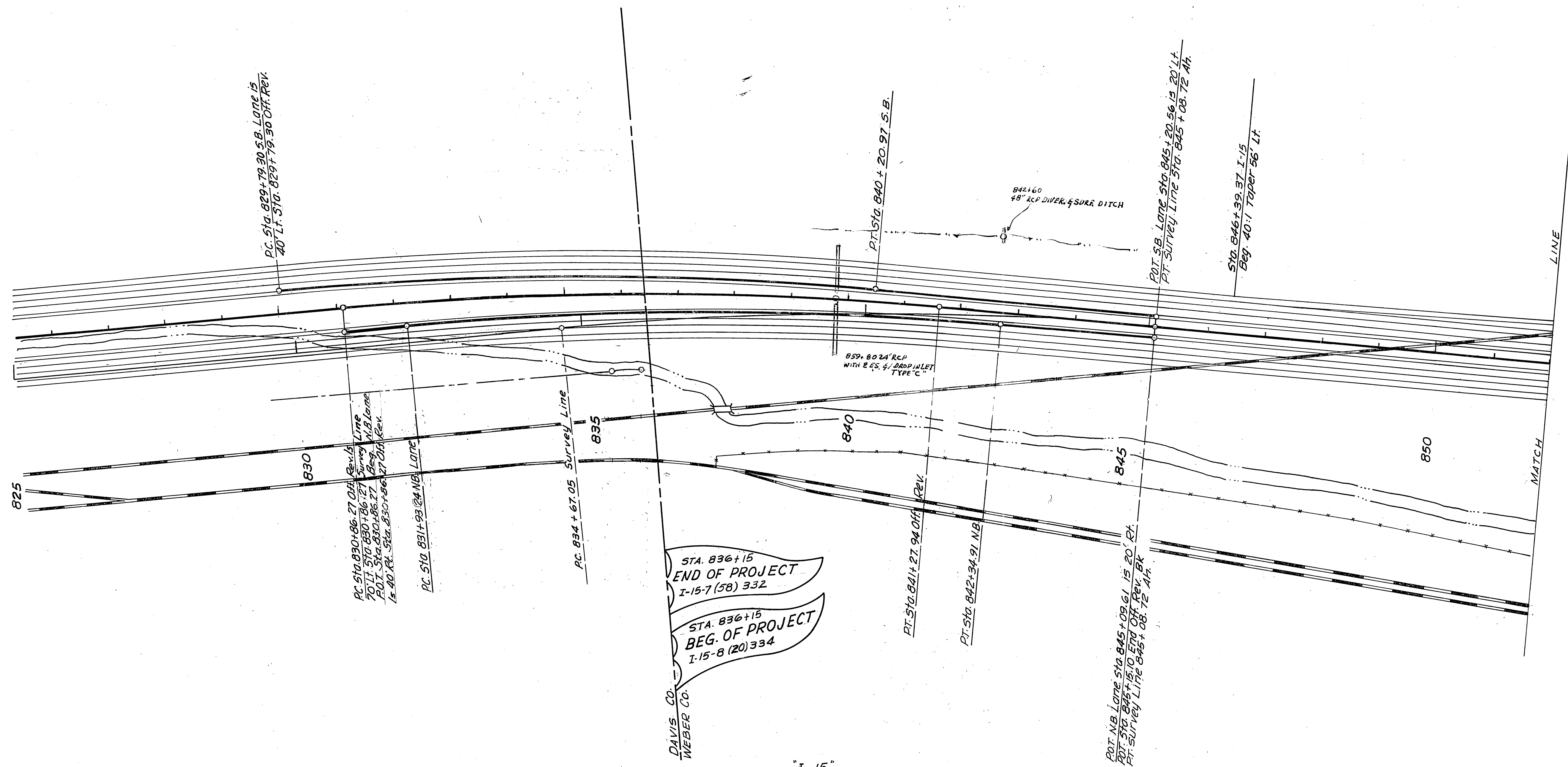


830 31 32 33 34 35 36 37 38 39 840 41 42 43 44 845 46 47 48 49 850 51 52 53 54 855 56 57 58 59 860

7-66B		FISCAL YEAR	SHEET NO.	TOTAL SHEETS
FED. ROAD DIST. NO.	STATE	PROJ. NO.		
UTAH	UTAH	I-15-7(58)332	14	
		I-15-8(20)334		
IG 15 8(18)334				



REVISIONS		
DATE	BY	REV.



STA. 836+15
END OF PROJECT
I-15-7 (58) 332

STA. 836+15
BEG. OF PROJECT
I-15-8 (20) 334

DAVIS CO.
WEBER CO.

"1-15"

$\Delta = 10^{\circ} 25' \text{ Rt.}$
 $D = 1^{\circ} 00'$
 $R = 5729.58'$
 $T = 522.27'$
 $L = 1041.67'$

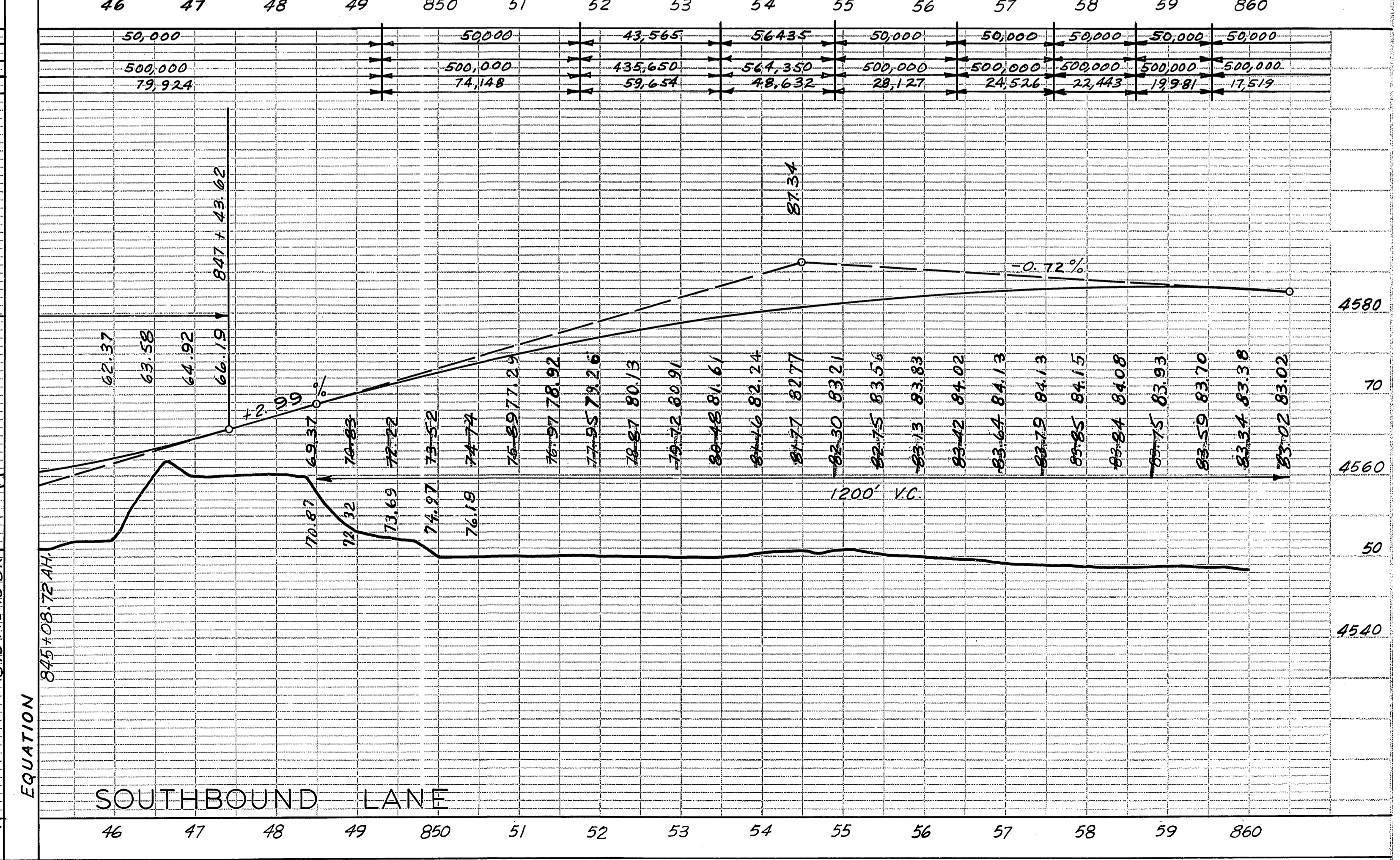
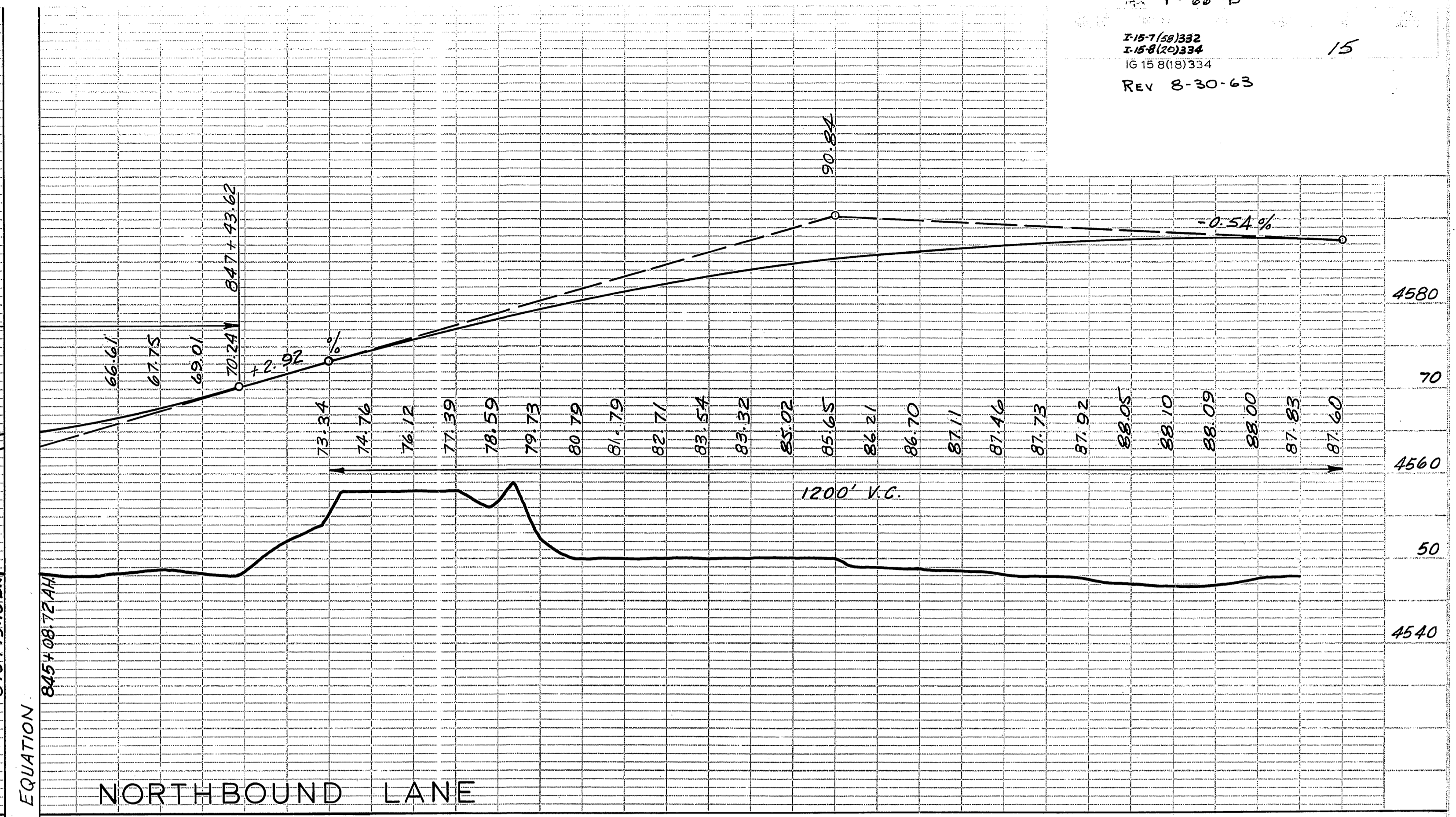
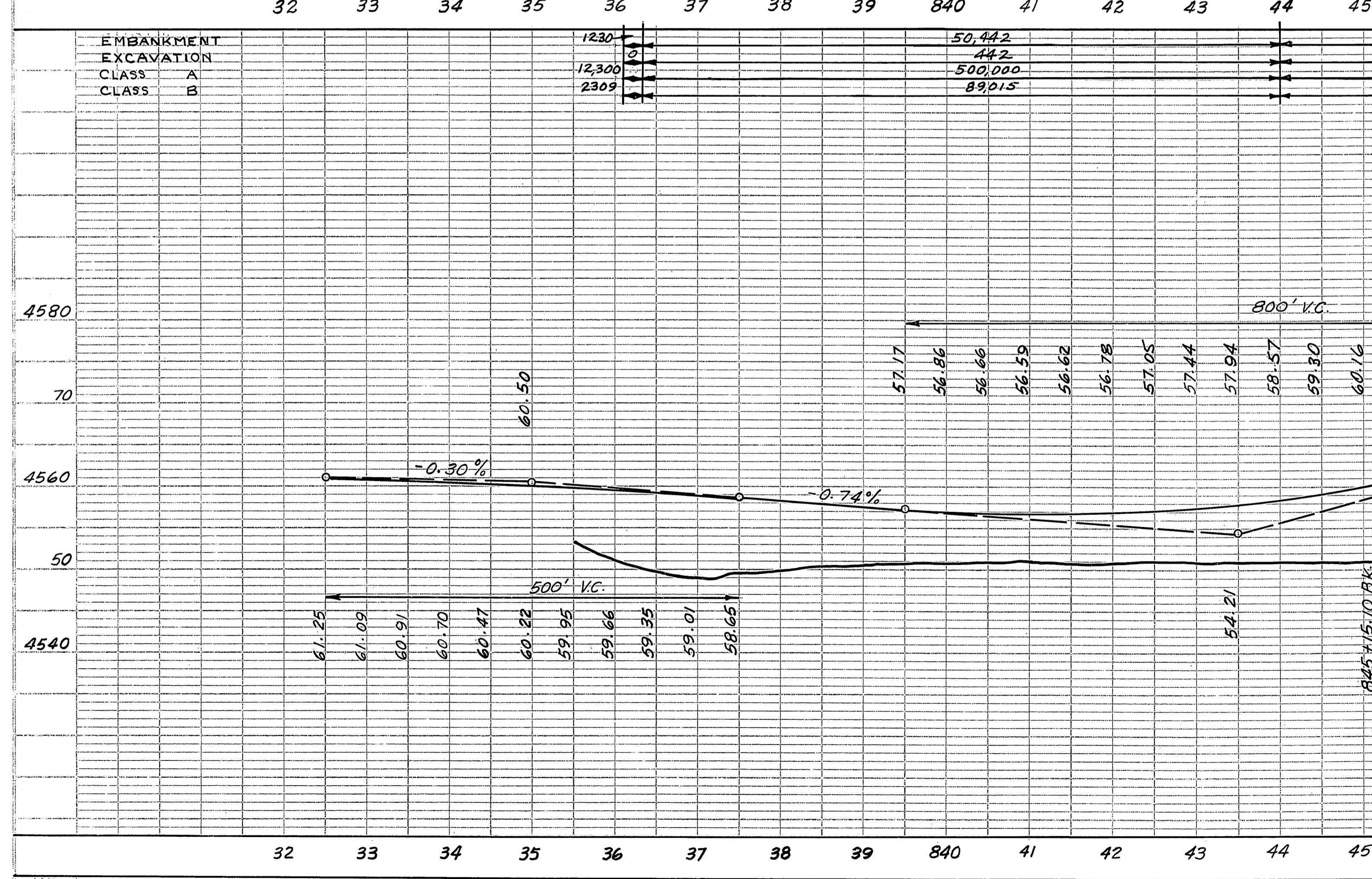
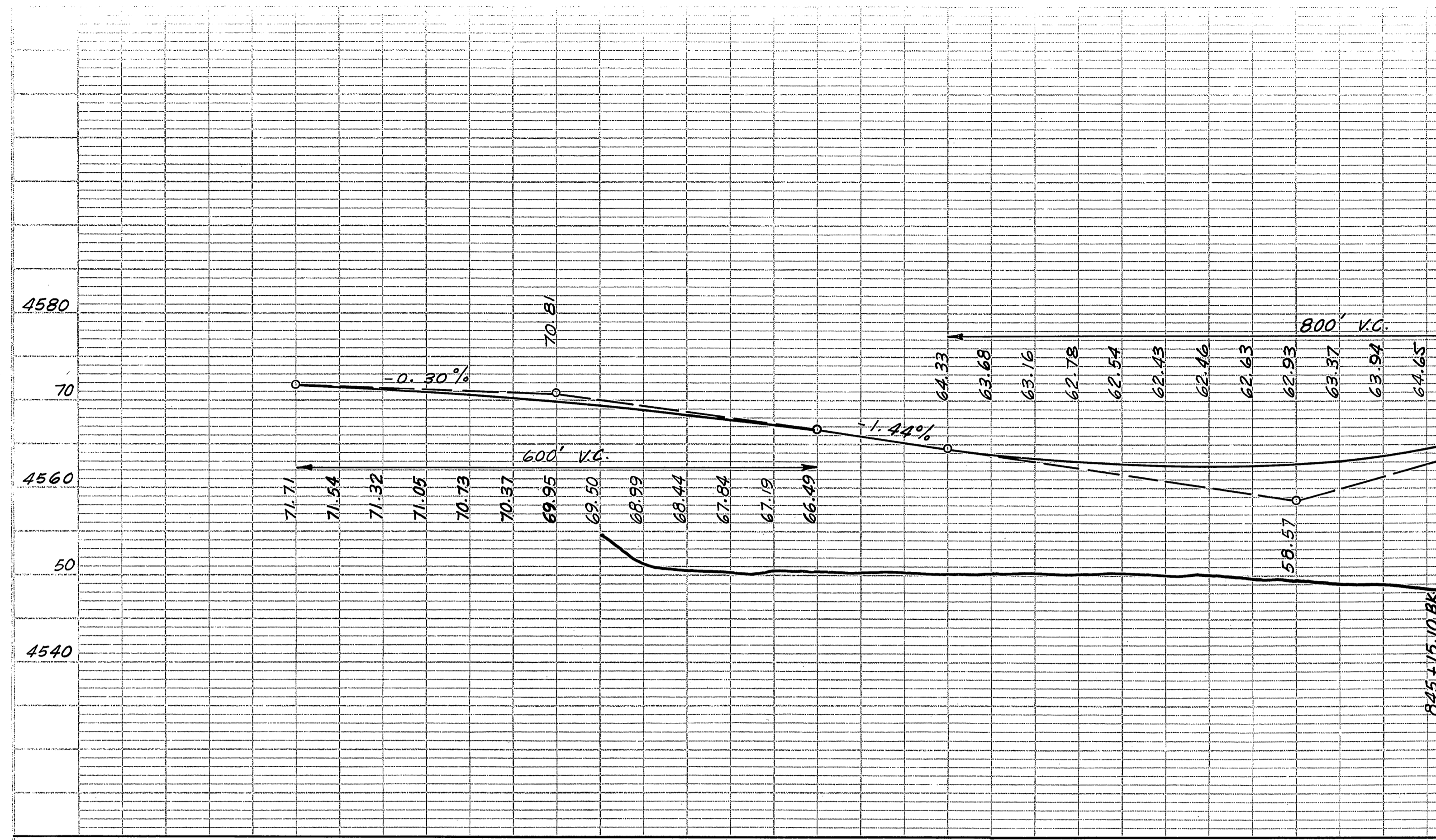
PI. 9 Survey Line Sta. 839+89.32
 Coord: 299, 395.29 N
 1,856, 105.53 E

PI. 9 Off. Revision Sta. 836+08.54
 Coord: 299, 014.87 N
 1,856, 033.64 E

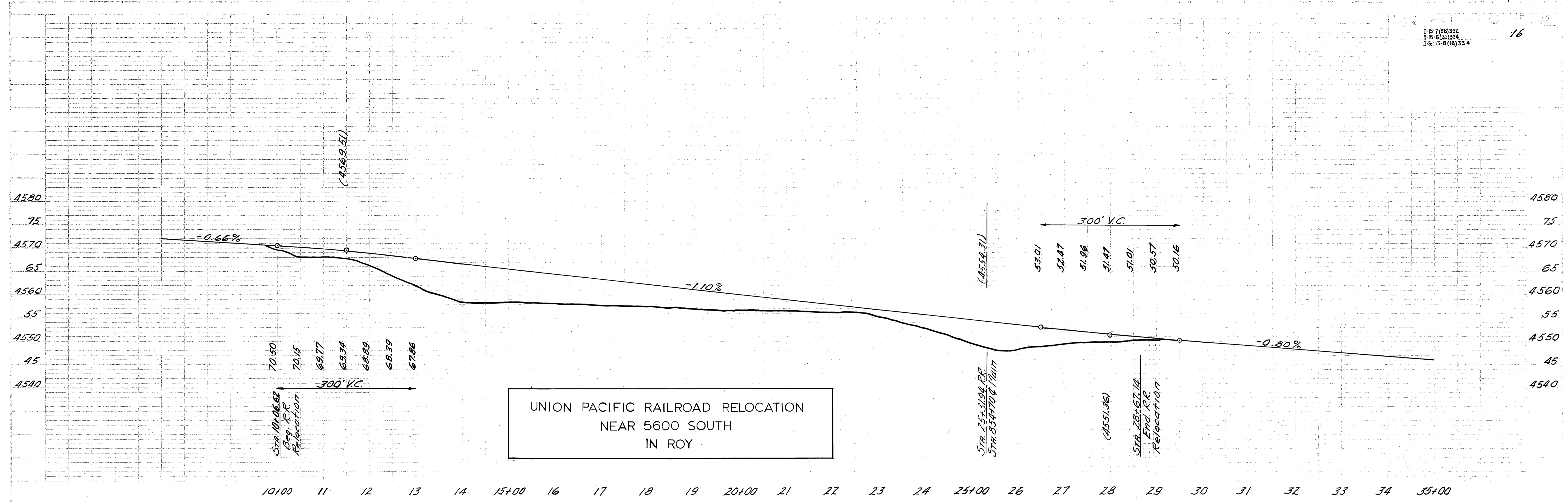
PI. 9 S.B. Sta. 835+01.57
 Coord: 298, 908.09 N
 1,855, 993.11 E

PI. 9 N.B. Sta. 837+15.51
 Coord: 299, 121.64 N
 1,856, 074.17 E

MA T-66-B
I-15-7(58)332
I-15-8(20)334
16 15 8(18)334
REV 8-30-63

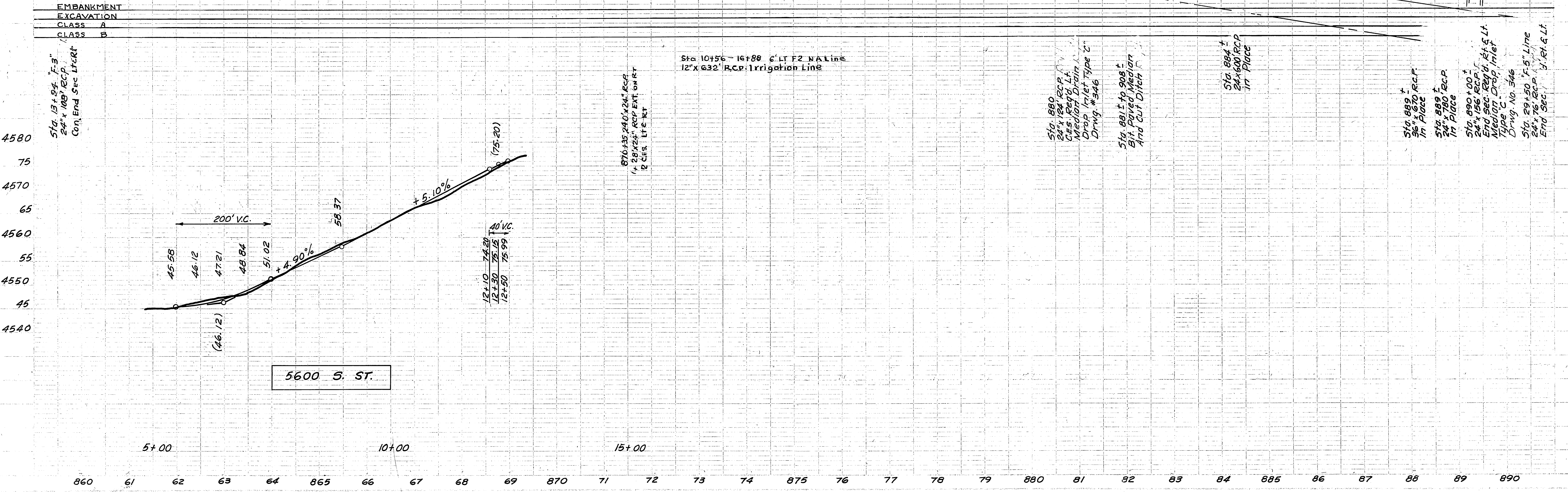
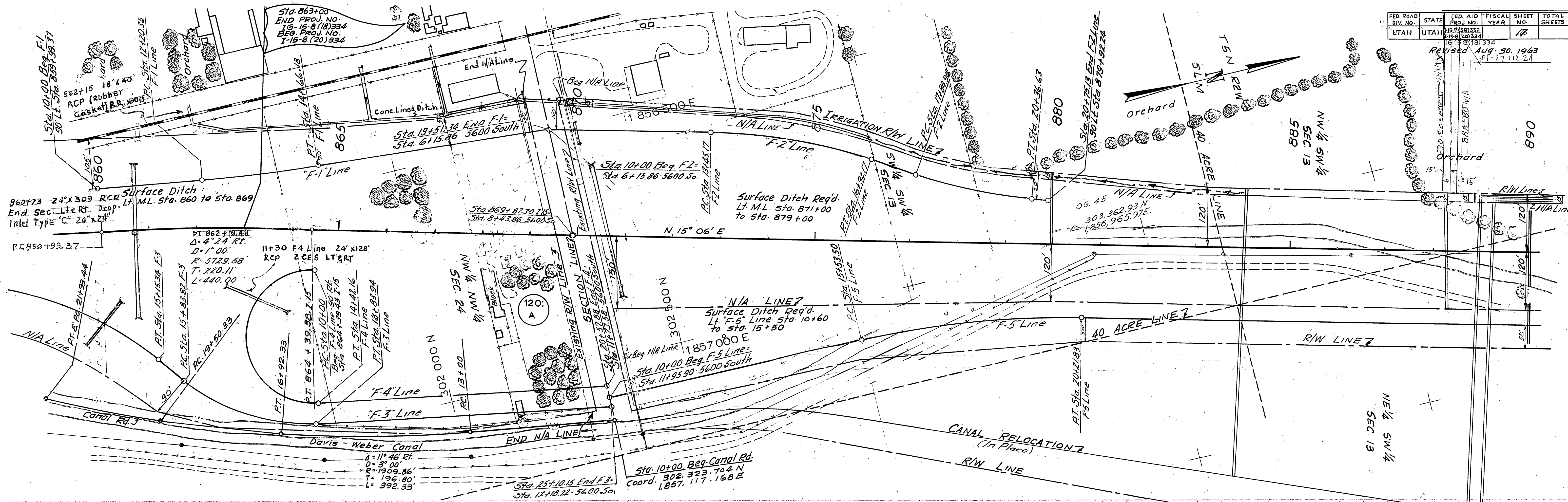


Station	Excavation Class A	Excavation Class B	Embankment
32			
33			
34			
35			
36	1230	2309	1230
37			
38			
39			
40	50,442		
41	442		
42	500,000		
43	89,015		
44			
45			

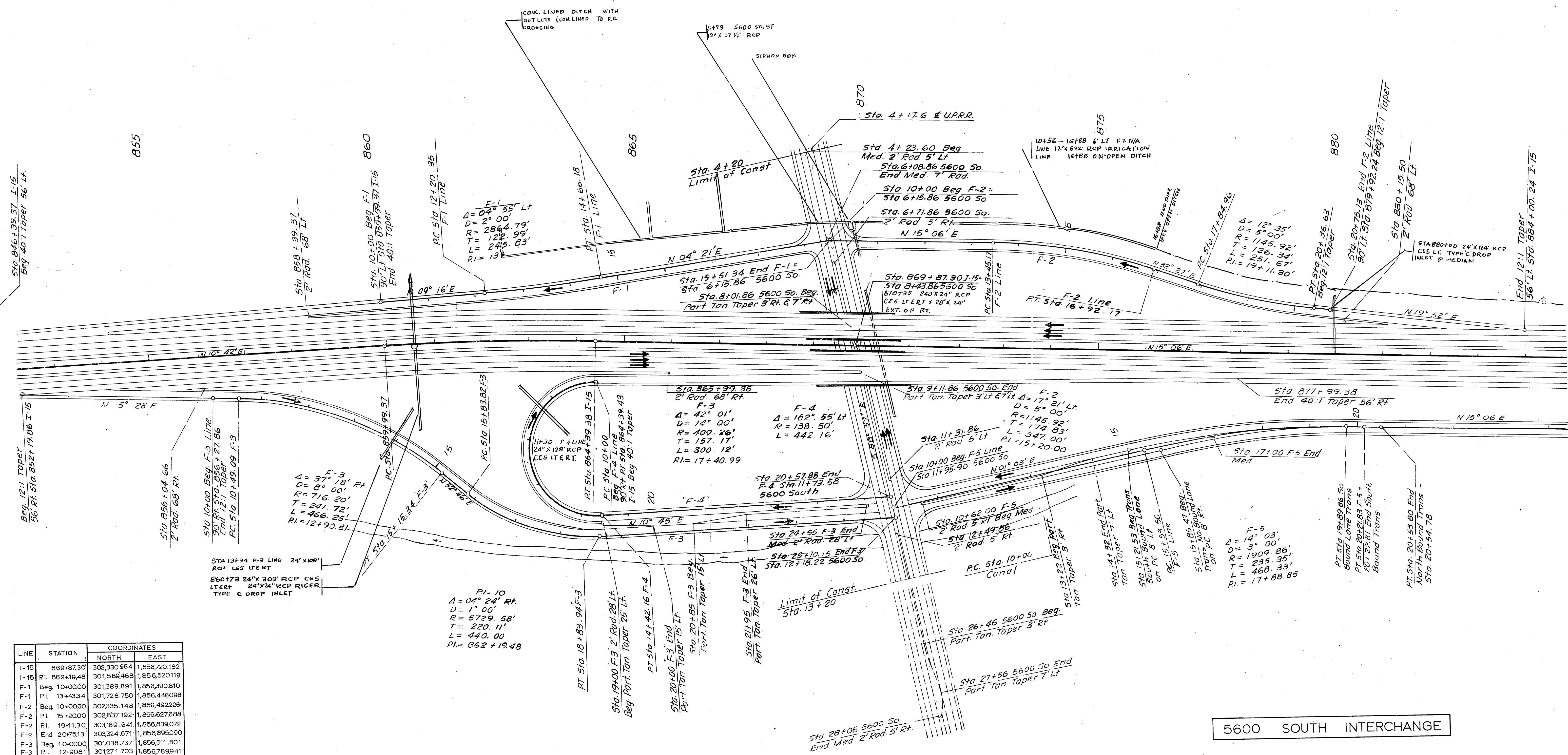
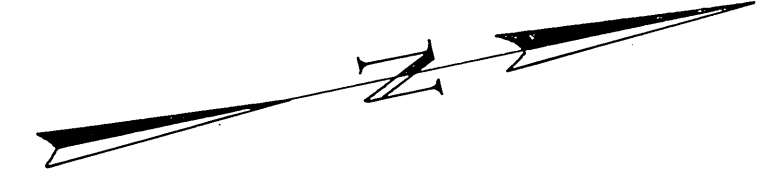


FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	15-8(18)334	1963	13	18

Revised Aug. 30, 1963
 15-8(18)334
 15-8(20)334

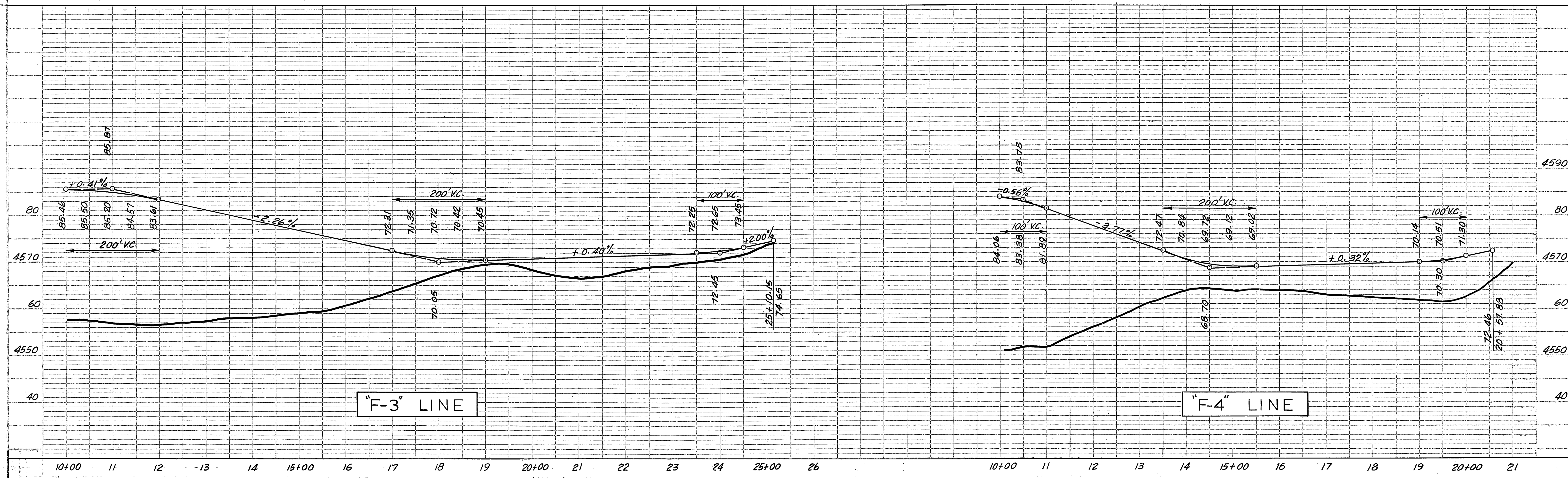
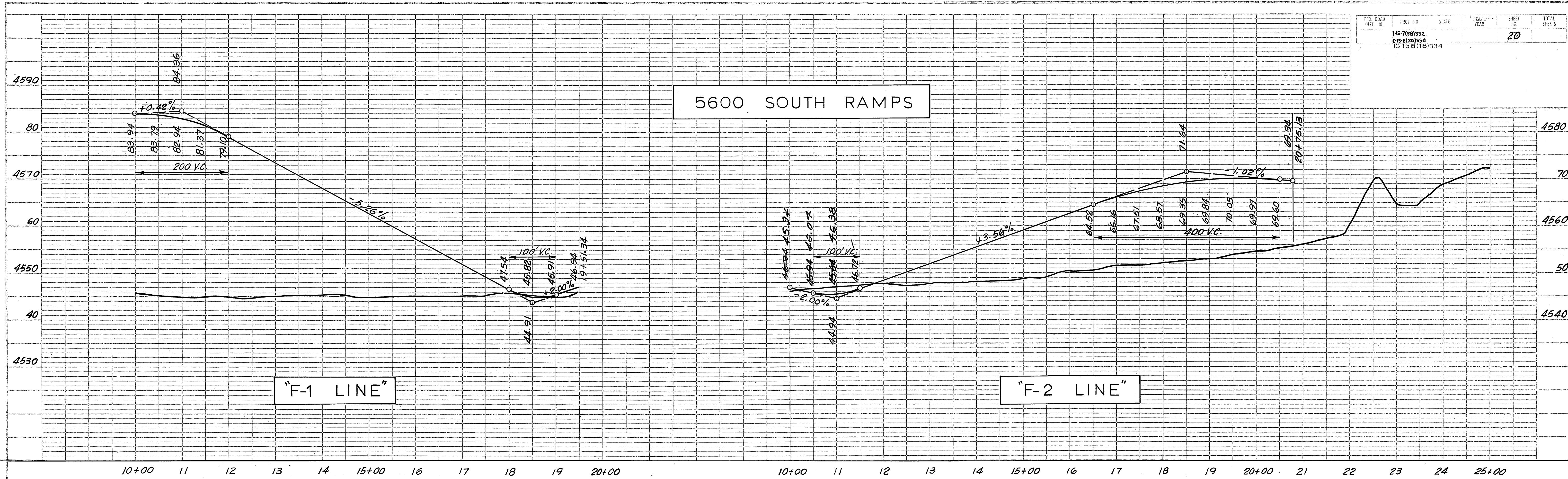


AC 7-14-66 13		PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
FED. ROAD DIST. NO.	STATE	UTAH	1967	18	18
UTAH		116-7(5) 131	115-6(2) 134	16 15 8(1) 1334	

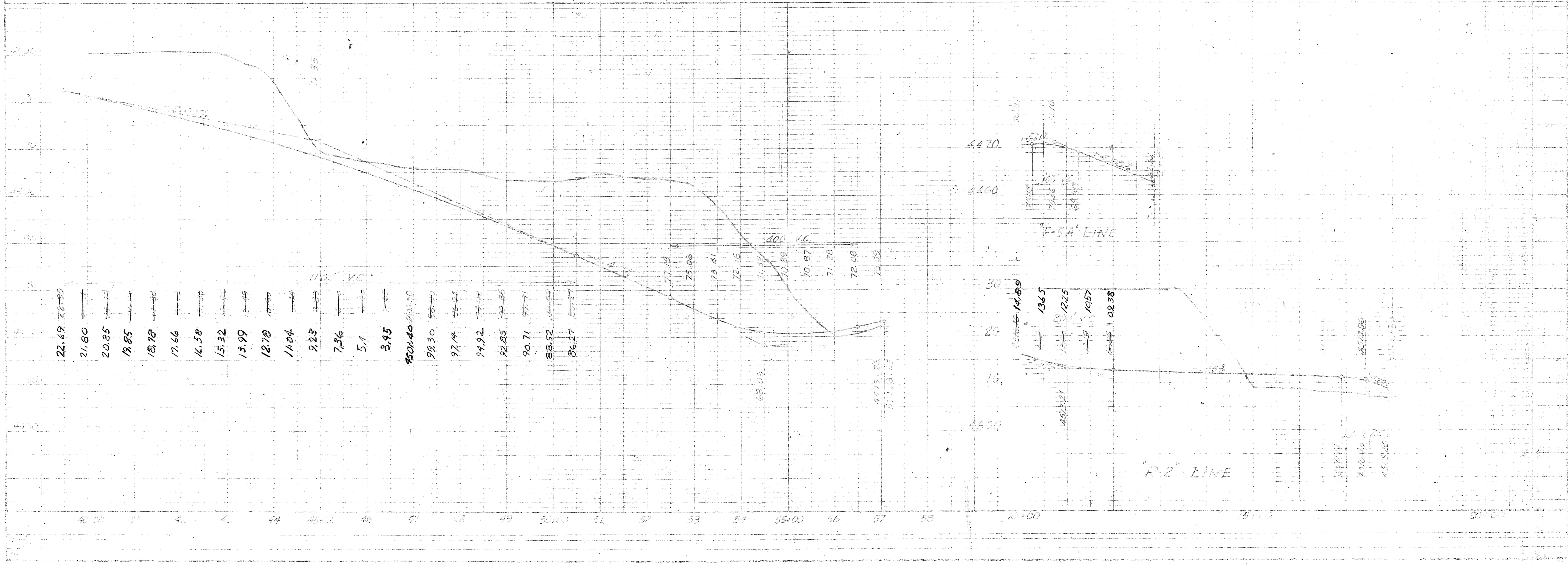
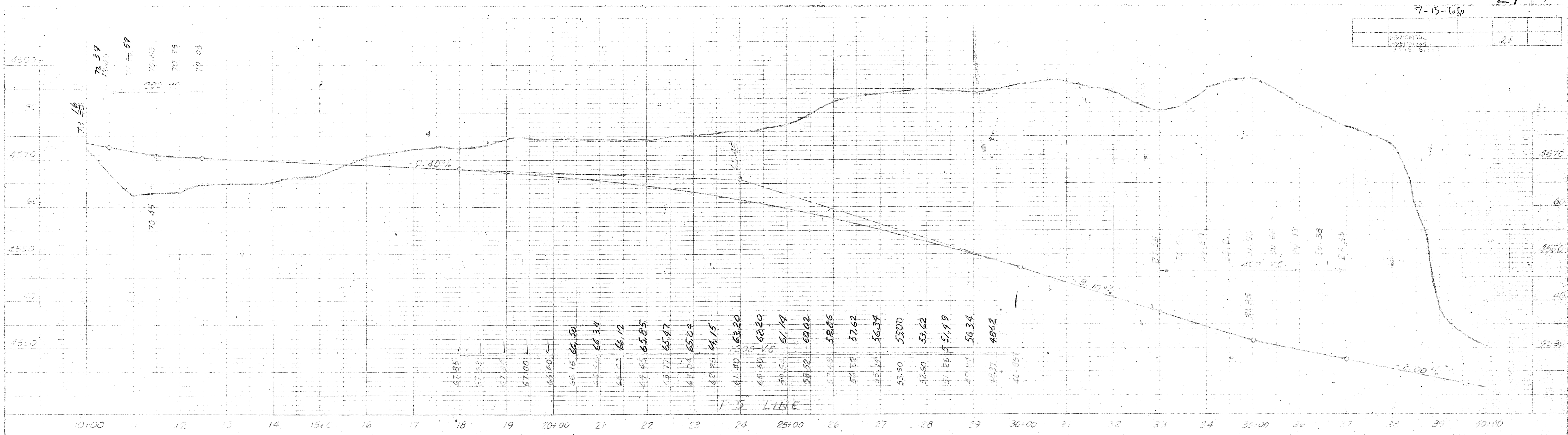


5600 SOUTH INTERCHANGE

5600 SOUTH RAMPS

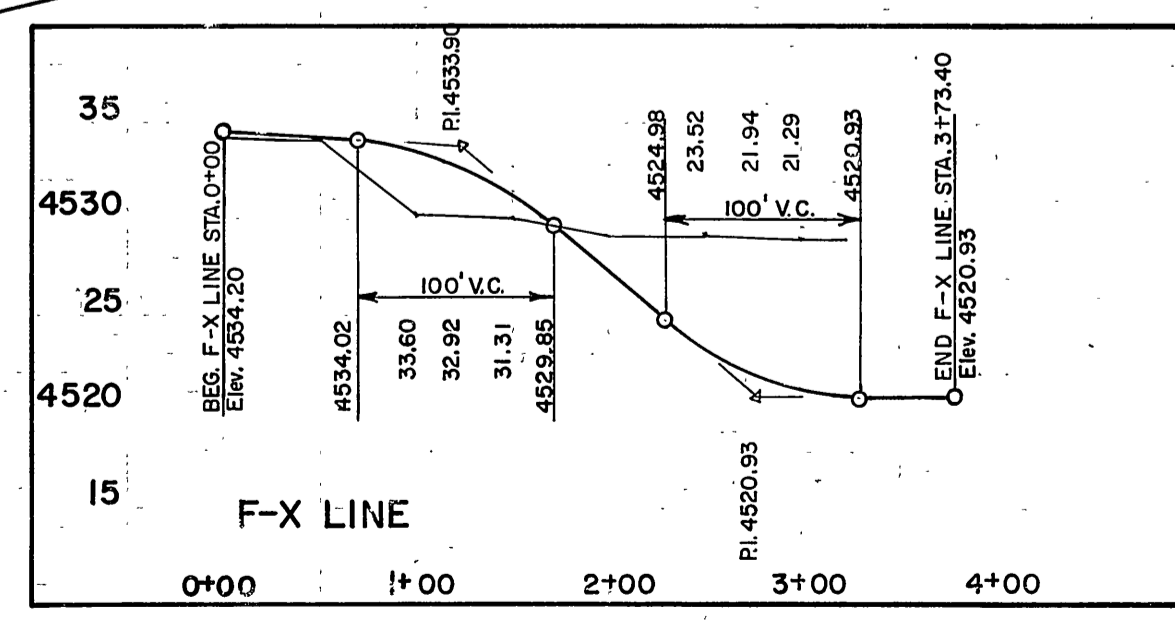
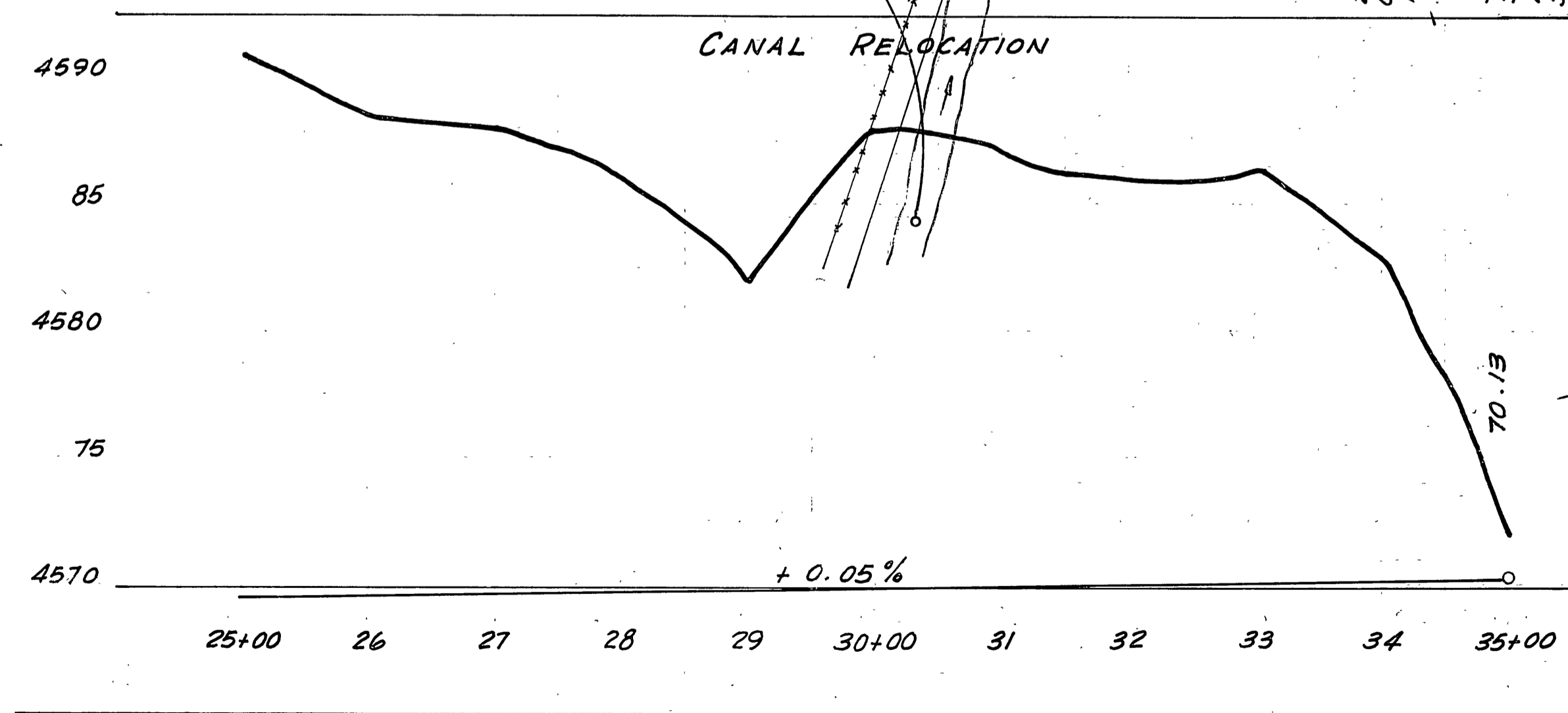
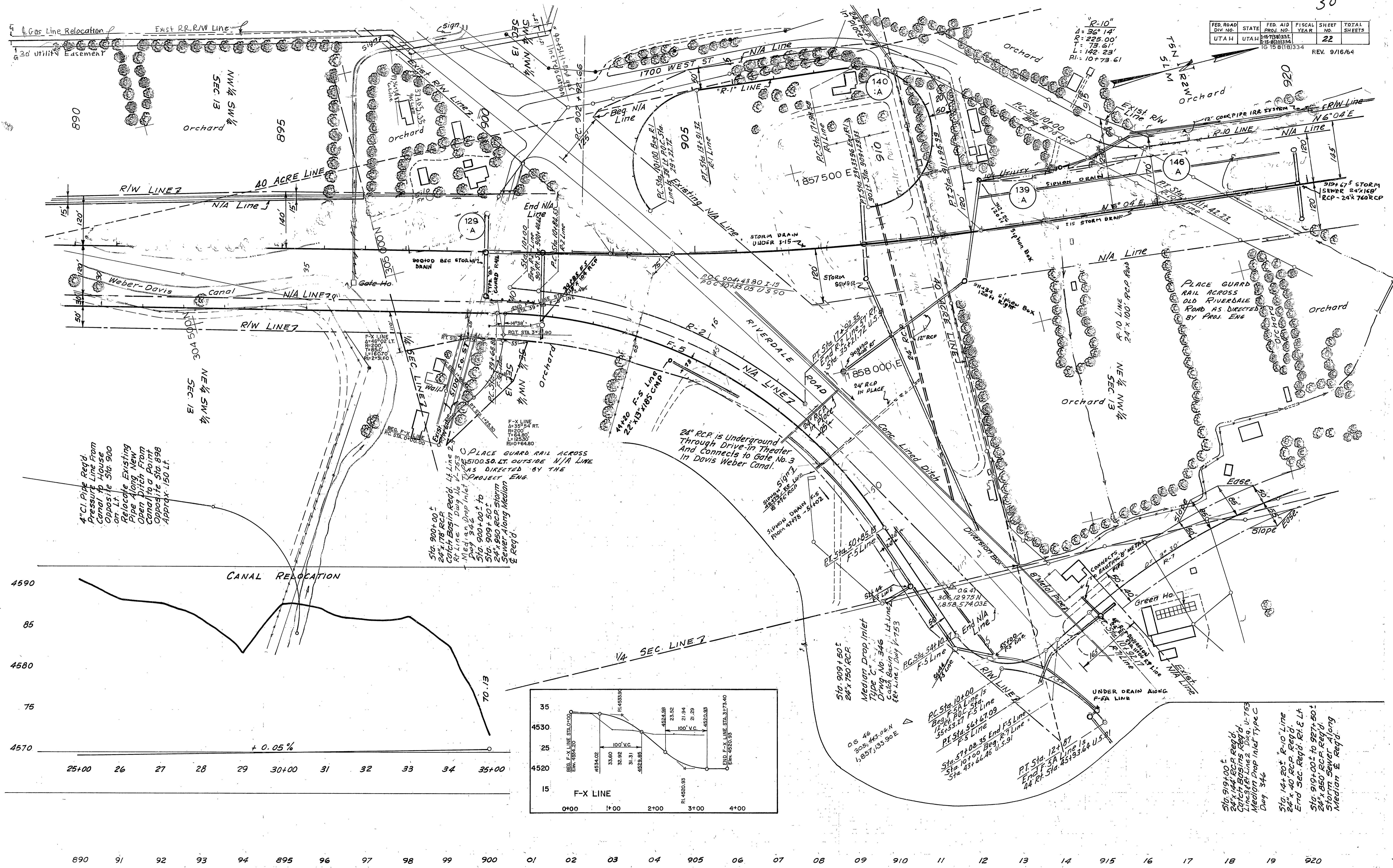


1-27-34-53-2	21
1-28-34-53-1	



FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	44-708(33)	1964	22	16

REV. 9/16/64



890 91 92 93 94 895 96 97 98 99 900 01 02 03 04 905 06 07 08 09 910 11 12 13 14 915 16 17 18 19 920

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
UTAH	UTAH	1-157(50)332	1-158(29)334	24	16 15 8(18)334

Ac. 7-18-66 B.

